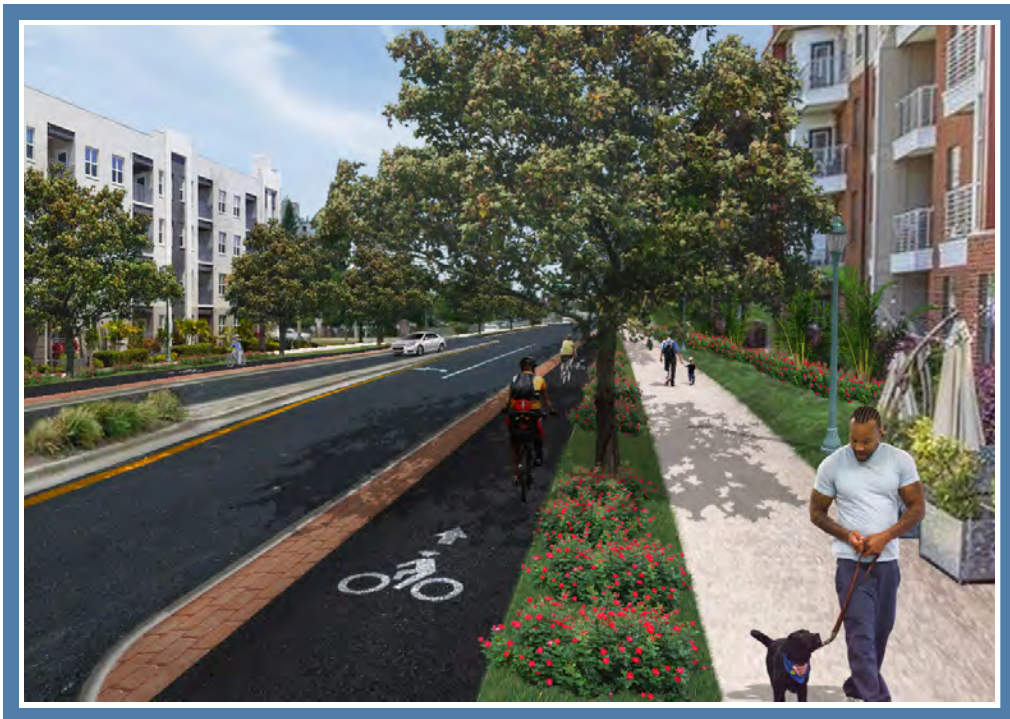




CITY OF SARASOTA BOULEVARD OF THE ARTS AND 10TH STREET



COMPLETE STREETS BASIS OF DESIGN REPORT JULY 2022



Prepared for:



Special thanks to the thousands of citizens and community organizations who dedicated their personal time to participating in public outreach as part of this project.

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Cover photos counter-clockwise from top:

Sarasota-in-Motion Transportation Master Plan, BoTA and 10th Open House May 23, 2022, Sarasota-in-Motion Transportation Master Plan, BoTA and 10th Open House May 23, 2022

TABLE OF CONTENTS

| | |
|--|-----|
| CHAPTER 1: EXECUTIVE SUMMARY | 1 |
| CHAPTER 2: PROJECT OVERVIEW | 3 |
| CHAPTER 3: BACKGROUND | 5 |
| CHAPTER 4: EXISTING CONDITIONS INVENTORY | 9 |
| CHAPTER 5: CITIZEN INVOLVEMENT AND PUBLIC INPUT | 19 |
| CHAPTER 6: CONCEPT PLANS | 39 |
| CHAPTER 7: LONG RANGE ESTIMATE | 45 |
| CHAPTER 8: TRAFFIC STUDY | 49 |
| CHAPTER 9: CONCLUSION | 51 |
| APPENDIX A: PUBLIC INPUT SURVEY RESULTS | 53 |
| APPENDIX B: BOULEVARD OF THE ARTS AND 10TH FULL SIZE CONCEPT PLAN | 194 |
| APPENDIX C: TRAFFIC STUDY REPORT | 198 |

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LIST OF FIGURES

| | |
|--|----|
| Figure 2-1: Adopted Multimodal Connections Plan | 4 |
| Figure 3-1: The Bay Park Master Plan | 7 |
| Figure 4-1: Existing Conditions - Points of Interest..... | 10 |
| Figure 4-2: Existing Conditions - Existing Mobility Options..... | 11 |
| Figure 4-3: Existing Conditions - Neighborhoods..... | 11 |
| Figure 4-4: Existing Conditions - Zoning | 12 |
| Figure 4-5: Existing Conditions - Future Land Use..... | 13 |
| Figure 4-6: Existing Conditions - Speed Limit..... | 14 |
| Figure 4-7: Existing Conditions - SCAT Routes and Stops | 15 |
| Figure 4-8: Existing Conditions - Right-of-Way Width..... | 16 |
| Figure 4-9: Existing Conditions - Curb-to-Curb Width | 17 |
| Figure 5-1: Survey Results for Question 1..... | 20 |
| Figure 5-2: Survey Results for Question 2..... | 21 |
| Figure 5-3: Survey Results for Question 3..... | 21 |
| Figure 5-4: Survey Results for Question 4..... | 22 |
| Figure 5-5: Survey Results for Question 5..... | 23 |
| Figure 5-6: Survey Results for Question 6..... | 24 |
| Figure 5-7: Survey Results for Question 7..... | 25 |
| Figure 5-8: Survey Results for Question 8..... | 26 |
| Figure 5-9: Survey Results for Question 9..... | 27 |
| Figure 5-10: Survey Results for Question 10 | 28 |
| Figure 5-11: Survey Results for Question 11..... | 29 |
| Figure 5-12: Survey Results for Question 12..... | 30 |
| Figure 5-13: Survey Results for Question 13 | 31 |
| Figure 5-14: Survey Results for Question 14 | 32 |
| Figure 5-15: Survey Results for Question 15 | 33 |
| Figure 5-16: Survey Results for Question 16 | 34 |
| Figure 5-17: Survey Results for Question 17 | 35 |
| Figure 5-18: Hardscape and Landscape Bulletin Dot Board | 37 |
| Figure 5-19: Site Furnishings Bulletin Dot Board..... | 37 |
| Figure 6-1: Segment 1 Concept Plan..... | 41 |
| Figure 6-2: Segment 2 Concept Plan..... | 41 |
| Figure 6-3: Segments 3 and 4 Concept Plan..... | 42 |
| Figure 6-4: Segment 5 Concept Plan | 42 |
| Figure 7-1: Engineers Estimate of Probable Cost..... | 45 |

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CHAPTER

1

EXECUTIVE SUMMARY

As downtown Sarasota has continued to grow into a vibrant community where dining at renowned restaurants, shopping at unique boutiques, and visiting beautiful parks is commonplace. The demand for transportation alternatives such as walking, biking, and transit have increased. In addition, the emerging Rosemary District has become a central hub of activity for a diverse crowd of young professionals, artists, families and empty nesters.

This resurgence has resulted in a dire need for supportive infrastructure for pedestrians and bicyclists. As a result, the redesign of Boulevard of the Arts (BotA) and 10th Street (10th) were identified as priority projects in *Sarasota in Motion*, the City's Transportation Master Plan, accepted in 2020.

BotA and 10th serve as key links between greater Sarasota and the Bay Park (The Bay), a master planned initiative to redevelop 53-acres of city-owned land along Sarasota Bay on the west side of US 41/ Tamiami Trail. 10th will functionally serve as The Bay and a future Performing Art Center's front door for most visitors while BotA will remain as an important pedestrian connector to the park district.

To facilitate the conceptual reimagining of these vital streets, the City of Sarasota tasked Kimley-Horn to develop a basis of design (BOD) report to assess the existing conditions and contemplate potential multimodal improvements for BotA and 10th. The study area for BotA extended from Orange Avenue to the terminus of the City right-of-way in the west. The study area for 10th also extended from Orange Avenue in the east to the extent of the recently improved roundabout at US 41 in the west. Please see Chapter 4 for additional information on the study area.

After examining the physical right of way, studying traffic patterns and land uses, and speaking with the community about their desires through both community surveys, typical sections were developed that respond to the unique context of the two streets. In order to accommodate the varying right-of-way widths and other community needs, of BotA and 10th, five typical segments are proposed. See Chapter 6 for the proposed segments and proposed concept plan.

While the concepts for the two streets both increase canopy street tree coverage and promote bike/ped and micromobility safety, there are key differences in the proposed concepts because these streets serve different needs, users, and purposes. BotA makes use of sharrows within the travel lane as bicycle infrastructure and prioritizes the pedestrian realm between the back of curb and building

facades. The addition of on-street parking, reduction in unnecessary turn lanes, widening sidewalks, adding street trees, and narrowing of the curb-to-curb width will increase "friction" for drivers and lower vehicle speeds, increasing pedestrian comfort and safety. BotA continues to display retail and storefront viability, and wider sidewalks would support additional café seating and street activation. 10th will become a true complete street, with the addition of protected bike lanes on both sides of the roadway, widened sidewalks, stormwater treatment and additional landscape/street tree opportunities. Turn lanes will continue to ensure adequate traffic flow on this important corridor that serves a variety of land uses, including industrial users north of downtown and serves travelers from throughout the City and region. All segments accommodate Sarasota County Area Transit (SCAT) shelters where necessary.

The project is consistent with guidance published by the Florida Department of Transportation (FDOT), Florida Greenbook, American Association of State Highway Transportation Officials (AASHTO) Manual, National Association of City Transportation Officials (NACTO) Urban Bikeway Design Guide and Urban Street Design Guide for pedestrian and bicycle accommodations. The conceptual design was developed with significant input from the public and City staff. A traffic study was also performed to review impacts of the proposed improvements on motor vehicle traffic. The study found that there would be minimal impact to traffic flows from the proposed improvements and that the bicycle/ pedestrian facility improvements were both appropriate and balanced, thus benefiting the urban environment.

Overall, the implementation of the recommendations included in this report will result in a transformation of BotA and 10th with a multimodal vision aligned with the City's Long-Range Planning goals. The design will allow for safe bicycle travel, increased pedestrian activity, and encourage other micro-mobility opportunities. The concept plan fits the context of a highly urban area; supports economic development; complements the surrounding land uses; simplifies conflicts with other modes of travel; and directly increases safety of all road users, including those walking, biking, and driving.



CHAPTER

2

PROJECT OVERVIEW

This basis of design (BOD) report is focused on planning and engineering elements of the Boulevard of the Arts (BotA) and 10th Street (10th) Complete Street projects. It is the result of a technical evaluation, project planning meetings, review of industry best practices, traffic data collection and analysis, and input gathered during the public engagement process. This BOD serves as a record to document guidance and decisions made during the design development process that affect the implementation of these complete streets projects.

The vision for the BotA and 10th Project is to provide safe and comfortable connections for pedestrians, cyclists, and drivers between adjacent neighborhoods of the Rosemary District, Central Coconut, and Gillespie Park, as well as the Bay Park and the Quay. The project, as planned, will enhance the multimodal transportation network within the City and region, support economic development, transportation, and recreation, and is consistent with the City's Comprehensive Plan, Multimodal Connections Plan (See Figure 2-1) and recently completed Citywide Transportation Master Plan, *Sarasota in Motion*.

The following methodology describes the stepped scientific process of evaluation and appraisal proposed to develop a basis of design:

- Researching history and vision for the corridor
- Examining existing transportation and land use conditions
- Soliciting comments from the public and key community stakeholders
- Development of a concept plan
- Study of transportation operations and impacts

The subsequent chapters describe each step of the process and detail their individual findings. The executive summary provides a succinct description of the overall basis of design.

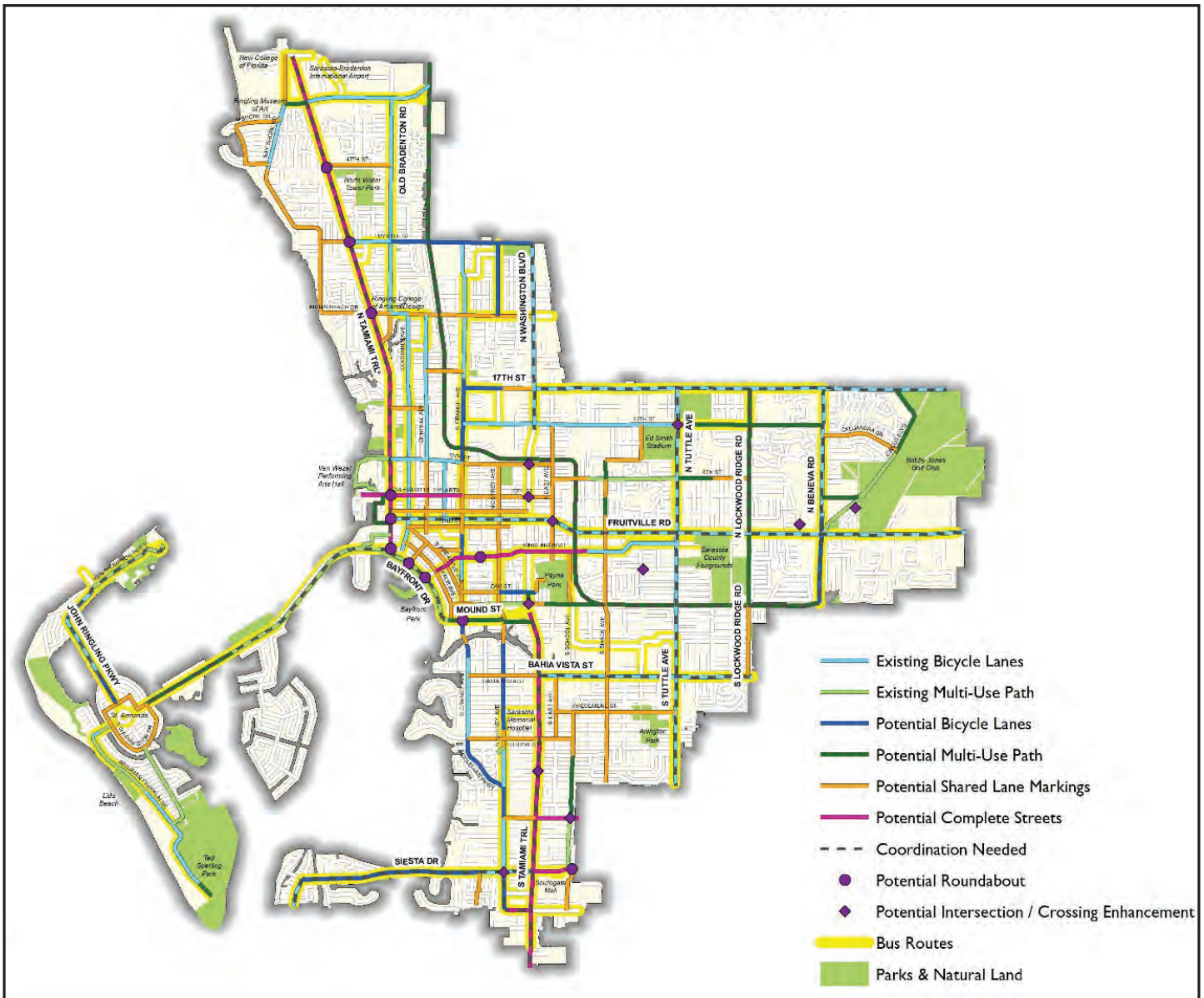


Figure 2-1: Adopted Multimodal Connections Plan

CHAPTER

3

BACKGROUND

The City of Sarasota has a long history of proactive urban planning. Its first Comprehensive Plan was adopted in 1925, well before it was required through the Florida Local Government Comprehensive Planning and Land Development Regulation Act of 1975 (later amended in 1985). Since then, the City has been an active participant in the creation of other plans and documents focused on maintaining the high quality of life in the City, most notably the *Ringling Trail Basis of Design Report*, the *Sarasota Bayfront Master Plan*, the *Downtown Mobility Study*, the *Sarasota-in-Motion Citywide Transportation Master Plan (Sarasota in Motion)*, the *Bayfront Connectivity Plan*, and the *City of Sarasota Downtown Master Plan*.

The *City of Sarasota Downtown Master Plan* was prepared in the year 2000 to provide the City with a clear vision for future development within its downtown area. While a relatively small geographic area, the downtown is considered the economic and employment “heart” of the city. When planning for continued revitalization and redevelopment, the City sought to address the following issues: connection of the downtown to the bayfront area, walkability, a balanced transportation system, civic and cultural improvements, and implementable objectives.

Over the course of the past two decades the downtown has boomed, as many citizens have rediscovered the appeal of urban living. Street design investments to support the boom have been focused on the premier commercial corridors of Main Street and Palm Avenue with priority given to pedestrian experience, safety, and parking opportunities. Very few bicycle facilities have been built in or around the downtown area and most bicyclists ride on sidewalks or in the street, mixing with traffic.

Sarasota in Motion, the City’s Transportation Master Plan, identifies both Boulevard of the Arts (BotA) and 10th Street (10th) as part of the top ten priorities for funding and investment in the City’s multimodal transportation network. In 2020, the City Commission adopted a Five-Year Capital Improvement Plan that included funding for the planning phase of each of these projects.

Both streets, BotA and 10th, serve as key transportation corridors to the City. Unique to them are the surrounding land uses, active projects, and the neighborhood known as the Rosemary District.

Most of what is now Rosemary District was Sarasota’s first documented African American community — known as Overtown until the 1990s. The name generally associated with the initial settlement of Overtown is Lewis Colson. This formerly enslaved man arrived in Sarasota in 1884 and, as assistant to engineer/surveyor Richard E. Paulson, drove the first stake in the ground in 1885 to plat the original town of Sarasota.

By 1920, Overtown was a thriving community. Its population grew as African Americans moved to Sarasota for jobs in the hotel and construction industries. Its business community grew to provide goods and services that were otherwise only available to white Sarasotans. As a result, Overtown had a variety of single-family homes, commercial buildings,



churches, schools and social clubhouses — many built in the mid-1920s.

Today, Overtown has redeveloped into the Rosemary District. It is a popular locale for a diverse crowd of young professionals, artists, and retirees with a variety of housing, restaurants and hotels. This downtown neighborhood is eclectic, artful, and vibrant and continues to grow and attract more energy. This boom in redevelopment has resulted in increased demand for supportive infrastructure for pedestrians and bicyclists.

BotA and 10th also serve as key connections to The Bay Park. The Bay Park is a master planned initiative to redevelop 53-acres of city-owned land along Sarasota Bay on the west side of US 41/Tamiami Trail, see Figure 3-1 for the master plan. The Bay’s mission is to establish a cultural and economic legacy for the region while ensuring open, public access to Sarasota Bay. The 10-acre Phase I of The Bay is anticipated to open in the Fall of 2022. The remaining acreage will come online over the course of the next 5-10 years, with a focal point being a new Sarasota Performing Arts Center (SPAC). 10th will functionally serve as The Bay and the SPAC’s front door for most visitors while BotA will remain as an important pedestrian connector to the improvements.

Both BotA and 10th (east of Tamiami Trail) are primary

streets or proposed primary streets as recorded in the Proposed Primary Grid Street Map, 2022. Bicycle/ pedestrian infrastructure on these routes will provide support for the growing Rosemary commercial sector. Businesses in the Rosemary District are expected to see great benefits from increased connectivity in the project area, as several recent studies have found. For example, the *National Street Improvements Study*, conducted by Portland State University in conjunction with PeopleForBikes, researched the economic effects of bicycle infrastructure on 14 corridors across six American cities in various geographic areas and contexts. The Study found bicycle and pedestrian infrastructure improvements resulted in generally positive impacts on the local economy as measured through sales and employment.

Another study, *The Consumer Behavior and Travel Choices: A Focus on Cyclists and Pedestrians Study*, conducted by Portland State University in conjunction with PeopleForBikes, researched the links between consumer behavior and the mode of transportation used to access local destination. The Study found that on average customers that arrive by modes of transportation other than motor vehicles spend similar amounts or more and are also more frequent patrons.

Another study, *The Active Transportation and Real Estate the Next Frontier Study* conducted by the Urban Land Institute inventoried other studies and shared that the Salt Lake City

Department of Transportation found that in the first six months of the next year after adding protected bike lanes, retail sales were up 8.8 percent compared with a citywide increase of only 7 percent. The Study also showed that a study in Dunedin, Florida of the Pinellas Trail found that retail vacancies declined more than 50 percent in the town after opening the trail.

In addition to the economic development benefits from investing in these corridors, and perhaps more importantly will come corresponding improvements to safety for all users. *The Measuring the Street: New Metrics for 21st Century Streets Study*, conducted by New York City Department of Transportation (NYCDOT) in conjunction with PeopleForBikes, details the metrics NYCDOT uses to evaluate street projects. The Study found that a protected bicycle lane in Manhattan for 8th and 9th Avenues lead to 35% decrease in injuries to all street users (8th Ave.), 57% decrease in injuries to all street users (9th Ave.), and up to 49% increase in retail sales (locally based businesses on 9th Avenue from 23 to 31st Streets, compared to 3% borough wide).

In summary, both BotA and 10th Complete Street redesigns were identified in *Sarasota in Motion* as priority projects after a careful evaluation of a variety of factors related to safety, transportation, recreation, economics, and overall context sensitivity. The proposed concept plans seek to implement the vision established in the plan.



Figure 3-1: The Bay Park Master Plan

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CHAPTER

4

EXISTING CONDITIONS INVENTORY

The purpose of this existing conditions inventory is to identify constraints and opportunities for the redesign of Boulevard of the Arts (BotA) and 10th Street (10th), analyze its function, and begin the process of formulating recommendations. The data for this analysis of existing conditions was compiled from the City of Sarasota and Sarasota County GIS databases, as well as several site visits to the corridors.

Project Context

BotA and 10th provide access to and from various parks and entertainment venues along the Bay, businesses, residential apartments, condominiums, and hotels. These points of interest along the roadway represent the fine-grained mix of uses that contribute to the surrounding vibrant community (See Figure 4-1). More significantly for the purposes of this study, these places represent origins and destinations. Origins and destinations are one of two main contributors to a successful transportation network, the other being network infrastructure.

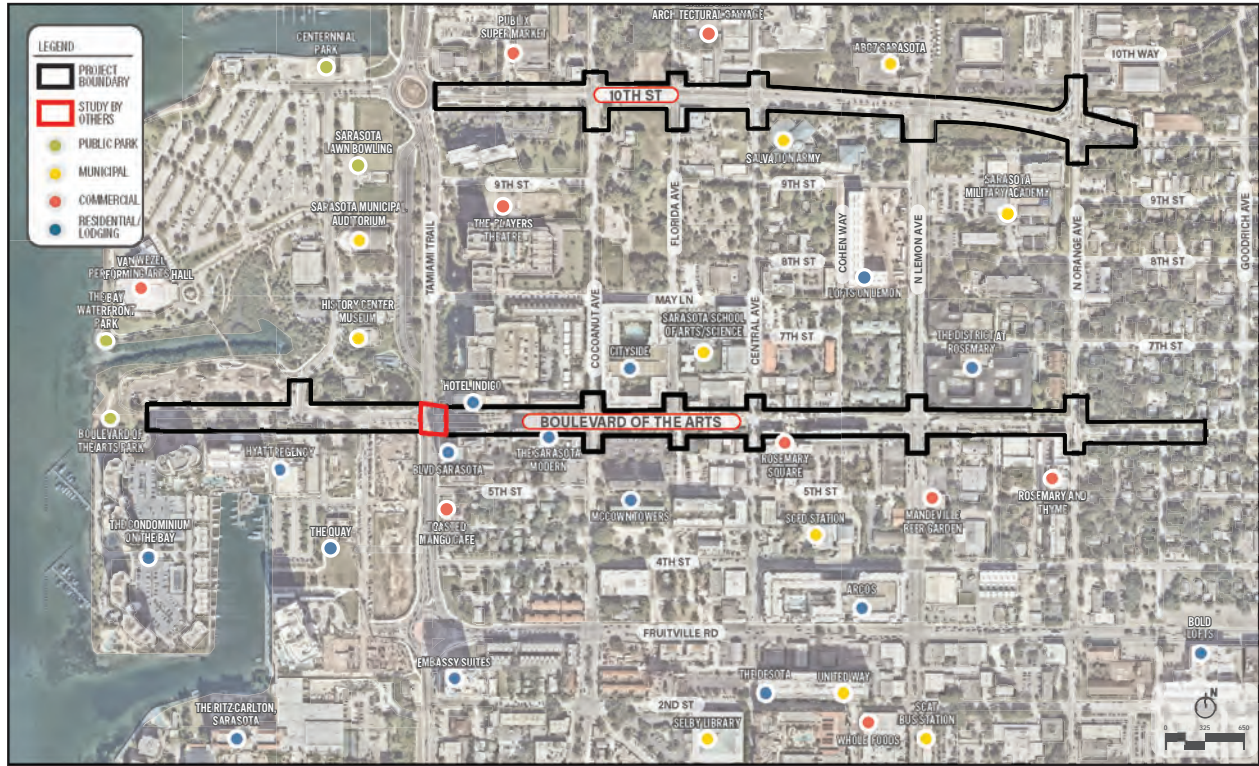


Figure 4-1: Existing Conditions - Points of Interest

The urban environment is made up of a combination of variables that contribute to the availability of mobility options (See Figure 4-2). Increased use of alternative modes is achieved through a balance of these variables. Encouraging areas to develop with a diverse mix of uses, an interconnected street grid, increased densities, and improved access to employment and other non-residential uses increases the percentage of people who can benefit from alternative transportation modes.

Neighborhoods within the project's vicinity are shown in Figure 4-3. These neighborhoods include established neighborhoods such as the Rosemary District, Gillespie Park, Central Coconut, and Five Points. A mix of housing types, ages, and sizes create a natural diversity in these neighborhoods. Residents of these neighborhoods have generally located in these areas because of access to the amenities, employment opportunities, and services which the Downtown offers.

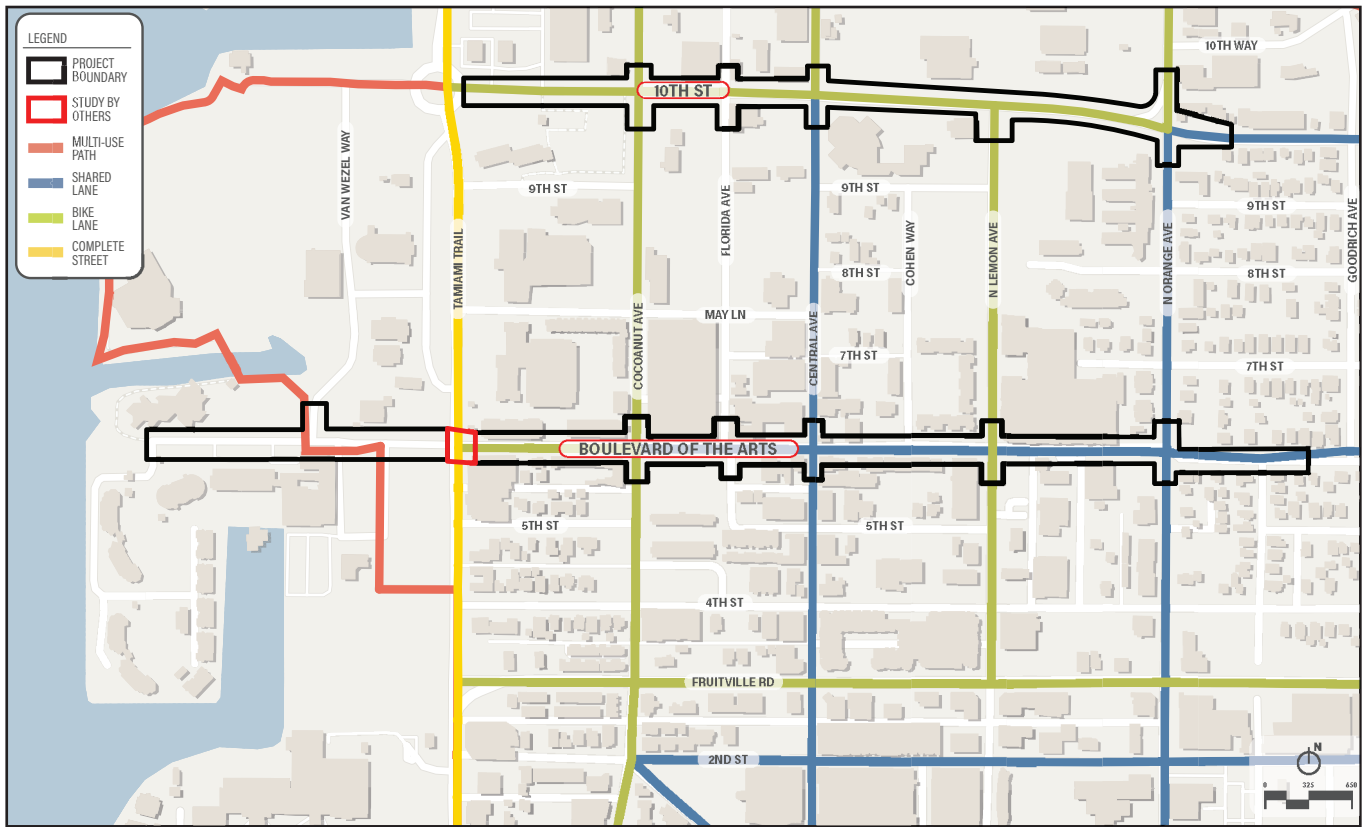


Figure 4-2: Existing Conditions - Existing Mobility Options

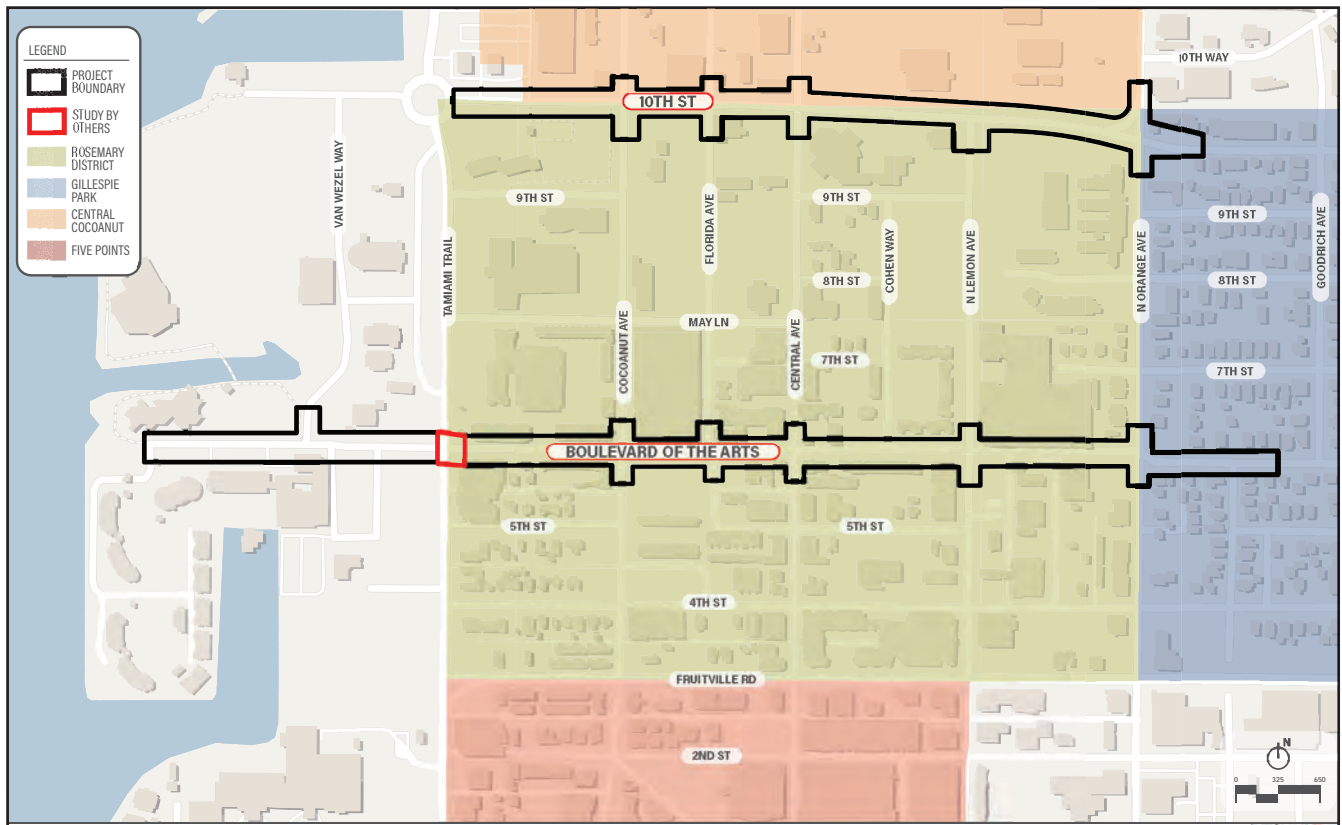


Figure 4-3: Existing Conditions - Neighborhoods



Zoning and Future Land Use

The project area's existing zoning and future land use designations are displayed in Figure 4-4 and Figure 4-5, respectively. The majority of the project area falls within the Downtown Edge (DTE) zoning district, with portions of the project area within the Downtown Neighborhood District, Downtown Core, Downtown Bayfront, Governmental, Commercial Intensive, Commercial Residential, District North Trail, Residential Multi-family, Office Park, Industrial Light, and Industrial zoning districts. Each of these zoning districts provide for the opportunity of development and redevelopment that promotes the creation of additional points of interest and a diverse mix of uses which will continue to increase the percentage of people who will benefit from alternative transportation modes. Transportation improvements create a more people friendly, attractive environment which encourages additional quality development, or redevelopment, and lead to positive outcomes for the larger community.

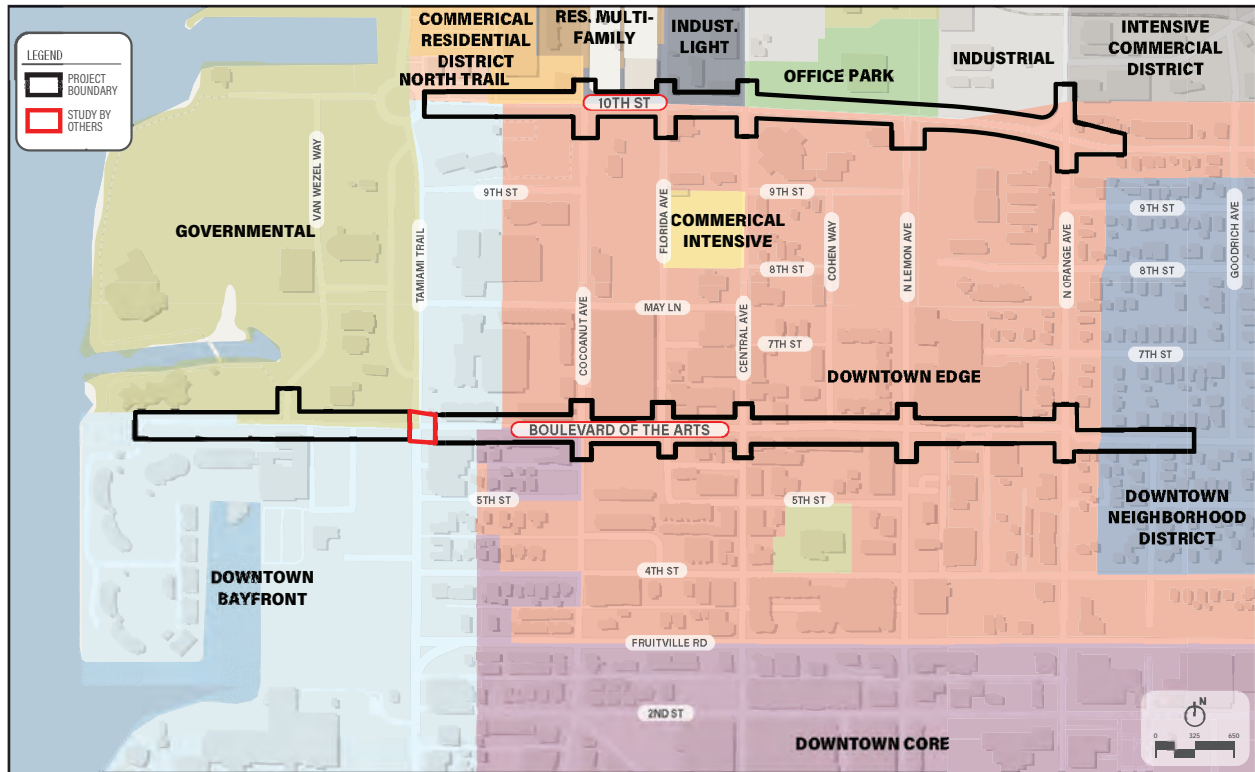


Figure 4-4: Existing Conditions - Zoning

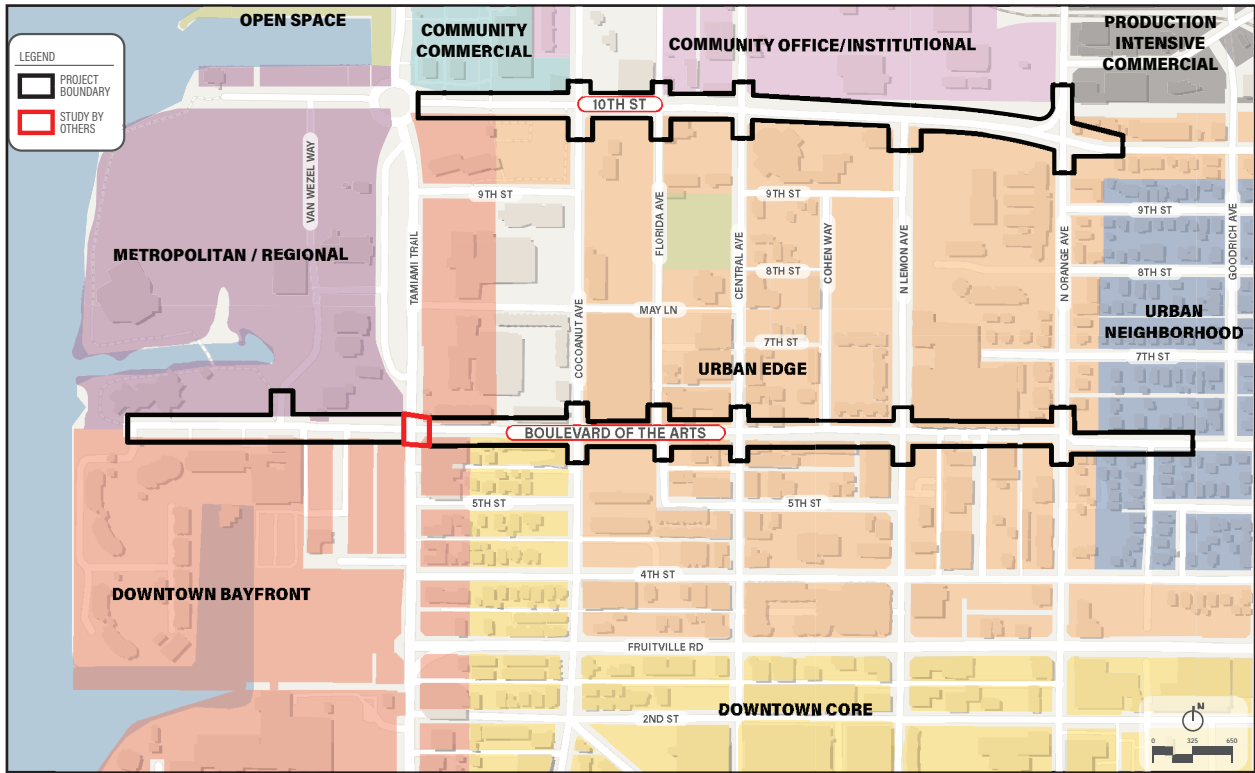


Figure 4-5: Existing Conditions - Future Land Use

Speed Limit

The posted speed limit (See Figure 4-6) displays both BotA and 10th as single speed in the study area. It is the aim of the proposed design to encourage safe travel for all mobility options. BotA is 25 mph for the entire study area. This is a lower vehicle speed that is safe and comfortable for shared lane bicycle riding. However, 10th's posted speed limit is 35 mph.

This is a high vehicular speed for safe and comfortable on-street or shared lane bicycle riding. Although sharrows may be permissible on 10th at these speeds, they are not recommended due to the speed and volume of motor vehicles and potential conflicts between the two modes. The existing roadway section is designed for high speeds and large volumes of cars, however the community context and public input warrant lower speeds in order to accommodate all users. Furthermore, the traffic study, in [Appendix C](#), shows that the existing and projected vehicle volumes are not consistent with the current roadway design.

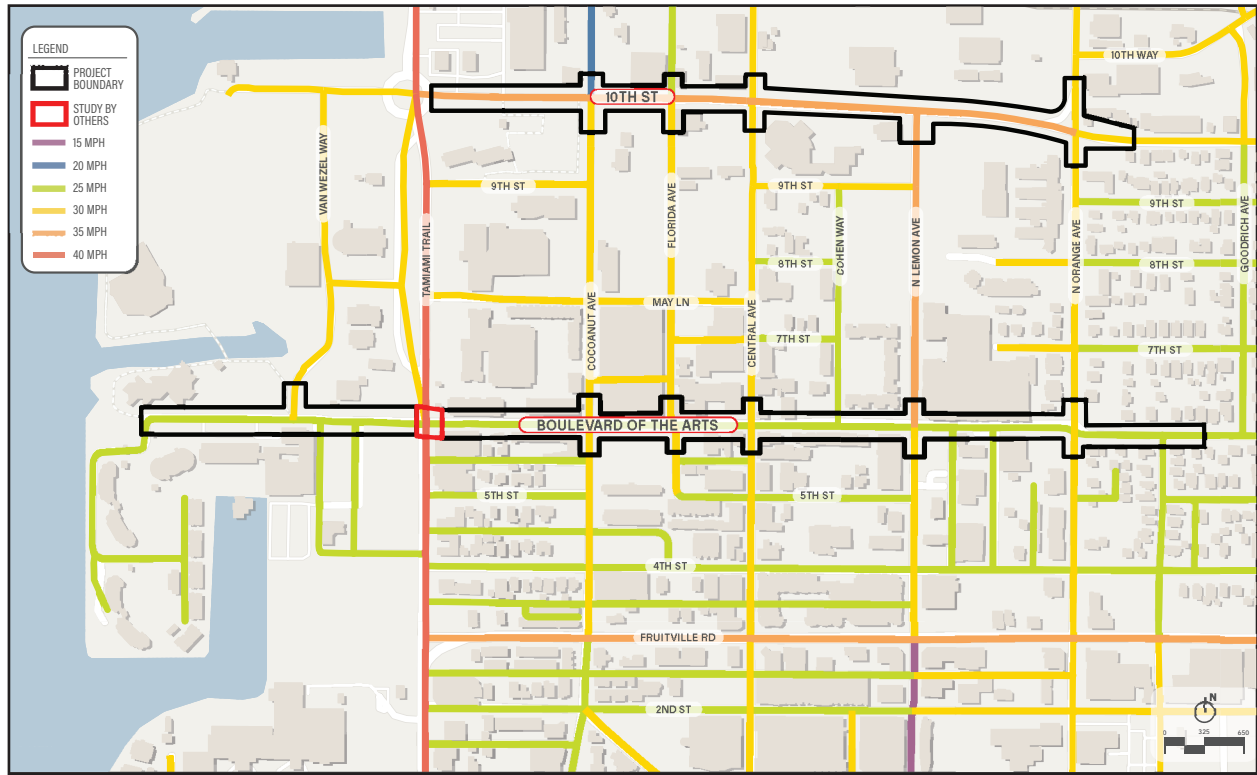


Figure 4-6: Existing Conditions - Speed Limit

Transit

Neither BotA nor 10th are Sarasota County Area Transit (SCAT) bus routes, however SCAT does drive on the road segment between Tamiami Trail and Coconut Ave on BotA. Figure 4-7 displays the SCAT bus routes and stops within the project area. These bus routes and bus stops were taken into consideration and incorporated into the proposed design of the project. When two lanes are available the buses stop in the outside through lane while loading and unloading while other vehicles pass in the median side lane. When the road is only one-lane wide with no lane for vehicles to pass, SCAT prefers to have a bus pullout in which to stop while loading and unloading. The bus pullouts are 150 feet long, with a 50-foot taper into the bus stop, 50-feet for the bus stop and a 50-foot taper out of the bus stop. It is important in creating a multimodal corridor that all forms of transportation are designed for which this project aims to accomplish.

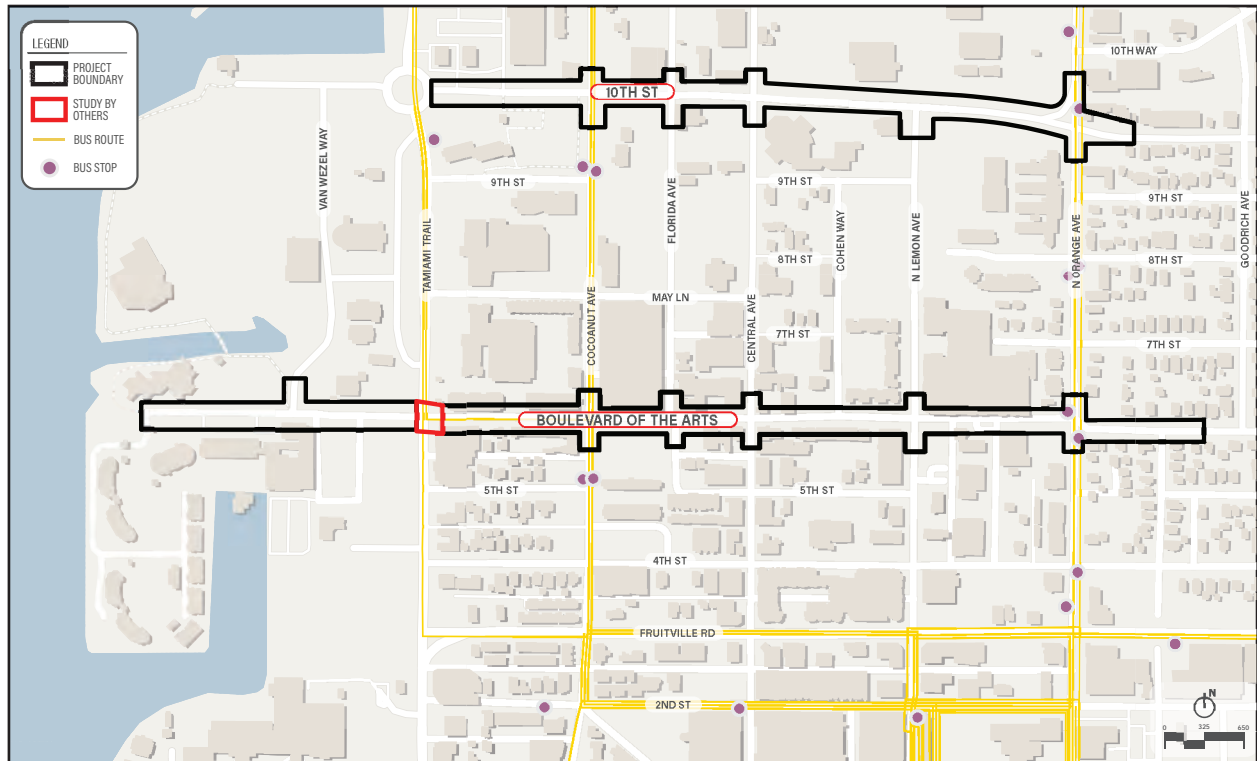


Figure 4-7: Existing Conditions - SCAT Routes and Stops

Project Area Widths

The existing approximate right-of-way width and curb-to-curb widths of the project area are shown in Figures 4-8 and 4-9. The right-of-way widths vary throughout the project area, ranging from 60 feet to 195 feet wide. Similarly, the curb-to-curb widths also vary throughout the project area, ranging from 20 feet to 70 feet wide. The proposed design will accommodate the varying width of BotA and 10th while maintaining user experience and safety particularly for non-motorized modes of travel.

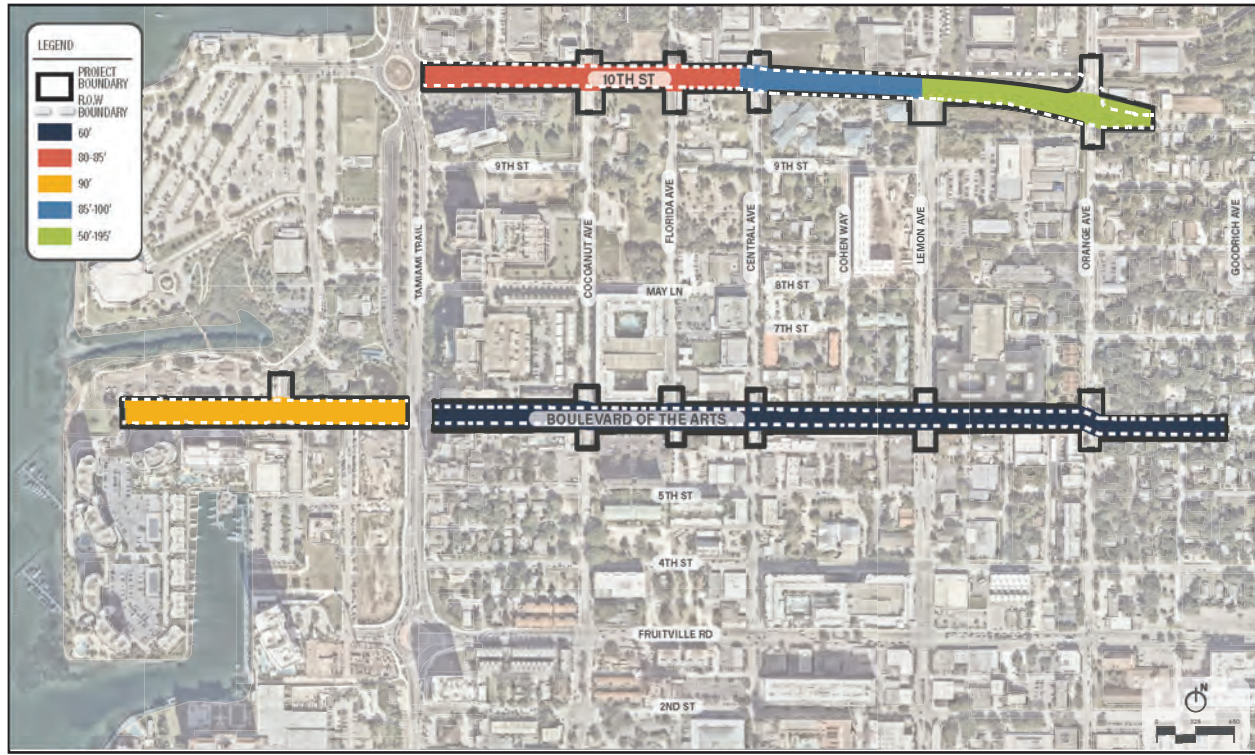


Figure 4-8: Existing Conditions - Right-of-Way Width

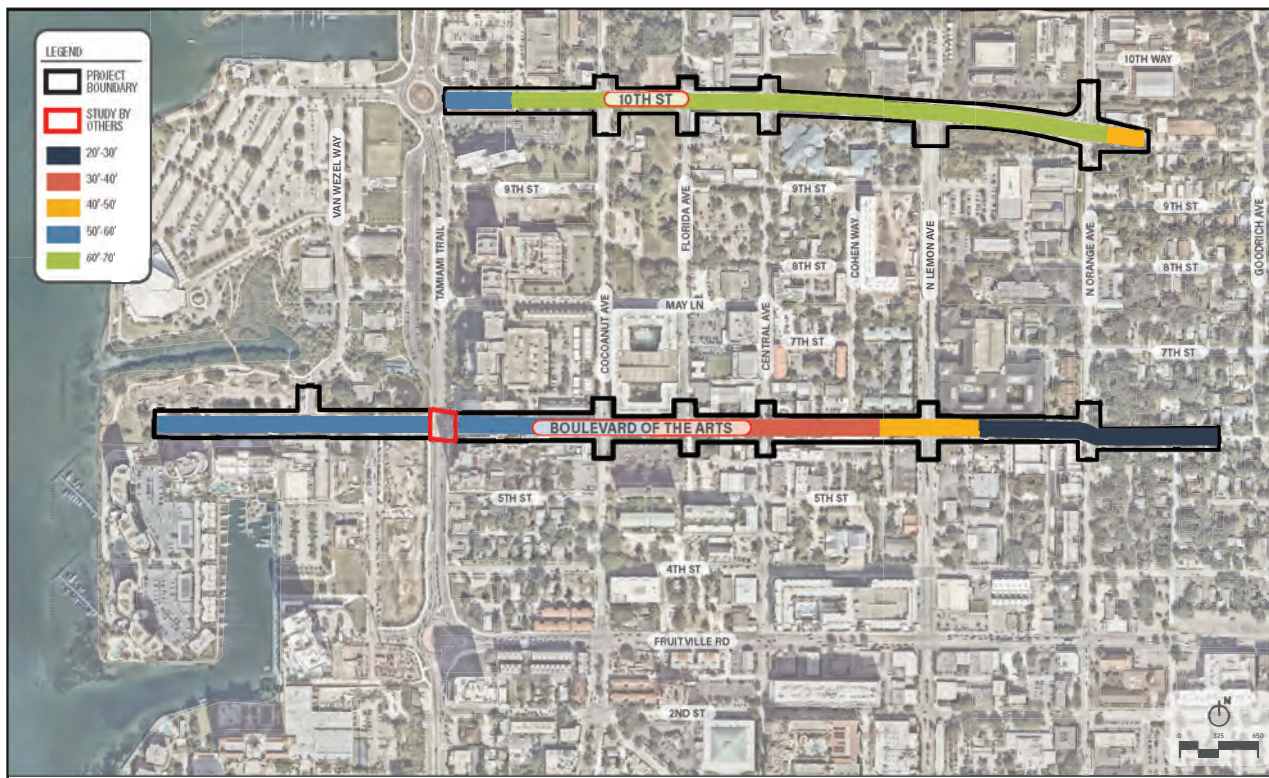


Figure 4-9: Existing Conditions - Curb-to-Curb Width

Conclusion

In summary, this inventory presented opportunities for this project to improve upon the existing conditions of the corridors. These two roadways have different forms and functions, as identified here, and the recommendations for improvements will meet the needs of the different users of these facilities both now and in the future. The proposed projects will provide: safe and comfortable East-West bicycle routes to and through Downtown and the Bayfront designed to be inclusive of all abilities, ages, and trips; sidewalk enhancements, opportunity for sidewalk café seating, increased number of shade trees; and improved context sensitivity and consistency between the roadway design and land uses.

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CHAPTER

5

CITIZEN INVOLVEMENT AND PUBLIC INPUT

Public involvement was a vital component of the Boulevard of the Arts (BotA) and 10th Street (10th) Basis of Design (BOD) development process. Input solicited from residents and other stakeholders informed the project team about the community's preferences and concerns and provided valuable feedback on the proposal itself.

Public comments were encouraged throughout the BOD process. Stakeholder input was solicited via several means, including meetings with neighborhood associations and partners, an online community survey, and City of Sarasota social media channels. To understand the community's current and future desires for BotA and 10th, an online survey was developed to gather input from the community during February 2022. Over 1,000 people participated in the online survey.

The City of Sarasota also hosted two open houses on Monday, May 23rd, 2022 at the SRQ Media Studio in the City Hall Annex, to reveal draft concepts. Attendees were given the opportunity to learn more and share feedback to further improve the conceptual designs. This chapter serves as recognition of the formal and informal input received from the public throughout the course of design development.

Public Survey

The online survey was publicly available February 1, 2022 through February 28, 2022 and consisted of 17 questions, each addressing different elements of the BotA and 10th Complete Streets redesign. The survey was advertised via City social media and communications channels, and partner newsletters like the February 22, 2022 edition of the Rosemary District Association newsletter, and The Bay. Over the course of the four weeks, 1,084 people participated in the online survey, generating over 18,400 data points and 2,223 comments. Participants were asked about their preferences for redesigning BotA and 10th, including what functions the roadways should serve and what amenities would best support placemaking and mobility. The participants were also asked why they visited each of the roadways, and how they currently access businesses or services on BotA and 10th. Data collected from the survey can be viewed in following figures, and the full survey results are included in [Appendix A](#).

BotA Survey Questions

Question 1 - What is your connection to Boulevard of the Arts?

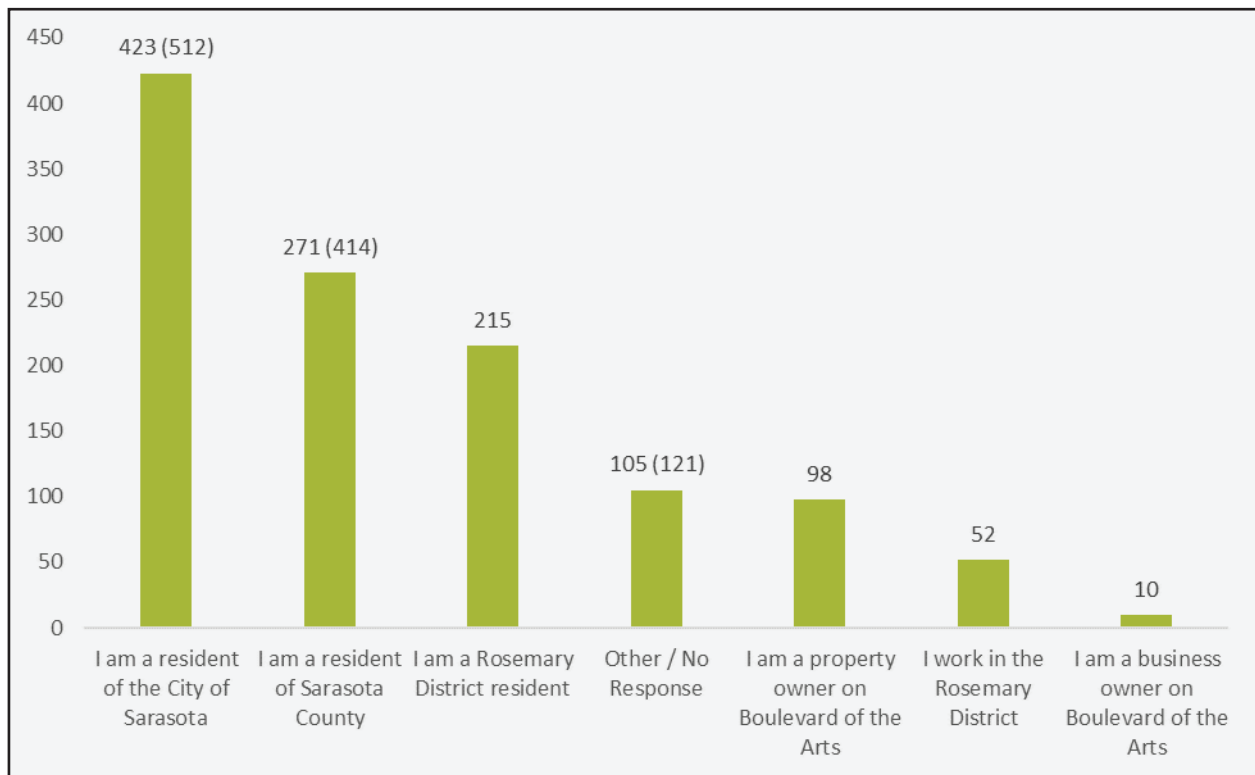


Figure 5-1: Survey Results for Question 1

Note: Multiple responses were available for selection. The raw responses were evaluated, and more precise “resident” answers that exclude repetitive responses are included in the table above, with the total responses in parentheses. Of the 1084 total respondents, 215 identified as residents of the Rosemary District. Given that the Rosemary District is within the City of Sarasota, and the City of Sarasota is within Sarasota County, several repetitious responses were excluded in favor of the most precise attribute. As such, a plurality of survey respondents identified as City of Sarasota residents outside of the Rosemary District, while nearly half as many were residents of Sarasota County outside of the City. Very few local business owners responded to the survey, and many of those respondents are residents.

Question 2 - Rank in order of priority what function you think a traditional 'main' street like Boulevard of the Arts (east of Tamiami Trail) should serve:

| Rank | Option | Score (max. 3) |
|------|---|----------------|
| 1. | Support local businesses by enhancing the public realm (i.e. providing 'spill over' areas for sidewalk cafes, curbside parking, street trees & benches) | 2.46 |
| 2. | Provide high quality public space and opportunities for gathering (i.e. festival street) | 1.84 |
| 3. | Provide efficient traffic flow through the Rosemary District to and from other destinations | 1.72 |

Figure 5-2: Survey Results for Question 2

Participants were asked to list their most important considerations from one to three; one being the most important and three being the least important. Support for local businesses by enhancing the public realm was selected by greater than half of the respondents (55%) as the top priority. Though the other two options are close in score, it is notable that the Efficient traffic flow option was considered over half of the respondents' least important concern.

Question 3 - What do you perceive to be the most important for consideration in redesigning Boulevard of the Arts (east of Tamiami Trail)? Rank in order of priority.

| Rank | Option | Score (max. 3) |
|------|--|----------------|
| 1. | Incorporating street trees, landscaping and green space | 3.73 |
| 2. | Designing flexible spaces for pedestrian activity | 3.27 |
| 3. | Increase safety and accessibility (Additional lighting, improved crossings, ADA compliance) | 3.08 |
| 4. | Adding opportunities for gathering spaces | 2.61 |
| 5. | Pursuing traffic calming methods (i.e. speed tables or raised intersections) | 2.38 |

Figure 5-3: Survey Results for Question 3

Participants were asked to list in priority order the above list of redesign considerations. Incorporating street trees, landscaping, and green space had significantly more support for top priority than the other considerations. Nearly 40% of responses considered it to be most important in the redesign of BotA. The least important consideration was pursuing Traffic calming methods. More than a third of total responses considered it to be the lowest priority in the redesign of the roadway. It should be noted that designing flexible spaces for pedestrian activity and Increase safety and accessibility received similar mid-priority support, but the former received the same number of lowest-priority votes as Incorporating street trees, landscaping, and green space (less than seven percent). It is clear that the top two priorities are highly desired by stakeholders.

Question 4 - Why do you visit Boulevard of the Arts (east of Tamiami Trail)?

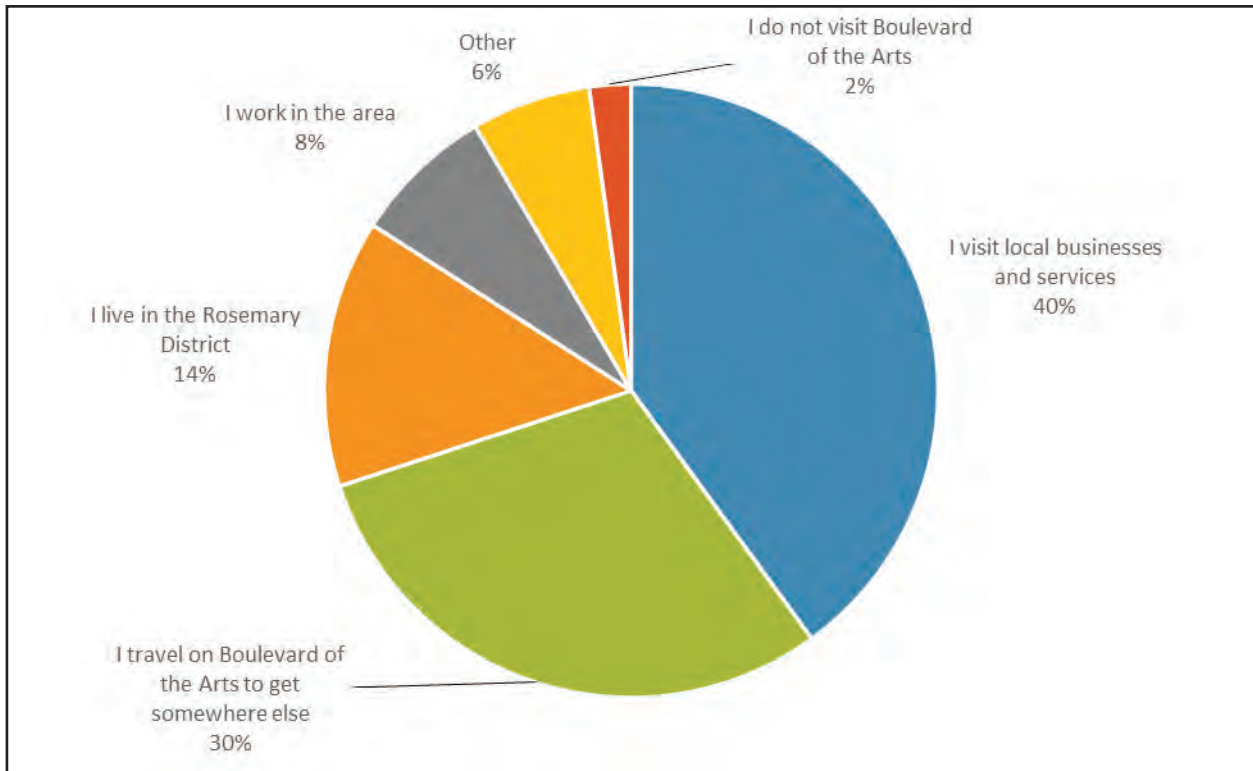


Figure 5-4: Survey Results for Question 4

Survey participants were asked about reasons for visiting BotA, allowing multiple options to be chosen. The top three responses comprised nearly 85% of total responses. These responses were “I visit local businesses and services”, “I travel on Boulevard of the Arts to get somewhere else”, and “I live in the Rosemary District”. This provides additional information in how BotA currently serves as both a destination and a throughfare for vehicular traffic. Other reasons include a variety of activities such as bike rides, accessing Van Wezel, and visiting the parks.

Question 5 - How do you access businesses or services on Boulevard of the Arts (east of Tamiami Trail)?

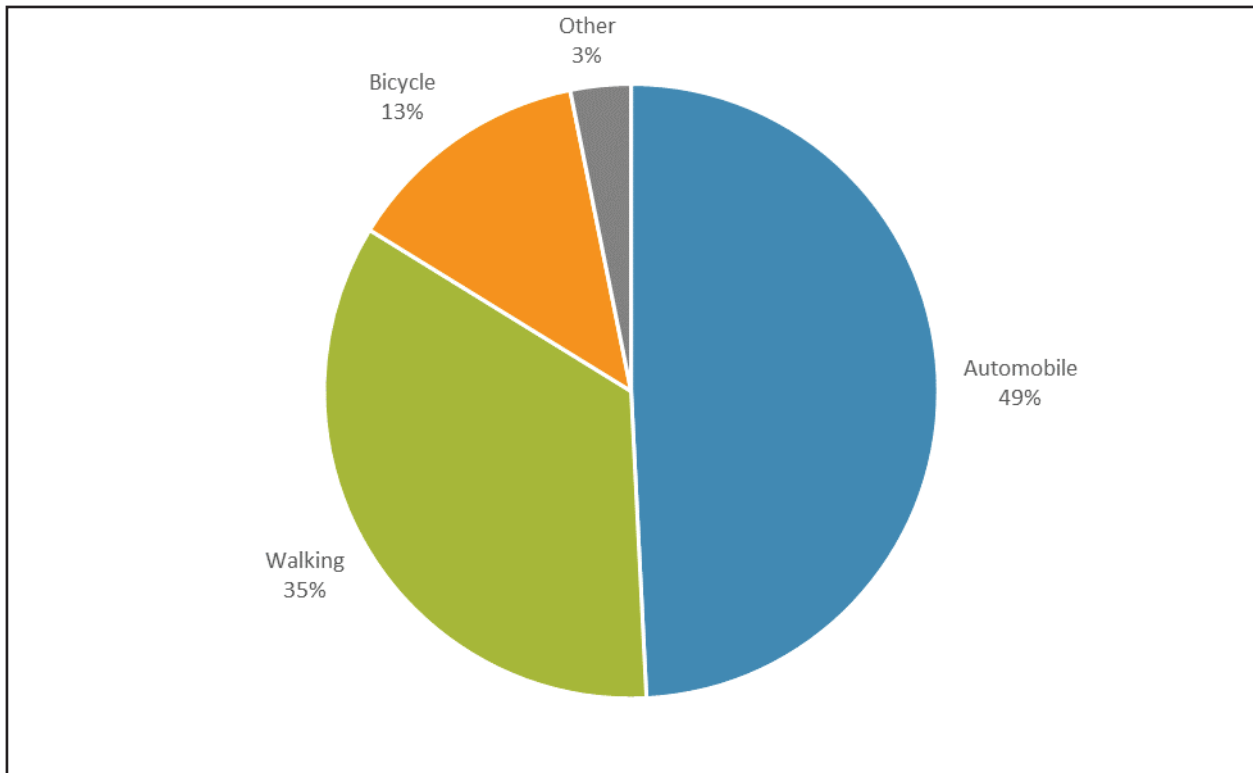


Figure 5-5: Survey Results for Question 5

Nearly half of responses (49%) selected Automobile as means of accessing Boulevard of the Arts attractions. Additional responses stated Walking (35%) and Bicycles (13%) as additional primary means of accessing the corridor. Other responses included micromobility (bikeshare, scooter, golf cart), public transportation and rideshare services (Uber/Lyft/taxi).

Question 7 - What types of activities or features would draw more people to Boulevard of the Arts (east of Tamiami Trail)?

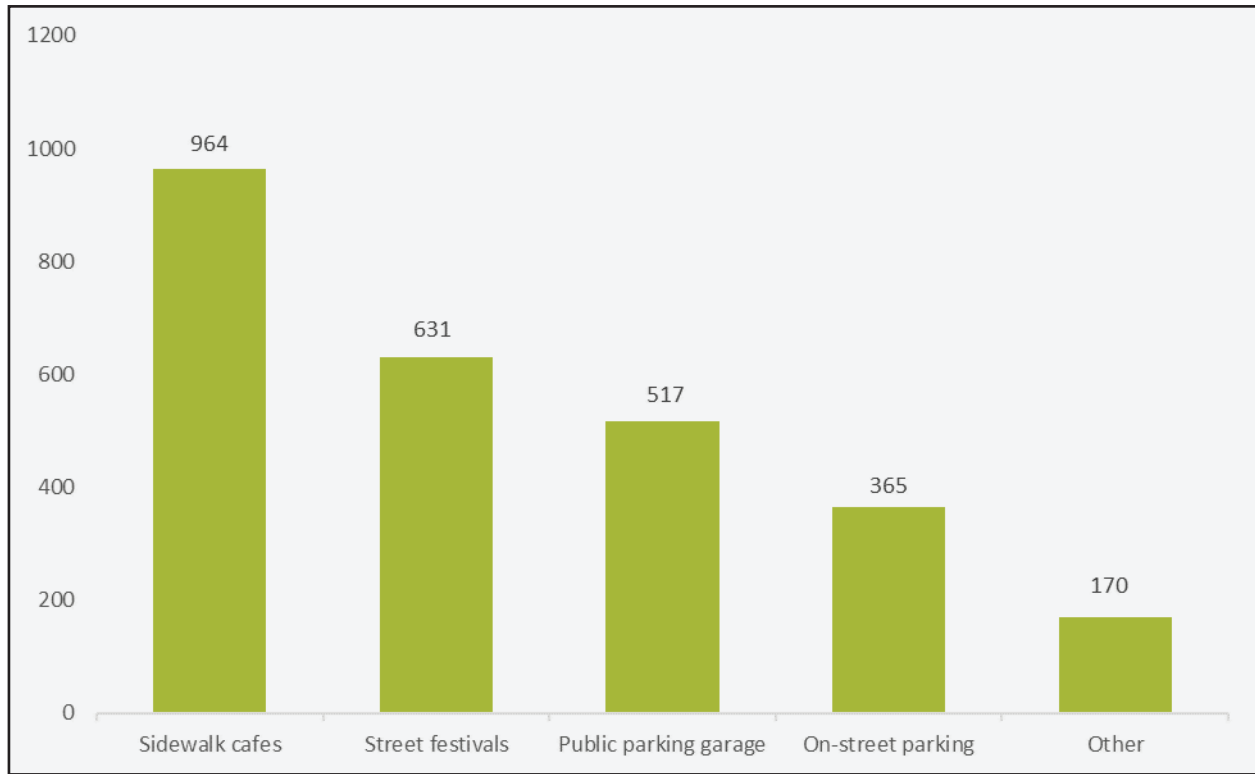


Figure 5-7: Survey Results for Question 7

Over 950 of the 1084 survey takers selected sidewalk cafes as a feature that would draw more visitors to BotA. Both Street festivals and Public parking garage were other features that around half of respondents selected. Selecting Other allowed respondents to input additional information. Of these free responses, several common suggestions included the following: parks/green space, pedestrian improvements, discouraging vehicles from using the area, public art.

Question 8 - What amenities would you like to see on Boulevard of the Arts (east of Tamiami Trail)? Pick your top three.

| Rank | Option | Percent of Respondents That Selected* |
|-------------|---|--|
| 1. | Trees | 80.1% |
| 2. | Flexible spaces (i.e. pocket parks, parklets, outdoor market spaces) | 69.5% |
| 3. | Pedestrian-focused lighting | 59.4% |
| 4. | Furniture (benches, garbage/recycling bins) | 46.1% |
| 5. | Bicycle racks | 28.3% |
| 6. | Gateway feature (a formal entrance to Boulevard of the Arts) | 23.8% |
| 7. | On-street parking | 23.6% |
| 8. | Other | 8.9% |

Figure 5-8: Survey Results for Question 8

The results of Question 8 reflected similar preferences for the selections made in Question 3. A significant number of the responses selected Trees as a top three preference. Flexible Spaces, Pedestrian-focused lighting, and Furniture (benches, garbage/recycling bins) also received a significant amount of support. As such, it can be concluded that improving the streetscape to promote pedestrian activity is a priority among the respondents.

Question 9 - What improvements would you like to see on Boulevard of the Arts (east of Tamiami Trail)?

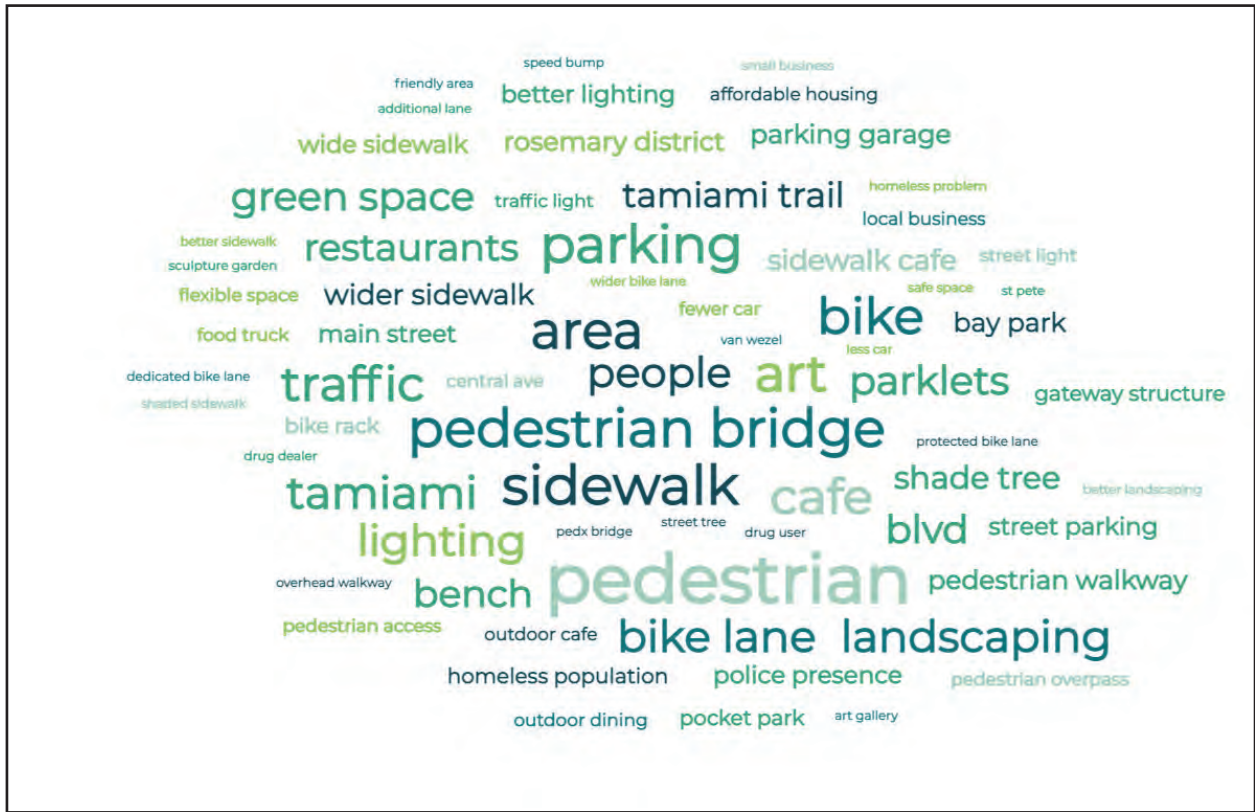


Figure 5-9: Survey Results for Question 9

Question 9 was a free response question, which prompted 667 responses. Some of the most common responses included the following: multimodal accommodation, public space, safety, traffic management and parking, place identity as an arts and cultural center, landscaping, trees, and greenspace, and economic development.

10th Survey Questions

Question 10 - What is your connection to 10th Street?

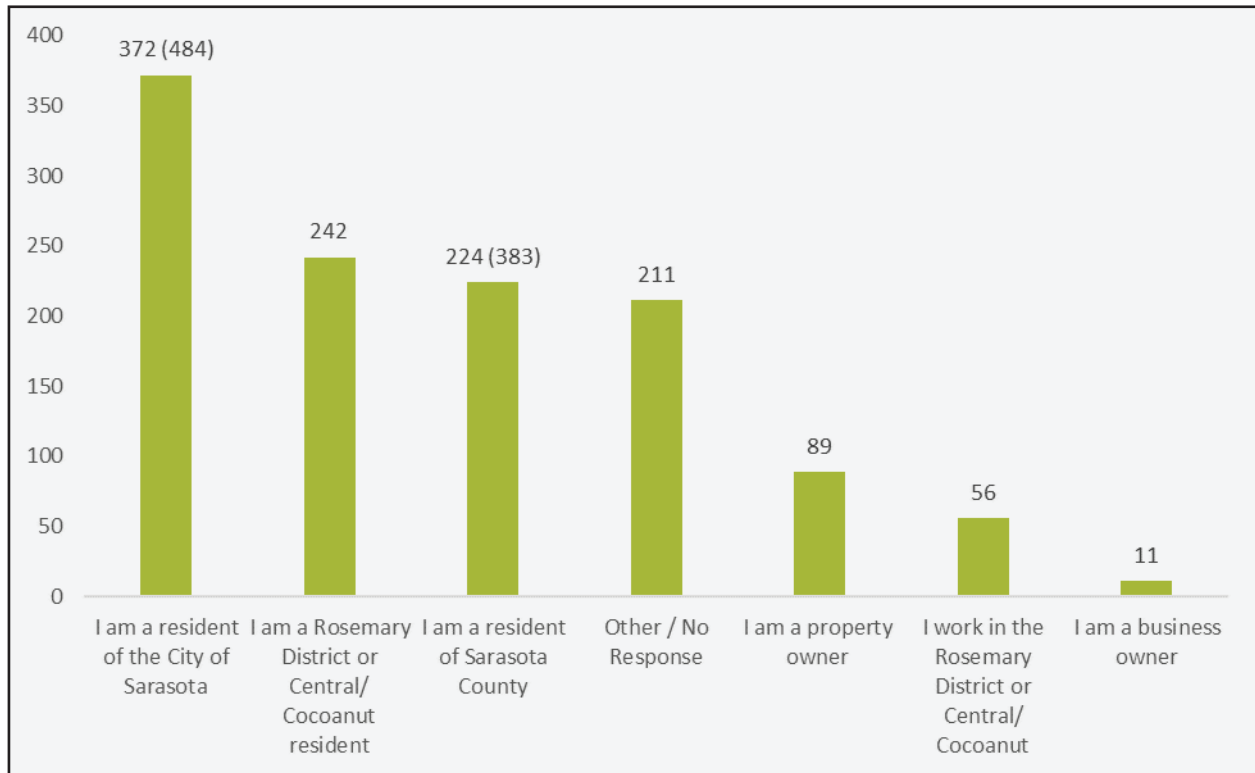


Figure 5-10: Survey Results for Question 10

Multiple responses were available for selection. The raw responses were evaluated, and more precise “resident” answers that exclude repetitive responses are included in the table above, with the total responses in parentheses. Of the 1084 total respondents, 242 identified as residents of the Rosemary District or Central/Cocoanut area. Given that these areas are within the City of Sarasota, and the City of Sarasota is within Sarasota County, several repetitious responses were excluded in favor of the most precise attribute. As such, a plurality of survey respondents identified as City of Sarasota residents outside of the Rosemary District, while nearly half as many were residents of Sarasota County outside of the City. Very few local business owners responded to the survey, and many of those respondents are residents.

Question 11 - Rank in order of priority what function you think a 'complete street' like 10th Street should serve:

| Rank | Option | Score (max. 4) |
|-------------|---|---------------------------|
| 1. | Support local businesses by enhancing the public realm (i.e. providing 'spill over' areas for sidewalk cafes, curbside parking, street trees & benches) | 3.22 |
| 2. | Enable efficient traffic flow to destinations | 2.81 |
| 3. | Provide high quality placemaking opportunities | 2.23 |
| 4. | Accommodate street festivals and events | 1.85 |

Figure 5-11: Survey Results for Question 11

Participants were asked to list their most important considerations from one to three; one being the most important and three being the least important. Support for local businesses by enhancing the public realm was selected by nearly half of the respondents (44%) as the top priority. It is notable that the Accommodate street festivals and events option was considered nearly half of the respondents' (48%) least important concern.

Question 12 - What do you perceive to be the most important for consideration in redesigning 10th Street? Rank in order of priority.

| Rank | Option | Score (max. 3) |
|-------------|--|---------------------------|
| 1. | Incorporating street trees, landscaping, and green space | 6.15 |
| 2. | A gateway to The Bay Park and Van Wezel | 5.51 |
| 3. | Designing flexible spaces for pedestrian activity | 5.25 |
| 4. | Increase safety and accessibility (Additional lighting, improved crossings, ADA compliance) | 5.17 |
| 5. | Protected bicycle lanes/ infrastructure | 5.08 |
| 6. | Pursuing traffic calming methods (i.e. speed tables or raised intersections) | 4.37 |
| 7. | Stormwater treatment/ water quality enhancements | 3.93 |

Figure 5-12: Survey Results for Question 12

Participants were asked to list in priority order the above list of redesign considerations. Incorporating street trees, landscaping, and green space, and a gateway to the The Bay Park and Van Wezel had very similar support for top priority than did the other considerations. Around 30% of responses considered each option to be most important in the redesign of 10th. The least important consideration was Stormwater treatment/ water quality enhancements.

Question 13 - Why do you visit 10th Street?

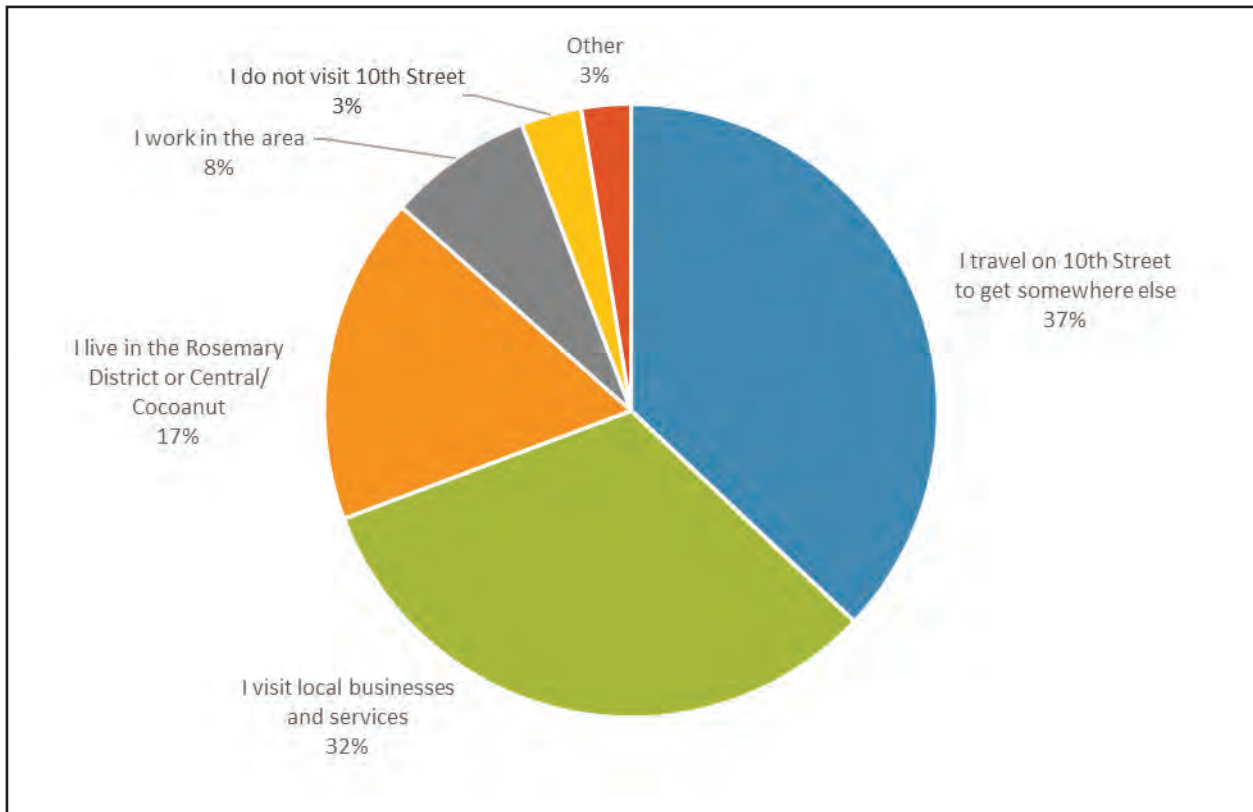


Figure 5-13: Survey Results for Question 13

Survey participants were asked about reasons for visiting 10th, allowing multiple options to be chosen. The top three responses comprised greater than 85% of total responses. These responses were “I travel on 10th Street to get somewhere else”, “I visit local businesses and services”, and “I live in the Rosemary District or Central/Cocoanut”. This demonstrates that 10th currently serves largely as a throughfare for vehicular traffic and to a slightly lesser extent, a destination.

Question 14 - How do you access businesses or services on 10th Street?

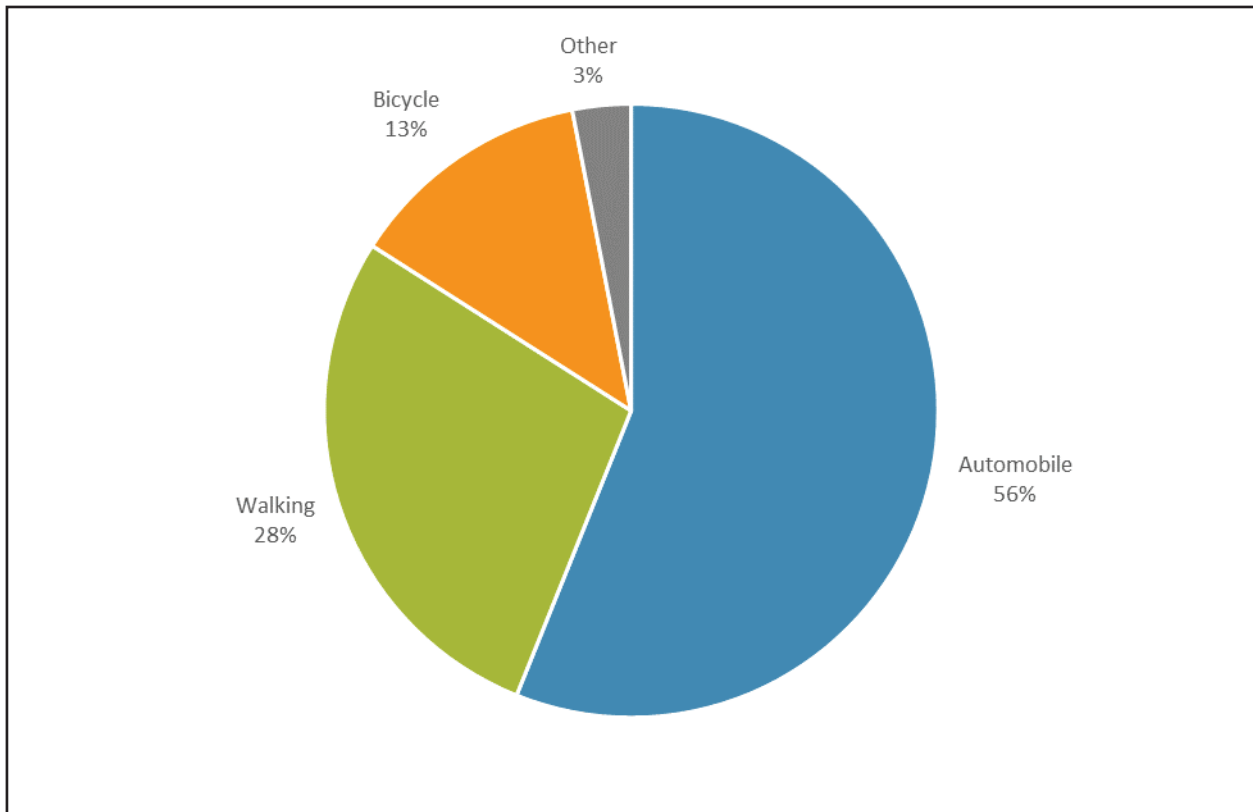


Figure 5-14: Survey Results for Question 14

Nearly half of responses (56%) selected Automobile as means of accessing 10th businesses and services. Additional responses stated Walking (28%) and Bicycles (13%) as additional primary means of accessing the corridor. Other responses included micromobility (bikeshare, scooter, golf cart), public transportation and rideshare services (Uber/Lyft/taxi).

Question 16 - What amenities would you like to see on 10th Street?

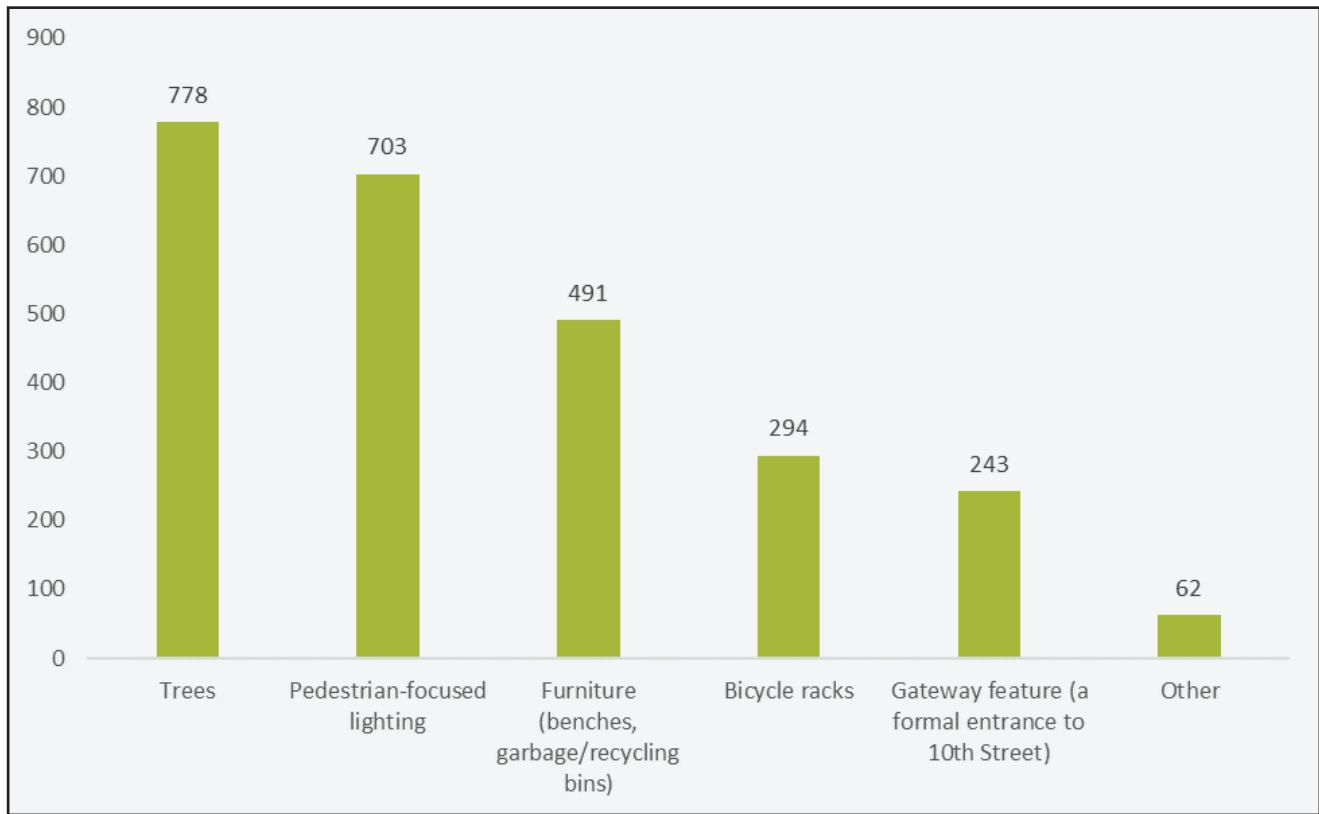


Figure 5-16: Survey Results for Question 16

Question 16 asked respondents to select what they would consider their top three preferences. Over 750 of the 897 respondents to Question 16 selected trees as a feature that they would like to see more of on 10th. Pedestrian-focused lighting and furniture also received significant levels of support from those responding. The other options were selected by fewer than a third of those responding.

As seen from the above data, responses vary somewhat from the related Question 12. Whereas landscaping and trees remains a consistent high priority, a 10th Gateway feature ranks much lower here than was communicated by Question 12.

Question 17 - What improvements would you like to see on 10th Street?



Figure 5-17: Survey Results for Question 17

Question 17 was a free response question, which prompted 375 responses as can be read in Appendix A. Some of the most common concerns include the following: multimodal accommodation, safety and lighting, trees, landscaping, and green space, economic development, traffic management and parking, homelessness, and public art.

Summary of Survey Results

Through the public involvement process, the City and project team were able to gain valuable insight into the needs and desires of the community and their preferences regarding the future of these corridors.

BotA - overall, survey respondents addressing BotA felt strongly that incorporating street trees, landscaping and green space was most important, as was designing flexible spaces for pedestrian activity, and increasing safety in the BotA corridor. Sidewalk café accommodations was the most common response for drawing more visitors to the BotA corridor, though there were also responses that expressed concern about sidewalks being too narrow to support additional activity.

Many responses addressed the desire to enhance the public realm by providing areas for sidewalk cafes, curbside parking, street trees, and benches. Participants also suggested incorporating street trees, landscaping, and greenspace as major priorities for the redesign of BotA. As previously stated, nearly 90 percent of respondents support sidewalk cafes as a feature that would draw more people to the BotA. Street festivals received a significant amount of support as well. It was also clear that providing streetscape to improve pedestrian access assumes a high priority in the redesign of BotA.

10th - responses addressing 10th varied somewhat from BotA responses. Prioritizing “Street trees, landscaping, and green space” was reflected in a way that remained a high priority for BotA. However, more 10th responses expressed the need for balancing support for local business and economic development with addressing traffic flow to key destinations. Providing a “gateway to The Bay Park” was a common sentiment unique to the 10th responses. Adding accommodations for multimodal trips and enhancing area safety each scored highly as priorities.

The variability in some of the responses regarding 10th is reflective of how visitors access and experience the corridor. Despite this variety in preferences, an overwhelming majority of respondents selected trees as an amenity of which they would like to see more on 10th. There was also strong support for pedestrian-focused lighting and public space furniture as amenities that would enhance the corridor.

Continued Public Outreach

Business Outreach

The online survey results informed the project team how people used the streets and what improvements they wanted to see along the parallel corridors. More importantly, it exposed how the business community on both Blvd of the Arts and 10th Street were underrepresented. On May 11 and May 12, 2022, City Staff walked both corridors sharing information with business owners and employees about the Complete Street project prior to the upcoming meetings. During this outreach, City Staff visited over 29 businesses, including retail, restaurants, theatres, studios, and organizations alike.

Open House Public Meetings

The City of Sarasota hosted two open houses on Monday, May 23rd, 2022 at the SRQ Media Studio in the City Hall Annex to reveal draft concepts for the construction of complete streets on BotA and 10th. The City of Sarasota advertised the Open Houses to the public via a Press Release that was posted on May 13th, 2022. Additionally, local news outlets and digital media reflected the posting of the press release. Over 85 people attended the public meetings. Attendees were given the opportunity to learn more about the designs and share feedback to further improve the conceptual designs.

At the events, attendees were asked to mark on a bulletin board their preferred hardscape and landscape treatments to gauge the interest of certain design features. See Figure 5-18 on page 37. Attendees were more supportive of pavers, landscape beds, and street trees. While they were interested in decorative concrete, it was not as high of a priority in the attendees’ preferences.



Figure 5-18: Hardscape and Landscape Bulletin Dot Board



Figure 5-19: Site Furnishings Bulletin Dot Board

Attendees were also asked to mark on a bulletin board their preferred site furnishes. Attendees were more supportive of unique seating, bike racks, and benches. While they were interested in bollards, it was not as high of a priority in the attendee's preferences. See Figure 5-19 on page 37.

Attendees were also given the opportunity to leave comment cards providing additional feedback. Of the 25 comment cards provided, there were concerns over the reduction of 10th from four lanes to two lanes and the desire for canopy trees and greenery along the corridor as well as at the intersection of 10th Street and Orange Ave. Additionally, a concern was expressed regarding pedestrian safety at the proposed roundabout and how it would affect the flow of traffic during school pick-up and drop-off hours.

A copy of the City of Sarasota's press release can be found in [Appendix A](#).

Public Comments on Draft Concepts

Between May 25th and June 3rd, 2022, the City of Sarasota provided an online form for the public to submit their feedback regarding the Complete Streets conceptual designs revealed on May 23rd, 2022. During those two weeks, 17 responses were received. The first response expressed a desire for a more reliable and expansive bus transit system to better serve the growing population of the neighborhood with the rest of Sarasota County. The second response reflected a concern over pedestrian safety in the conceptual designs of BotA and 10th. The respondent expressed an interest in adding an intersection neckdown to discourage vehicular traffic and make pedestrian crossings shorter as well as the inclusion of traditional radial sidewalk that can discourage vehicles to turn more quickly. All of the comments can be seen in [Appendix A](#).

Conclusion

After listening to the residents during this public outreach effort, it is clear that stakeholders in both corridor areas believe that providing improvements to multimodal access and safety should be balanced with safe and efficient vehicle travel. There is also significant desire to see enhanced safety throughout the neighborhood as well as additional streetscaping and landscaping to enhance pedestrian activity.

Citizen Involvement, local organizations, neighborhood groups, and citizen input played a vital role in informing the concept designs, discussed in Chapter 6, and will continue to drive project decision-making moving forward.

CHAPTER

6

CONCEPT PLANS

The concept plans depicted in Figure 6-1 through 6-4 are the result of findings from the existing conditions analysis, programming studies, public input, and guidance from City staff. The concept plans are consistent with best practices published by the National Associations of City Transportation Officials (NACTO) Urban Bikeway Design Guide and Urban Street Design Guide for pedestrian and bicycle facilities.

Considerations were taken during the design process for the safety of all users, the navigation of roundabouts by bicyclists, any necessary intersection improvements within the study areas, and the maintenance of the proposed improvements. These considerations and the designs of the segments are further outlined below.

Safety:

Signing and Pavement Markings – Proper signing and markings for vehicles, bicyclists, and pedestrians should be utilized in order to minimize conflict and ensure proper usage of the facilities. In addition to the pavement markings shown on the concept plan in Figure 6-1, a complete signing and marking plans package should be developed as part of the construction documents process. Signing and pavement markings should be designed in accordance with the Manual on Uniform Traffic Control Devices standards.

Driveways – Driveway connections onto BotA and 10th introduce unsignalized conflict points between turning motorists and with bicyclist using the corridor. Conflict zone striping should be used to alert cyclists and motorists of potential conflict. The buffer zone should be kept free of parking, large vegetation, or other objects that may obstruct the view of drivers for 20’ on either side of the driveway location.

Roundabouts:

Navigating a Roundabout by Bike – Users of the separated bike lane will encounter two options for navigation of roundabouts at Orange Avenue. In accordance with current Federal Highway Administration guidelines, the protected bike lane will be terminated prior to entry into the roundabout, allowing bicyclists to navigate the roundabout either with vehicular traffic or as a pedestrian. Experienced bicyclists who chose to navigate the roundabout like a vehicle will use the dashed transition zone to merge with vehicular traffic before navigating the roundabout as any vehicle and observing the normal flow of traffic. Bicyclists who chose to navigate the roundabout like a pedestrian will utilize the bike ramps provided near the terminus of the bike lane to exit the roadway onto the sidewalk where they will navigate the roundabout like a pedestrian, re-entering the bike lane via the bike ramp opposite the roundabout in their chosen direction of travel. Due to the high number of pedestrians and insufficient sidewalk widths at the existing roundabouts it is recommended that bicyclists be required to walk their bicycles when using the sidewalk.

Intersection Improvements:

Orange Avenue Roundabout - The roundabout at 10th and Orange will provide a continuous flow of traffic while introducing a traffic calming feature. The roundabout geometry elevates driver awareness through roadway geometry, introducing curvature prior to and upon entry of the circulating roadway, provides opportunity for landscape treatments in the center island and in areas between curb and sidewalk. Landscaping buffers and center island treatments break up driver sight lines further focusing motorist’s on traffic patterns within the intersection, specifically left of entry points. This further reduces speed and enhances safety. Pedestrian accommodations through the intersection include wide cross walks and sidewalks (for mixed use) and median refuges which enhance safety by allowing pedestrians to navigate only a single direction of travel at a time.

10th and Lemon - Due to the proposed reduction in lanes on 10th, the intersection of 10th and Lemon has been redesigned. The proposed redesign removes one of the northbound left turn lanes on Lemon. This would allow for additional road verge on the east side of N Lemon Ave. A road verge is a strip of grass or plants between the roadway and the sidewalk. In addition to the increase in road verge, the eastern crosswalk at the intersection will directly connect to the sidewalk; instead of connecting in front of the right turn lane.

Boulevard of the Arts (BotA) and Tamimi Trail - Intersection Control Evaluation Summary (Study done by ADEASQ)

The US 41 multimodal roundabout corridor in Downtown Sarasota consists of mixed-use redevelopment with significant density increases compared to 20 years ago. A phase of the Quay redevelopment on the southwest corner is under construction, and The Bay Park is undergoing significant redevelopment on the northwest corner. Hotel Indigo is located on the northeast corner and The Blvd mixed use condominium tower recently under construction is located on the southeast corner.

Various forms of traffic control at this intersection were evaluated. This intersection is one of the last remaining signalized intersections within the US 41 Multimodal Roundabout Corridor. Currently, the intersection is signalized with BotA having minimal influence constraining capacity along US 41. However, these minor street volumes are anticipated to increase with the construction of The Bay and The Quay redevelopments.

Initially, a roundabout was conceived and evaluated in coordination with the Florida Department of Transportation (FDOT). However, right-of-way limitations at the adjacent corners, particularly the Southwest corner, inhibit the ability to install a roundabout with preferred radii and approach angles. The Northwest corner is owned by the City where an additional easement could be accommodated. The conceptual roundabout results in off-tangent approach/deflection angles. Four different roundabout alignments were evaluated. Unless additional right-of-way can be obtained, a roundabout here is not feasible.

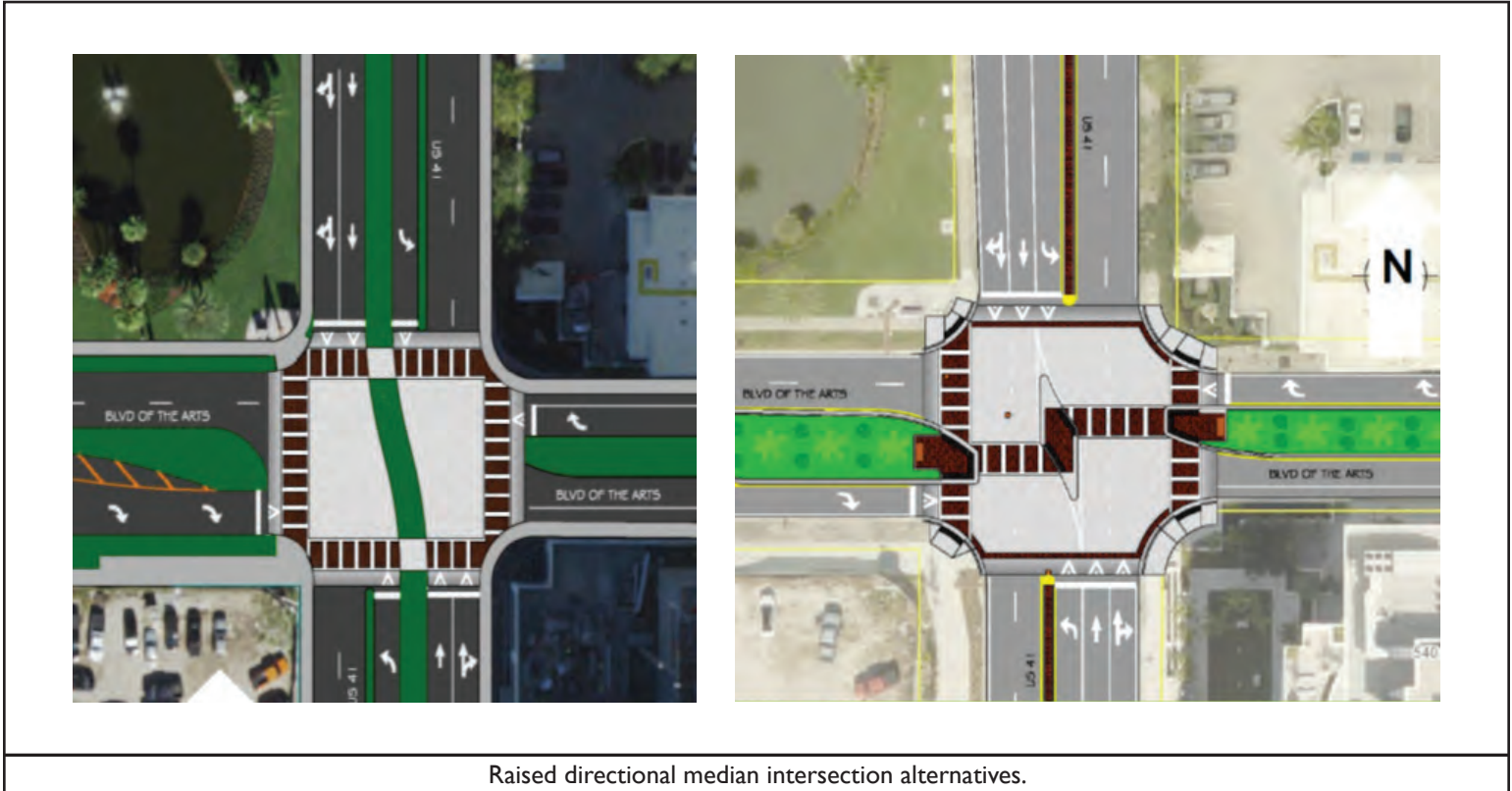
Even though a roundabout was not a feasible alternative given right-of-way constraints in the Southwest quadrant, the City pursued a project to refresh the intersection's aesthetic and improve operations.



One of the four different roundabout alternatives evaluated at this intersection. To make this a feasible alternative, additional right-of-way access is needed.

A second alternative considered involved a “directional” median opening at Blvd of the Arts, allowing only lefts-in, rights-in and rights-out. This alternative consisted of limiting access to left-turns only from US 41 onto BotA. Left turns

from BotA would not be accessible. This option would increase throughput on US 41, but would increase congestion at the adjacent intersections of 10th Street and at Fruitville Road. To improve multimodal access and improve speed control, this option considered raising a speed table across the intersection, and also pedestrian signalization to facilitate access across US 41. Either option would allow for a pedestrian refuge area in the center median and thus a two-stage pedestrian crossing on US 41. However, at the public meetings this option was resisted due to the removal of left-turn from BotA.



Raised directional median intersection alternatives.

Therefore, a third alternative was evaluated to raise the intersection. This alternative would keep the existing signalized traffic control. However, a decorative speed table would go across the intersection to maintain speed control through the intersection for all vehicles. While not the preferred form of traffic control along this corridor, keeping the intersection signalized preserves full pedestrian access and vehicle turning movements. The design speed for this option would be 25 mph, which would represent a significant departure from the current posted speed of 35 mph. Such a departure would not be advisable unless a 25 mph speed control could also be established at other points along US 41. Additional traffic calming measures, such as raised midblock pedestrian crossings to the north and to the south, would be needed so this alternative does not pose a safety hazard to drivers approaching the raised intersection at 35 mph. Currently, this is the alternative being considered for the intersection.

Maintenance:

Maintenance and Street Cleaning – In order to maintain functionality and ensure user safety it is important that the bike lanes be kept clear of waste and debris. The City should include regular maintenance of the corridor as a part of normal downtown street cleaning and maintenance. In design, placement of long-term permanent buffers should accommodate the 8.5' minimum clearance of the City's current street sweeper.

Segment 1

Segment 1 is located on BotA west of Tamiami Trail. It is characterized by the future road access and bus parking for the Bay Park, landscaped center medians, and curbside management zones. Additional pedestrian improvements are proposed, including raised crosswalks with enhanced hardscape and preserving large shade trees and adding additional landscape where possible – especially in road verges. The center medians provide an opportunity for two art installations, especially when paired with pedestrian refuges. Additional pedestrian-oriented improvements recommended include widening the sidewalk on the north side of BotA west of Van Wezel Way and creating bulb-outs at intersections. The bulb-outs would shorten the crossing distances at intersections and paired with pedestrian refuge areas in the landscaped median provide both traffic calming and enhanced pedestrian safety. The detailed concept plan for Segment 1 can be found in [Appendix B](#).

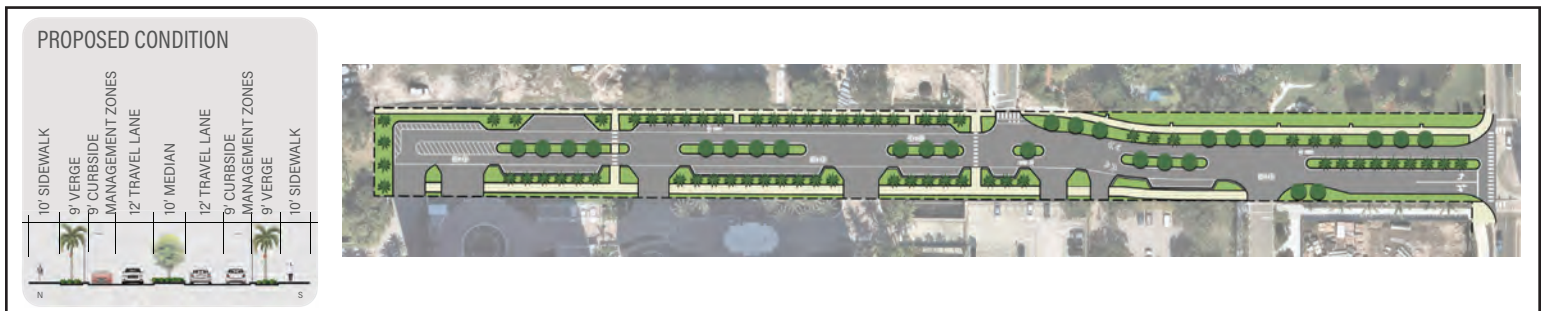


Figure 6-1: Segment 1 Concept Plan

Segment 2

Segment 2 is located on BotA east of Tamiami Trail to Orange Avenue. It is characterized by a mix of businesses and land uses along the corridor, wide sidewalks with outdoor seating space, and on street parking to support businesses and calm traffic. Similar to Segment 1, raised intersections and different paving materials are proposed for pedestrian safety improvements. Additional pedestrian-oriented recommendations include utilizing wide sidewalks for restaurant café seating and introducing more decorative elements like decorative sidewalks, decorative tree grates, site furnishings, and raised planter opportunities. Preserving existing shade trees and landscaping and adding additional landscape beds, shade trees, and specimen trees at gateways and intersections are also proposed. These pedestrian oriented improvements paired with on street parallel parking and verge landscaping will create a safe and friendly pedestrian experience in the corridor. The detailed concept plan for Segment 2 can be found in [Appendix B](#).

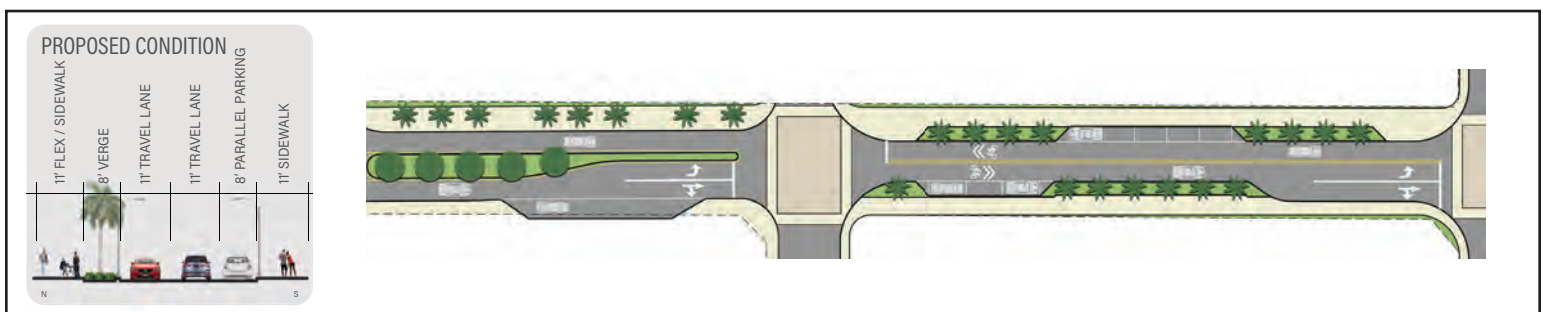


Figure 6-2: Segment 2 Concept Plan

Segments 3 and 4

Segments 3 and 4 are located on 10th from Tamiami Trail to Lemon Avenue. They are characterized by dedicated left turn lanes, the introduction of dedicated bike lanes, and stormwater treatment opportunities. To allow for buffered and protected bike lanes on this higher speed facility, 10th will need to be reduced to one, 11-foot travel lane with a dedicated left turn lane traveling in either direction. This will allow for increased verge landscaping and a wider sidewalk. This two-lane cross-section is consistent with the cross-section on the segment of 10th to the east of this project between Orange Ave and US 301. Due to the high travel speed of vehicles on 10th, raised delineators, or armadillos, are proposed to provide protected spaces for cycling. Armadillos are small, oblong bumps that are used instead of bollards, or similar, to remind motorists to stay out of the bike lane. In case of an emergency, ambulances, fire trucks, and police vehicles can all safely drive over the armadillo. The detailed concept plan for Segments 3 and 4 can be found in [Appendix B](#).

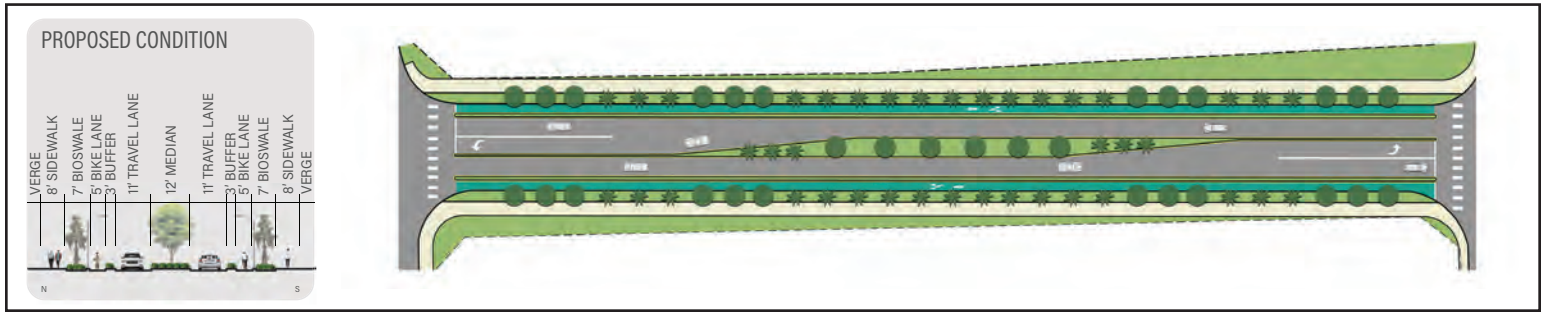


Figure 6-3: Segments 3 and 4 Concept Plan

Segment 5

Segment 5 is located on 10th from Lemon Avenue to Orange Avenue. It is characterized by the proposal of a roundabout at the intersection of 10th and Orange Avenue, protected bike lanes, and naturalized stormwater treatment opportunities. The roundabout will serve as a gateway to both the Gillespie Park neighborhood to the east and 10th to the west providing an opportunity for public art or other placemaking structures. To support the introduction of the protected bicycle lanes and wider pedestrian sidewalks, 10th will be narrowed to one 11-foot vehicular travel lane in each direction. The plan also proposes enhanced hardscape at pedestrian crosswalks and intersections to provide for safer crossings. The detailed concept plan for Segment 5 can be found in [Appendix B](#).

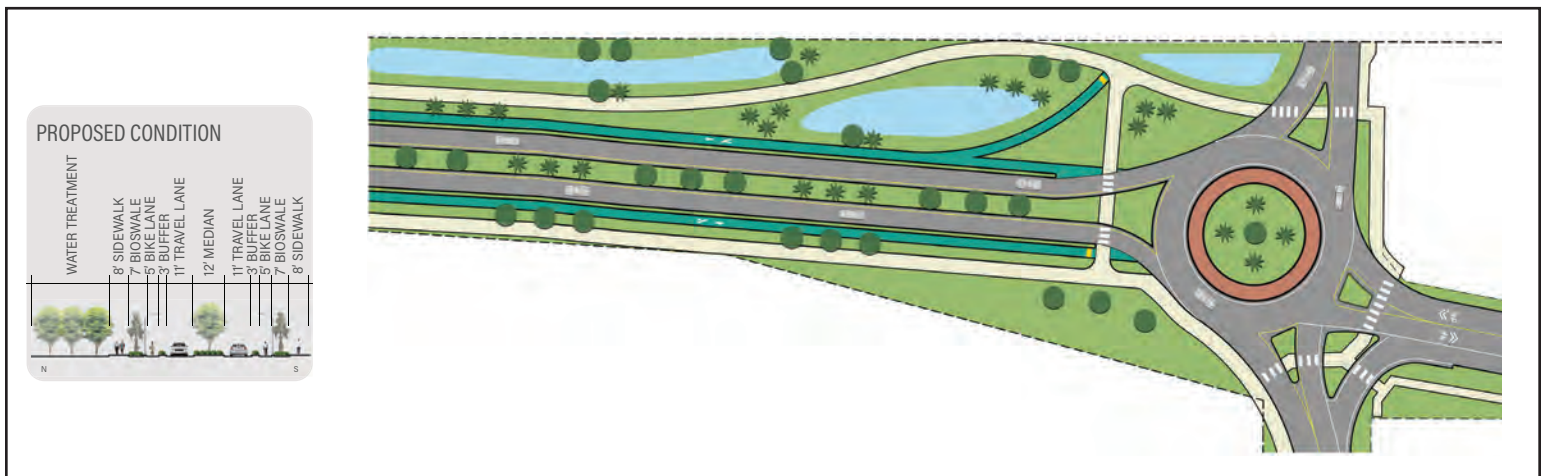


Figure 6-4: Segment 5 Concept Plan

CHAPTER

7

LONG RANGE ESTIMATE

Based on the 30% Concept Plans developed in [Chapter 6](#), a long-range estimate of the proposed design was developed. The Engineers Estimate of Probable Cost, shown in Figure 7-1, reflects a high level estimate of the corridor design as outlined in Chapter 6 and [Appendix B](#). This estimate includes all construction related costs, including roadway micro surfacing, striping, landscaping, and contractor fees. The cost estimates for both corridors were combined due to their proximity as well as their importance to the community, as described in Sarasota in Motion. Combining the corridors also created cost saving opportunities in materials cost and contractor fees. Partial funding for this project is being sought through federal grant programs leveraging city funds as local contribution requirements.

Cost estimates for each item were developed using the Florida Department of Transportation Basis of Estimates and modified using the most recent bidding information available. A 25% contingency has been included and costs have been modified to reflect the most recent inflation projections for construction and materials costs. Additionally, the Engineer has no control over the cost of labor, materials, equipment, or over the Contractor’s methods of determining prices or over competitive bidding or market conditions. Opinions of probable costs are based on the information known to the Engineer at this time, such as the FDOT Historical Costs online archive. It represents the Engineer’s judgment as a design professional familiar with the construction industry. The Engineer cannot and does not guarantee that proposals, bids, or actual construction costs will not vary from its opinions of probable costs in the future.

Alternates

The long-range estimates have been separated into three design alternates. These were separated due to the rising prices of varying materials, as previously mentioned. These alternatives provide City leadership with different opportunities for constructing these projects based on available funding. These probable costs assume different materials used for the hardscaping opportunities identified in the Concept Plans. The details and design for the hardscaping opportunities and landscaping will be identified in future engineered documents.

The cost estimates for all alternates assume the following:

- Mill and resurfacing
- Protected bicycle facilities
- Intersection reconstruction at 10th and Orange
- Decorative light poles
- Modifying existing drainage structures
- Undergrounding utilities, cost is based on recent bids for other projects
- Cost for undergrounding utilities is based on recent bids for other projects.
- Cost increases for small quantities of materials.
- The final surface of painted pavement markings cost is 75% of the thermoplastic final surface cost.

| ALTERNATE 1 Concrete and Asphalt |
|---|
| <ul style="list-style-type: none"> • Cost: \$10,445,598 • Includes: <ul style="list-style-type: none"> ▪ Assumptions listed above ▪ Concrete or asphalt for all sidewalks, crosswalks, and intersections |

| ALTERNATE 2 Decorative Concrete |
|---|
| <ul style="list-style-type: none"> • Cost: \$12,947,426 • Includes: <ul style="list-style-type: none"> ▪ Assumptions listed above ▪ Decorative Concrete for all sidewalks, crosswalks, and intersections |

| ALTERNATE 3 Concrete Pavers |
|---|
| <ul style="list-style-type: none"> • Cost: \$13,344,676 • Includes: <ul style="list-style-type: none"> ▪ Assumptions listed above ▪ Concrete Pavers for all sidewalks, crosswalks, and intersections |



| BoTA and 10th Concept Plan - Engineers Opinion of Probable Costs | | | | | |
|--|---|------|--------|-----------------|----------------------|
| ITEM NO. | DESCRIPTION | UNIT | QTY | UNIT PRICE | AMOUNT |
| Roadway | | | | | |
| 102 1 | MOBILIZATION (10%) | LS | 1 | \$ 87,628.00 | \$ 87,628.00 |
| 102 2 | MAINTENANCE OF TRAFFIC (15%) | LS | 1 | \$ 131,442.00 | \$ 131,442.00 |
| 104 10 3 | SEDIMENT BARRIER | LF | 5179 | \$ 2.50 | \$ 12,947.50 |
| 104 18 | INLET PROTECTION SYSTEM | EA | 100 | \$ 150.00 | \$ 15,000.00 |
| 107 1 | LITTER REMOVAL | AC | 60.00 | \$ 100.00 | \$ 6,000.00 |
| 107 2 | MOWING | AC | 16.56 | \$ 100.00 | \$ 1,656.00 |
| 110 1 1 | CLEARING & GRUBBING | AC | 14.31 | \$ 35,000.00 | \$ 500,850.00 |
| 110 7 1 | MAILBOX, F&I SINGLE | EA | 9 | \$ 350.00 | \$ 3,150.00 |
| 110 4 10 | REMOVAL OF EXISTING CONCRETE | SY | 15669 | \$ 35.00 | \$ 548,415.50 |
| 120 1 | REGULAR EXCAVATION | CY | 5000 | \$ 20.00 | \$ 100,000.00 |
| 120 6 | EMBANKMENT | CY | 5500 | \$ 30.00 | \$ 165,000.00 |
| 160 4 | TYPE B STABILIZATION | SY | 5396 | \$ 15.00 | \$ 80,941.00 |
| 285 706 | OPTIONAL BASE, BASE GROUP 06 | SY | 4867 | \$ 40.00 | \$ 194,668.00 |
| 327 70 6 | MILLING EXISTING ASPHALT PAVEMENT, 1 1/2" AVG DEPTH | SY | 27344 | \$ 3.00 | \$ 82,032.90 |
| 334 1 53 | SUPERPAVE ASPHALTIC CONCRETE, TRAFFIC C, PG 76-22 | TN | 401.4 | \$ 165.00 | \$ 66,227.70 |
| 337 7 83 | ASPHALT CONCRETE FRICTION COURSE, TRAFFIC C, FC-12.5, PG 76-22 | TN | 2655.3 | \$ 180.00 | \$ 477,950.40 |
| 425 11 | MODIFY EXISTING DRAINAGE STRUCTURE | EA | 26 | \$ 5,000.00 | \$ 130,000.00 |
| 425 1 201 | INLETS, CURB, TYPE 3, <10' | EA | 2 | \$ 9,000.00 | \$ 18,000.00 |
| 425 1 203 | INLETS, CURB, TYPE 5, <10' | EA | 18 | \$ 8,000.00 | \$ 144,000.00 |
| 425 1 451 | INLETS, CURB, TYPE 6, <10' | EA | 15 | \$ 8,200.00 | \$ 123,000.00 |
| 430 175 118 | PIPE, CULVERT, OPTIONAL MATERIAL, ROUND, 18" SCD | LF | 468 | \$ 250.00 | \$ 117,072.50 |
| 430 982 125 | MITERED END SECTION, OPTIONAL ROUND, 18" CD | EA | 3 | \$ 3,083.21 | \$ 9,249.63 |
| 520 1 10 | CONCRETE CURB & GUTTER, TYPE F | LF | 16743 | \$ 40.00 | \$ 669,736.40 |
| 520 2 4 | CONCRETE CURB, TYPE D | LF | 209 | \$ 60.00 | \$ 12,541.20 |
| 520 2 8 | CONCRETE CURB, TYPE RA | LF | 236 | \$ 40.00 | \$ 9,424.80 |
| 520 5 11 | TRAFFIC SEPARATOR CONCRETE - TYPE L 4' WIDE | LF | 124 | \$ 75.00 | \$ 9,308.25 |
| 520 70 | CONCRETE TRAFFIC SEPARATOR, SPECIAL - VARIABLE WIDTH | LF | 203 | \$ 120.00 | \$ 24,366.00 |
| 522 1 | CONCRETE SIDEWALK AND DRIVEWAYS, 4" THICK | SY | 9677 | \$ 65.00 | SEE BELOW |
| 522 2 | CONCRETE SIDEWALK AND DRIVEWAYS, 6" THICK | SY | 2410 | \$ 80.00 | SEE BELOW |
| 526 1 1 | PAVERS, ARCHITECTURAL, ROADWAY | SY | 4494 | \$ 250.00 | SEE BELOW |
| 527 2 | DETECTABLE WARNINGS | SF | 747 | \$ 35.00 | \$ 26,134.85 |
| 570 1 2 | PERFORMANCE TURF, SOD | SY | 6675 | \$ 9.00 | \$ 60,073.20 |
| 1000 99 | UTILITY ADJUSTMENTS | LS | 1 | \$ 380,000.00 | \$ 380,000.00 |
| Signage | | | | | |
| 700-X-XXX | SIGNING | LS | 1 | \$ 30,000.00 | \$ 30,000.00 |
| 0710 11190 | PAINTED PAVEMENT MARKINGS, STANDARD, WHITE, ISLAND NOSE | SF | 30 | \$ 2.72 | \$ 81.33 |
| 710 11 290 | PAINTED PAVEMENT MARKINGS, STANDARD, YELLOW, ISLAND NOSE | SF | 68 | \$ 2.82 | \$ 192.89 |
| 710 90 | PAINTED PAVEMENT MARKINGS - FINAL SURFACE | LS | 1 | \$ 39,000.00 | \$ 39,000.00 |
| 711 16 101 | THERMOPLASTIC, STANDARD-OTHER SURFACES, WHITE, SOLID, 6" | GM | 4,260 | \$ 5,500.00 | \$ 23,430.00 |
| 711 11 123 | THERMOPLASTIC, STANDARD, WHITE, SOLID, 12" FOR CROSSWALK AND ROUNDABOUT | LF | 3541 | \$ 3.50 | \$ 12,394.20 |
| 711 11 124 | THERMOPLASTIC, STANDARD, WHITE, SOLID, 18" FOR DIAGONALS AND CHEVRONS | LF | 135 | \$ 4.50 | \$ 607.50 |
| 711 11 125 | THERMOPLASTIC, STANDARD, WHITE, SOLID, 24" FOR STOP LINE OR CROSSWALK | LF | 583 | \$ 6.00 | \$ 3,498.60 |
| 711 11 141 | THERMOPLASTIC, STANDARD, WHITE, 2-4 DOTTED GUIDELINE/ 6-10 GAP EXTENSION, 6" | GM | 0.150 | \$ 4,000.00 | \$ 600.00 |
| 711 11 144 | THERMOPLASTIC, STANDARD, WHITE, 2-2 DOTTED EXTENSION LINE, 12" FOR ROUNDABOUT | GM | 0.020 | \$ 6,120.00 | \$ 122.40 |
| 711 11 160 | THERMOPLASTIC, STANDARD, WHITE, MESSAGE OR SYMBOL | EA | 58 | \$ 190.00 | \$ 11,020.00 |
| 711 11 224 | THERMOPLASTIC, STANDARD, YELLOW, SOLID, 18" FOR DIAGONAL OR CHEVRON | LF | 358 | \$ 5.50 | \$ 1,966.25 |
| 711 14 125 | THERMOPLASTIC, PREFORMED, WHITE, SOLID, 24" FOR CROSSWALK | LF | 1899 | \$ 6.00 | \$ 11,392.20 |
| 711 16 131 | THERMOPLASTIC, OTHER SURFACES, WHITE, SKIP, 6", 10-30 SKIP OR 3-9 LANE DROP | GM | 0.005 | \$ 5,000.00 | \$ 25.00 |
| 711 16 201 | THERMOPLASTIC, STANDARD-OTHER SURFACES, YELLOW, SOLID, 6" | GM | 2,730 | \$ 5,000.00 | \$ 13,650.00 |
| Lighting / ITS | | | | | |
| 630 XXX | LIGHTING | LS | 1 | \$ 2,000,000.00 | \$ 2,000,000.00 |
| Landscape and Irrigation | | | | | |
| | CANOPY TREE (5" CAL) | EA | 130 | \$ 3,000.00 | \$ 390,000.00 |
| | PALM TREE | EA | 50 | \$ 1,700.00 | \$ 85,000.00 |
| | SPECIMEN PALM TREE (10" CT) | EA | 40 | \$ 3,500.00 | \$ 140,000.00 |
| | UNDERSTORY PLANTING | SF | 55,000 | \$ 3.00 | \$ 165,000.00 |
| | BAHIA SOD | SF | 25,000 | \$ 0.40 | \$ 10,000.00 |
| | IRRIGATION | ALL | 1 | \$ 170,000.00 | \$ 170,000.00 |
| Landscape and Irrigation Subtotal | | | | | \$ 960,000.00 |

| ITEM NO. | DESCRIPTION | UNIT | QTY | MATERIAL | AMOUNT | MATERIAL | AMOUNT | MATERIAL | AMOUNT |
|---|-------------------|------|-------|-----------------------------|-------------------------|--|------------------------|--|------------------------|
| | | | | UNIT PRICE | | UNIT PRICE | | UNIT PRICE | |
| Hardscape and Site Furnishings | | | | | | | | | |
| | SIDEWALK | SY | 12887 | SAWCUT CONCRETE \$ 72.50 | \$ 876,278.50 | DECORATIVE CONCRETE \$ 162.00 | \$ 1,958,029.20 | CONCRETE PAVERS \$ 180.00 | \$ 2,175,588.00 |
| | CROSSWALK | SY | 1075 | STRIPED ASPHALT \$ 14.85 | \$ 15,963.75 | DECORATIVE CONCRETE - VEHICULAR \$ 180.00 | \$ 193,500.00 | CONCRETE PAVERS - VEHICULAR \$ 198.00 | \$ 212,850.00 |
| | INTERSECTIONS | SY | 4494 | ASPHALT \$ 14.85 | \$ 66,735.14 | DECORATIVE CONCRETE - VEHICULAR \$ 180.00 | \$ 808,910.78 | CONCRETE PAVERS - VEHICULAR \$ 198.00 | \$ 889,801.85 |
| | SEATING | ALL | 1 | \$ 40,000.00 | \$ 40,000.00 | | | | |
| | TREE GRATE | EA | 25 | \$ 500.00 | \$ 12,500.00 | | | | |
| | BIKE RACK | EA | 4 | \$ 2,000.00 | \$ 8,000.00 | | | | |
| | LITTER RECEPTACLE | EA | 6 | \$ 1,200.00 | \$ 7,200.00 | | | | |
| | BOLLARD | ALL | 1 | \$ 15,000.00 | \$ 15,000.00 | | | | |
| Hardscape and Site Furnishings Subtotal | | | | | \$ 1,041,677.39 | Subtotal with Decorative Concrete | \$ 3,043,139.98 | Subtotal with Concrete Pavers | \$ 3,360,939.85 |
| BoTA & 10TH FINAL OPC | | | | | | TOTALS WITH DECORATIVE CONCRETE | | TOTALS WITH CONCRETE PAVERS | |
| SUBTOTAL (DOES NOT INCLUDE MOBILIZATION & MOT) | | | | | \$ 8,137,408.59 | \$ | 10,138,871.17 | \$ | 10,456,671.85 |
| MOBILIZATION & MOT | | | | | \$ 219,070.00 | | | | |
| O.P.C. SUBTOTAL | | | | | \$ 8,356,478.59 | \$ | 10,357,941.17 | \$ | 10,675,741.85 |
| 25% CONTINGENCY | | | | | \$ 2,089,119.65 | \$ | 2,589,485.29 | \$ | 2,668,935.26 |
| ESTIMATED O.P.C. TOTAL | | | | | \$ 10,445,598.23 | \$ | 12,947,426.47 | \$ | 13,344,676.31 |

The Engineer has no control over the cost of labor, materials, equipment, or over the Contractor's methods of determining prices or over competitive bidding or market conditions. Opinions of probable costs provided herein are based on the information known to Engineer at this time such as the FDOT Historical Costs online archive and represent only the Engineer's judgment as a design professional familiar with the construction industry. The Engineer cannot and does not guarantee that proposals, bids, or actual construction costs will not vary from its opinions of probable costs.

| Approximate Cost Based on Street Size | | | | | |
|---------------------------------------|-----------|-----------------|------------------|------------------|------------------|
| | Area (Ac) | % of Total Area | Option 1 | Option 2 | Option 3 |
| Total | 13,3026 | 100% | \$ 10,445,598.23 | \$ 12,947,426.47 | \$ 13,344,676.31 |
| BoTA | 6,1213 | 46% | \$ 4,806,627.31 | \$ 5,957,864.00 | \$ 6,140,661.76 |
| 10th | 7,1812 | 54% | \$ 5,638,970.92 | \$ 6,989,562.47 | \$ 7,203,914.24 |

Figure 7-1: Engineers Estimate of Probable Cost

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CHAPTER

8

TRAFFIC STUDY

The City of Sarasota intends to complete a complete streets project on the 10th Street corridor from the eastern edge of the US 41 (Tamiami Trail) roundabout intersection to Orange Avenue as well as safety improvements to Boulevard of the Arts (BotA) from Sarasota Bay to Orange Avenue. The 10th Street complete street project is expected to incorporate a change in the existing cross-section from a 4-lane roadway to a 2-lane roadway east of US 41 (Tamiami Trail) so as to replace excess vehicular capacity while providing enhanced facilities for non-motorists. The BotA complete street project is expected to result in changes to the cross-section with various treatments. Sidewalk widening is expected to occur throughout the corridor with curb extensions provided at various intersections, and the turn lanes at the Coconut Avenue and Lemon Avenue are expected to be removed. Enhanced landscaping is also anticipated throughout both corridors. The purpose of this report is to conduct an analysis of the impacts to level of service caused by the proposed lane reduction along 10th Street and changes at the intersections along both corridors.

This report analyzed the transportation impacts of proposed roadway improvements to the 10th Street and Boulevard of the Arts corridors. Traffic conditions under existing, future no-build, and future build scenarios with the improvements constructed were analyzed. Historic traffic data was used to calculate growth rates and project future conditions. Despite a negative trend in traffic growth over the 5-year history along US 41 (Tamiami Trail), a positive growth rate was assumed so as to analyze potential worse-case conditions.

Overall, the analysis finds that impacts to traffic along the 10th Street corridor due to the proposed lane-repurposing will be minimal when compared to the projected future no-build conditions. In many cases, the increase in delay projected at the signalized intersections amounts to a few additional seconds while the intersection at Orange Avenue is projected to experience a decrease in delay as well as an improvement in future LOS due to the proposed construction of a roundabout. Additionally, it is anticipated that no impacts to the performance of the US 41 (Tamiami Trail) intersection will occur as no roadway changes are proposed within approximately 400-feet of the existing roundabout.

Along the Boulevard of the Arts corridor there is expected to be a decrease in LOS with the change of intersection control from an all-way stop-control to a two-way stop-control at Central Avenue. This is largely due to the increased delay expected for the northbound and southbound movements. An increase in delay is also expected at the Coconut Avenue intersection. However, this intersection is expected to operate with a failing LOS with or without the proposed improvements. It was also noted that the historic growth rate along Boulevard of the Arts was likely driven by recent development along the corridor and, as opportunities for development decrease along the corridor, likely represents a conservative estimate of future traffic.

Overall, the proposed improvements are expected to produce limited negative impact to traffic along either the 10th Street or Boulevard of the Arts corridor. While some increases in delay are expected, the improvements are likely to provide benefits to the safety of non-motorists while also enhancing the user's experience of the study corridors.

The text in this chapter has been pulled from the Traffic Impact Report found in [Appendix C](#) of this document.

CHAPTER

9

CONCLUSION

In conclusion, as downtown Sarasota has continued to grow into a vibrant community where dining at renowned restaurants, shopping at unique boutiques, and visiting beautiful parks has become commonplace. Because of this, the demand for transportation alternatives such as walking, biking, and transit have increased. The proposed concept plans for these projects were shaped by the emerging Rosemary District, which has become a central hub of activity for a diverse crowd of young professionals, artists, families and empty nesters.

This resurgence has resulted in a dire need for supportive infrastructure for pedestrians and bicyclists. As a result, the redesign of Boulevard of the Arts (BotA) and 10th Street (10th) were identified as priority projects in *Sarasota in Motion*. BotA and 10th serve as key links between greater Sarasota and The Bay, the master planned initiative to redevelop 53-acres of city-owned land along Sarasota Bay on the west side of US 41/ Tamiami Trail into public parklands. 10th will functionally serve as The Bay and a future Performing Art Center’s front door for most visitors while BotA will remain an important pedestrian connector to the park district.

To facilitate the conceptual reimagining of these vital streets, the City of Sarasota tasked Kimley-Horn to develop a basis of design (BOD) report to assess the existing conditions and contemplate potential multimodal improvements for BotA and 10th. After gathering physical surveys, studying traffic patterns and land uses, and speaking with the community about their desires through both virtual surveys, typical sections were developed that respond to the unique context of the two streets. See [Chapter 6](#) for the proposed concept plans.

While the two streets both increase street tree coverage and promote bike/ped and micromobility safety, there are key differences. BotA makes use of sharrows within the travel lane as bicycle infrastructure and prioritizes the pedestrian realm between the back of curb and building facades. The addition of on-street parking, reduction in unnecessary turn lanes, widening sidewalks, adding street trees, and narrowing of the curb-curb width will increase “friction” for drivers and lower vehicle speeds, increasing pedestrian comfort and safety. BotA continues to display retail and storefront viability, and wider sidewalks will support additional café seating and street activation. 10th will become a true complete street, with the addition of protected bike lanes on both sides of the roadway, widened sidewalks, stormwater treatment and additional landscape/ street tree opportunities. Turn lanes will continue to ensure adequate traffic flow on this important corridor that serves a variety of land uses, including industrial users north of downtown. All segments accommodate Sarasota County Area Transit (SCAT) shelters where necessary.

The proposed concept plans are consistent with guidance published by the Florida Department of Transportation (FDOT), Florida Greenbook, American Association of State Highway Transportation Officials (AASHTO) Manual, National Associations of City Transportation Officials (NACTO) Urban Bikeway Design Guide and Urban Street Design Guide for pedestrian and bicycle accommodations. The conceptual design was developed with significant input from the public and City staff. A traffic study was performed to review impacts of the proposed improvements on motor vehicle traffic. The study found that there would be minimal impact to traffic flows from the proposed improvements and that the bicycle/ pedestrian facility improvements were appropriate and on-balance a net-benefit in an urban setting.

Overall, the implementation of the recommendations included in this report will result in a transformation of BotA and 10th with a multimodal vision aligned with the City’s Long-Range Planning goals. The design will allow for safe bicycle travel, increased pedestrian activity, and encourage other micro-mobility opportunities. The concept plan fits the context of a highly urban area; supports economic development; complements the surrounding land uses; simplifies conflicts between the modes of travel; and will ensure automobile traffic is well-managed.

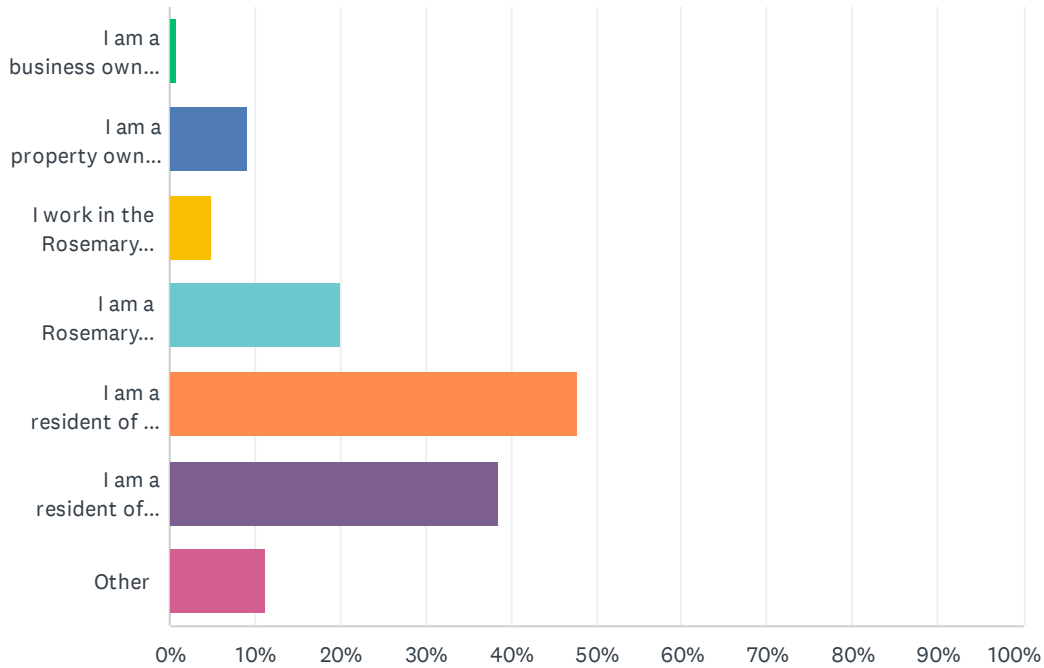
APPENDIX

A

PUBLIC INPUT SURVEY RESULTS

Q1 What is your connection to Boulevard of the Arts? Select all that apply.

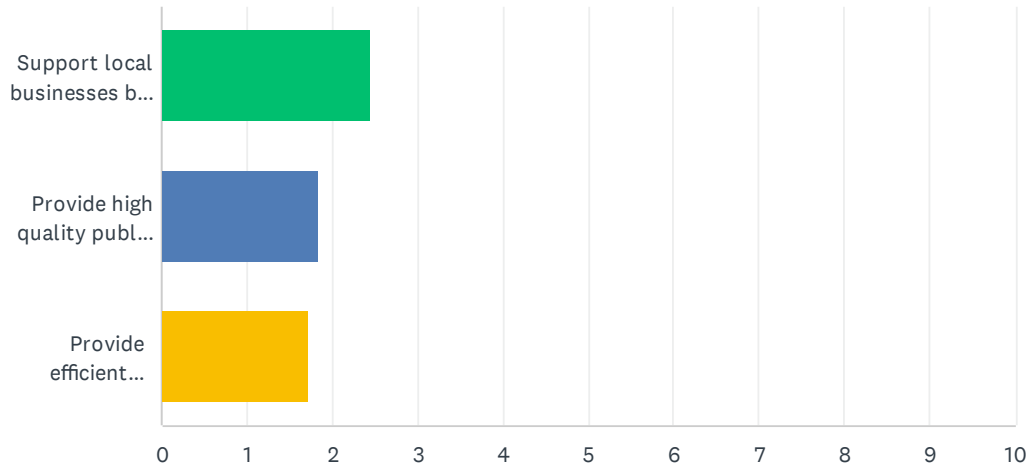
Answered: 1,071 Skipped: 13



| ANSWER CHOICES | RESPONSES | |
|--|-----------|-----|
| I am a business owner on Boulevard of the Arts | 0.93% | 10 |
| I am a property owner on Boulevard of the Arts | 9.15% | 98 |
| I work in the Rosemary District | 4.86% | 52 |
| I am a Rosemary District resident | 20.07% | 215 |
| I am a resident of the City of Sarasota | 47.81% | 512 |
| I am a resident of Sarasota County | 38.66% | 414 |
| Other | 11.30% | 121 |
| Total Respondents: 1,071 | | |

Q2 Rank in order of priority what function you think a traditional 'main' street like Boulevard of the Arts (east of Tamiami Trail) should serve:

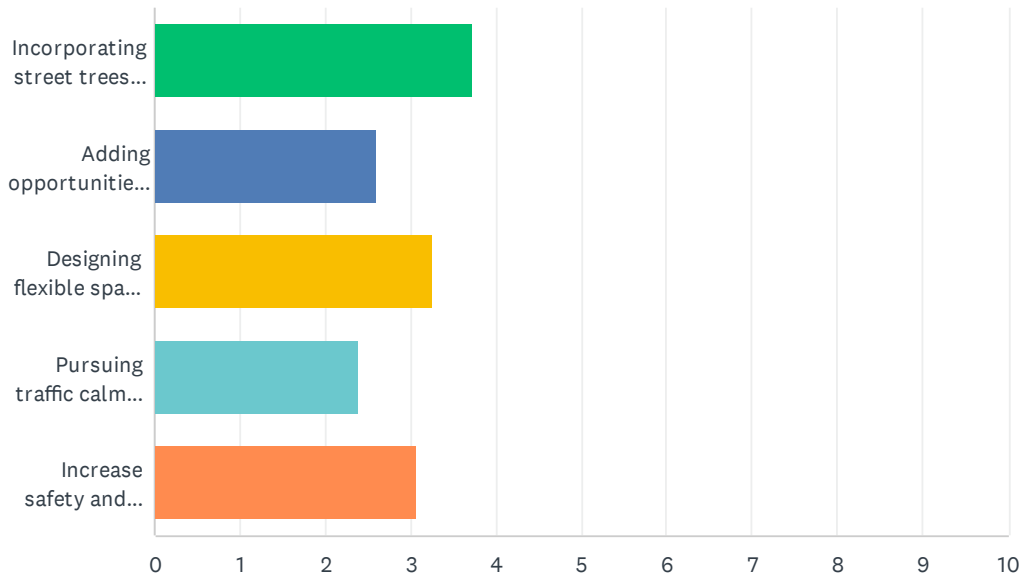
Answered: 1,064 Skipped: 20



| | 1 | 2 | 3 | TOTAL | SCORE |
|---|---------------|---------------|---------------|-------|-------|
| Support local businesses by enhancing the public realm (i.e. providing 'spill over' areas for sidewalk cafes, curbside parking, street trees & benches) | 55.92% 586 | 34.64% 363 | 9.45% 99 | 1,048 | 2.46 |
| Provide high quality public space and opportunities for gathering (i.e. festival street) | 21.65% 226 | 40.33% 421 | 38.03% 397 | 1,044 | 1.84 |
| Provide efficient traffic flow through the Rosemary District to and from other destinations | 23.58% 249 | 24.81% 262 | 51.61% 545 | 1,056 | 1.72 |

Q3 What do you perceive to be the most important for consideration in redesigning Boulevard of the Arts (east of Tamiami Trail)? Rank in order of priority.

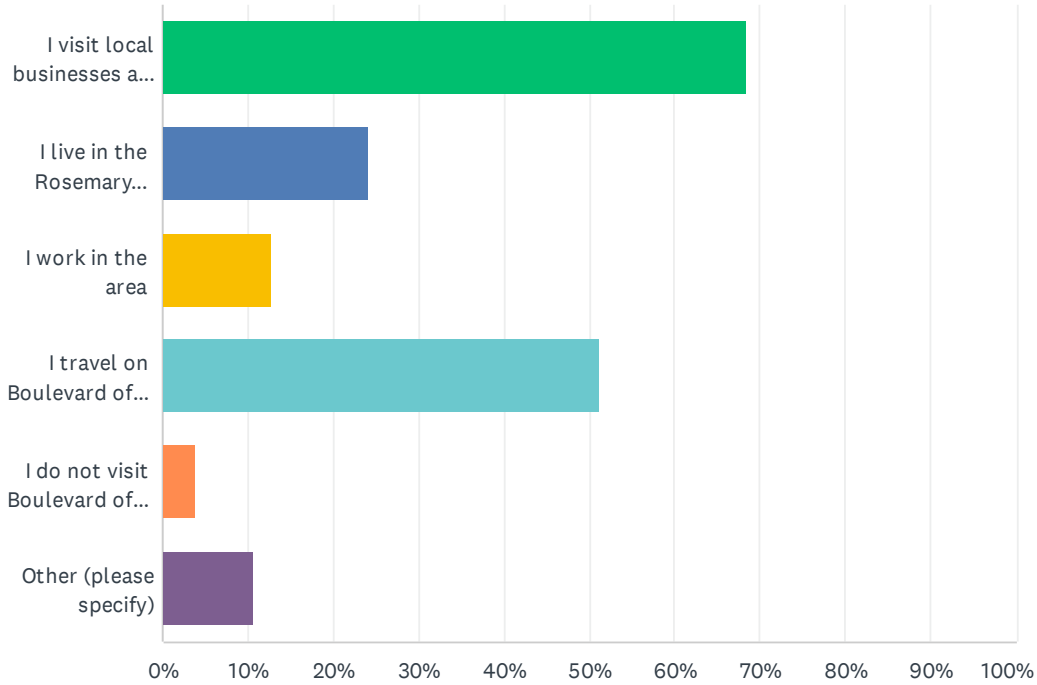
Answered: 1,054 Skipped: 30



| | 1 | 2 | 3 | 4 | 5 | TOTAL | SCORE |
|---|---------------|---------------|---------------|---------------|---------------|-------|-------|
| Incorporating street trees, landscaping and green space | 39.69% 408 | 21.40% 220 | 18.48% 190 | 13.52% 139 | 6.91% 71 | 1,028 | 3.73 |
| Adding opportunities for gathering spaces | 10.99% 112 | 18.94% 193 | 19.43% 198 | 21.10% 215 | 29.54% 301 | 1,019 | 2.61 |
| Designing flexible spaces for pedestrian activity | 16.63% 170 | 26.03% 266 | 32.09% 328 | 18.30% 187 | 6.95% 71 | 1,022 | 3.27 |
| Pursuing traffic calming methods (i.e. speed tables or raised intersections) | 9.31% 95 | 15.39% 157 | 15.78% 161 | 23.14% 236 | 36.37% 371 | 1,020 | 2.38 |
| Increase safety and accessibility (Additional lighting, improved crossings, ADA compliance) | 25.05% 261 | 19.00% 198 | 14.30% 149 | 22.36% 233 | 19.29% 201 | 1,042 | 3.08 |

Q4 Why do you visit Boulevard of the Arts (east of Tamiami Trail)? Select all that apply.

Answered: 1,069 Skipped: 15



| ANSWER CHOICES | RESPONSES | |
|---|-----------|-----|
| I visit local businesses and services | 68.38% | 731 |
| I live in the Rosemary District | 24.13% | 258 |
| I work in the area | 12.72% | 136 |
| I travel on Boulevard of the Arts to get somewhere else | 51.26% | 548 |
| I do not visit Boulevard of the Arts | 3.74% | 40 |
| Other (please specify) | 10.76% | 115 |
| Total Respondents: 1,069 | | |

| # | OTHER (PLEASE SPECIFY) | DATE |
|---|--|--------------------|
| 1 | WSLR Radio, BOULEVARD of the Arts and Orange. | 2/28/2022 12:12 PM |
| 2 | I live across from the Bay on Tamiami & 11th St. | 2/27/2022 9:35 PM |
| 3 | Holly Hall events | 2/27/2022 2:46 PM |
| 4 | Walking | 2/27/2022 9:08 AM |
| 5 | Attend performances at Van wezel hall | 2/25/2022 11:56 PM |
| 6 | Ssas | 2/25/2022 3:52 PM |
| 7 | Entertainment | 2/24/2022 10:44 AM |

Boulevard of the Arts & 10th Street Complete Streets Survey

| | | |
|----|---|--------------------|
| 8 | Walk from downtown to bayfront (Van wezel) frequently | 2/24/2022 8:47 AM |
| 9 | The roud abouts are a disgrace. The only straight street is Boulevard of the arts keep it that way | 2/24/2022 2:36 AM |
| 10 | Frequent attendee at Van Wezel. | 2/24/2022 12:20 AM |
| 11 | going to Van WESEL | 2/23/2022 11:58 PM |
| 12 | Restaurant s | 2/23/2022 9:48 PM |
| 13 | Access to city public amenities | 2/23/2022 5:30 PM |
| 14 | Van Wezel /Sarasota Opera patron | 2/23/2022 5:11 PM |
| 15 | Access to VanWezel | 2/23/2022 2:34 PM |
| 16 | I take classes at Sarasota Ballet 3-4 times/week & related events | 2/23/2022 2:10 PM |
| 17 | I live on Blvd of the Arts | 2/23/2022 1:37 PM |
| 18 | i live downtown | 2/23/2022 1:35 PM |
| 19 | Theater, plays, restaurants | 2/23/2022 1:14 PM |
| 20 | I live on LBK and will move downtown later this year. | 2/23/2022 11:29 AM |
| 21 | Go to Van Wezel | 2/23/2022 11:12 AM |
| 22 | To attend shows at Van Wezel | 2/23/2022 11:05 AM |
| 23 | Have used it in the past to go to Van Wezel. | 2/23/2022 11:04 AM |
| 24 | I live nearby | 2/23/2022 10:36 AM |
| 25 | I live next to it at Gillespie Park | 2/23/2022 10:24 AM |
| 26 | Get to theaters | 2/23/2022 10:21 AM |
| 27 | I walk in and he area and definitely want safe pedestrian access | 2/23/2022 10:13 AM |
| 28 | I own a local transportation company and are there "weekly" and "daily" with jobs. | 2/23/2022 10:05 AM |
| 29 | Cross Tamiami to get to park | 2/23/2022 10:03 AM |
| 30 | I live within walking distance | 2/23/2022 10:02 AM |
| 31 | Visit the park | 2/23/2022 10:01 AM |
| 32 | We enjoy the outdoor festivals and programs on weekends. | 2/23/2022 9:57 AM |
| 33 | I live on Gulfstream, We move downtown for the flexibility of having a walking area and enjoyed the businesses and entertainment that the downtown lifestyle provides | 2/23/2022 9:57 AM |
| 34 | I rent on Boulevard of the Arts | 2/20/2022 7:38 PM |
| 35 | I walk daily for exercise and try to walk instead of drive for errands. | 2/16/2022 6:49 AM |
| 36 | I live in Gillespie Park | 2/15/2022 3:16 PM |
| 37 | I have several friends that live there and I walk from downtown | 2/14/2022 7:09 PM |
| 38 | I also travel on bota to go other places | 2/14/2022 8:14 AM |
| 39 | To access The BAY Project | 2/13/2022 3:19 PM |
| 40 | Walk the dog | 2/12/2022 12:40 PM |
| 41 | recreation, access to west of Tamiami Trail, parking and business in that order | 2/12/2022 11:04 AM |
| 42 | I'm a lifelong resident and use the parks for recreation and mental health. | 2/12/2022 10:31 AM |
| 43 | I live one block from rosemary district in Central coaconut | 2/12/2022 8:06 AM |
| 44 | For nature and peace from the city | 2/10/2022 2:38 PM |

Boulevard of the Arts & 10th Street Complete Streets Survey

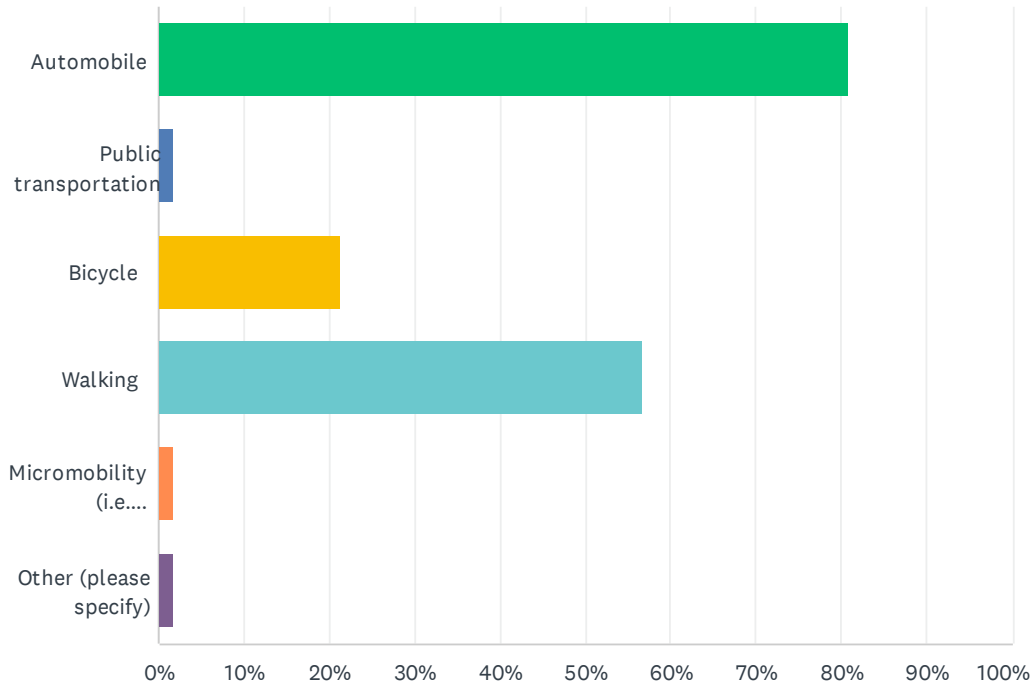
| | | |
|----|--|-------------------|
| 45 | Church | 2/10/2022 8:45 AM |
| 46 | Day trips from Tampa, former resident. | 2/9/2022 2:42 PM |
| 47 | Bike rider enjoying the route | 2/8/2022 8:35 AM |
| 48 | Regularly bike the road | 2/7/2022 9:15 PM |
| 49 | I live at CitySide Apartments | 2/7/2022 6:58 PM |
| 50 | I live on it | 2/7/2022 12:22 PM |
| 51 | I walk my dog here | 2/7/2022 12:18 PM |
| 52 | I enjoy peaceful neighborhood. I do not want our streets all torn up and the noise, traffic disruption and construction | 2/7/2022 12:13 PM |
| 53 | walk and ride bicycle along BOTA to The Bay Park--NEED A PEDX BRIDGE OVER 41!!! | 2/7/2022 11:40 AM |
| 54 | PEDESTRIAN BRIDGE ACROSS HWY 41!!!!!! IT'S VERY DANGEROUS TO CROSS EVEN AT THE BLVD OF THE ARTS TRAFFIC LIGHT. TRAFFIC IS CONSTANTLY BACKED UP AND DRIVERS ARE IMPATIENT AND NOT PAYING ATTENTION TO PEDESTRIANS CROSSWALKS. | 2/7/2022 9:46 AM |
| 55 | cut through to avoid 41 | 2/6/2022 1:52 PM |
| 56 | Live west if the trail on BOTS | 2/6/2022 12:56 PM |
| 57 | I live here | 2/6/2022 12:20 PM |
| 58 | ride a bicycle | 2/6/2022 10:37 AM |
| 59 | Local cultural activities | 2/6/2022 9:11 AM |
| 60 | Enjoy Sarasota Garden Club | 2/5/2022 9:27 PM |
| 61 | I walk between the Bay and downtown | 2/5/2022 5:14 PM |
| 62 | I live on the Blvd | 2/5/2022 4:55 PM |
| 63 | I use it to get to and from Van Wezel & Holley Hall | 2/5/2022 4:11 PM |
| 64 | I know nothing of interest there | 2/5/2022 1:09 PM |
| 65 | I visit friends there | 2/5/2022 12:46 PM |
| 66 | I like to walk around Sarasota near The Bay | 2/5/2022 12:45 PM |
| 67 | Dog walks | 2/5/2022 12:22 PM |
| 68 | Go to Van Wezel | 2/5/2022 12:09 PM |
| 69 | Cross Tamiami to reach the Bay Park | 2/5/2022 11:52 AM |
| 70 | I see it as a potential prime public resource | 2/5/2022 11:23 AM |
| 71 | I bike on Blvd of the Arts when traveling in/to the area. | 2/5/2022 10:15 AM |
| 72 | Getting to the Bay | 2/5/2022 9:57 AM |
| 73 | I live near this area. | 2/5/2022 9:35 AM |
| 74 | To get to The Bay | 2/5/2022 9:31 AM |
| 75 | Ride my bike through the area | 2/5/2022 7:27 AM |
| 76 | I travel through here by bicycle to avoid busier routes. | 2/5/2022 12:43 AM |
| 77 | We cycle on these streets | 2/4/2022 9:41 PM |
| 78 | live near 10th | 2/4/2022 9:28 PM |
| 79 | I live in Gillespie Park and often walk on Blvd. oh the arts and visit businesses there. | 2/4/2022 3:34 PM |
| 80 | I go to planned activities in the Rosemary Dist. | 2/4/2022 8:34 AM |

Boulevard of the Arts & 10th Street Complete Streets Survey

| | | |
|-----|--|-------------------|
| 81 | I live on 6th street | 2/3/2022 8:50 AM |
| 82 | evening walks and bike rides | 2/3/2022 3:47 AM |
| 83 | Will be living PT in the area | 2/2/2022 11:52 PM |
| 84 | I live very close to Rosemary District | 2/2/2022 1:39 PM |
| 85 | I walk the entire downtown and Rosemarie District every day | 2/2/2022 1:33 PM |
| 86 | live on 11th street | 2/2/2022 10:33 AM |
| 87 | I live in the Coconut District and consider Blvd of the Arts as part of my neighborhood | 2/2/2022 9:36 AM |
| 88 | I live in Gillespie Park, any ways to improve access, connectivity and responsible growth is appreciated. | 2/1/2022 11:23 PM |
| 89 | I am involved with a youth organization on Boulevard of the Arts | 2/1/2022 10:12 PM |
| 90 | The Rosemary District has suffered in recent years. It was a much more vibrant, destination for dining and shopping just a few years ago. I would like to see a return of the quaint shops and unique restaurants. | 2/1/2022 9:28 PM |
| 91 | I live in Coconut district...almost Rosemary | 2/1/2022 8:52 PM |
| 92 | I use it to get to the Van Wezel | 2/1/2022 8:30 PM |
| 93 | I live at N Palm Ave and 41 | 2/1/2022 7:57 PM |
| 94 | Own a business on Central Ave | 2/1/2022 7:39 PM |
| 95 | After a night downtown (love walking after a dinner at the ritz!) | 2/1/2022 7:19 PM |
| 96 | I live in the area and walk the Blvd of the Arts to get downtown and to the Rosemary District | 2/1/2022 5:30 PM |
| 97 | local street festivals | 2/1/2022 5:18 PM |
| 98 | Live on Blvd of the Arts | 2/1/2022 5:17 PM |
| 99 | Accessibility to the arts | 2/1/2022 5:08 PM |
| 100 | I bike | 2/1/2022 5:05 PM |
| 101 | Condo on the Bay owner | 2/1/2022 4:25 PM |
| 102 | I live just south of 10th street and walk through to get to downtown. | 2/1/2022 4:14 PM |
| 103 | interesting area to walk | 2/1/2022 3:44 PM |
| 104 | I live here | 2/1/2022 3:01 PM |
| 105 | Walk through frequently to/from Central Coconut | 2/1/2022 2:54 PM |
| 106 | It seems a central conduit to access 41, or to get from 41 to the network of roads east of 41. | 2/1/2022 2:32 PM |
| 107 | I live on Blvd of the Arts | 2/1/2022 12:55 PM |
| 108 | I walk everyday on Boulevard of the Arts | 2/1/2022 12:41 PM |
| 109 | Need to be able to cross 41 on BOTA | 2/1/2022 11:56 AM |
| 110 | I live in Gillespie Park and walk/bike the area | 2/1/2022 11:16 AM |
| 111 | Walk my dog | 2/1/2022 11:00 AM |
| 112 | I like to attend neighborhood events such as the 3rd Thursday Night Market | 2/1/2022 10:37 AM |
| 113 | My child attends school there | 2/1/2022 10:09 AM |
| 114 | Host programs and events | 2/1/2022 9:40 AM |
| 115 | Lived in Sarasota for 10 years | 2/1/2022 9:21 AM |

Q5 How do you access businesses or services on Boulevard of the Arts (east of Tamiami Trail)? Select all that apply.

Answered: 1,070 Skipped: 14



| ANSWER CHOICES | RESPONSES |
|--|------------|
| Automobile | 80.84% 865 |
| Public transportation | 1.68% 18 |
| Bicycle | 21.40% 229 |
| Walking | 56.82% 608 |
| Micromobility (i.e. Bikeshare, scooter, golf cart) | 1.78% 19 |
| Other (please specify) | 1.78% 19 |
| Total Respondents: 1,070 | |

| # | OTHER (PLEASE SPECIFY) | DATE |
|---|--|--------------------|
| 1 | Lyft/Uber | 2/25/2022 11:56 PM |
| 2 | uber, lyft and taxi | 2/24/2022 8:03 PM |
| 3 | Pedestrian bridge is badly needed to cross N. Tamiami | 2/24/2022 8:36 AM |
| 4 | boat | 2/23/2022 11:29 AM |
| 5 | Both auto and our private rental vehicles, trolleys, minibuses, etc. | 2/23/2022 10:05 AM |
| 6 | Right now, we access the area by car because of this traffic mess and unsafe drivers that are unaware of the traffic congestion. Eventually we want to be able to walk | 2/23/2022 9:57 AM |

Boulevard of the Arts & 10th Street Complete Streets Survey

| | | |
|----|--|--------------------|
| 7 | I would love a better public transportation option. The random SCAT bus is not sufficient for our communities needs. | 2/23/2022 9:52 AM |
| 8 | Typically automobile but I would like to have more transit options so I am excited for public micromobility | 2/14/2022 11:39 AM |
| 9 | I also walk and bike there | 2/14/2022 8:14 AM |
| 10 | Walk the dog, get to businesses and the Bayfront | 2/12/2022 12:46 PM |
| 11 | All you are doing is making it extremely unappealing and unnatural ! | 2/12/2022 10:31 AM |
| 12 | uber / lyft | 2/11/2022 5:18 PM |
| 13 | mostly walk or bicycle | 2/7/2022 11:40 AM |
| 14 | I would love to arrive by bike via the Legacy Trail and explore the city | 2/5/2022 2:11 PM |
| 15 | None | 2/2/2022 5:40 PM |
| 16 | I am a wheelchair user | 2/1/2022 4:01 PM |
| 17 | I am disabled. My walker doesn't fit on the sidewalks on Bv Arts. I wish I could go outdoors here! | 2/1/2022 1:02 PM |
| 18 | Use to use the free shuttle from downtown, we need something like that again | 2/1/2022 9:43 AM |
| 19 | Skateboard | 2/1/2022 9:28 AM |

Q6 What are appropriate amenities that would support cycling and public transit access and travel on Boulevard of the Arts (east of Tamiami Trail)? (e.g. bicycle lanes, bus shelters, among others)

Answered: 643 Skipped: 441

| # | RESPONSES | DATE |
|----|--|--------------------|
| 1 | Covered bus shelters, bike lanes, wider sidewalks, trees providing shade to both bike paths and sidewalks. | 2/28/2022 3:40 PM |
| 2 | No amenities that attract more people. Keeping automobile flow is the most important issue. Thousands of cars idling on Tamiami is creating an air pollution disaster and will destroy Sarasota. | 2/28/2022 1:42 PM |
| 3 | more non-driving areas for walkers and cyclists | 2/28/2022 12:12 PM |
| 4 | Shade from trees, landscaping for aesthetics (Michigan Ave in Chicago is tree and flower enhanced) cafés and kiosks for stopping, small squares with benches, water sources and shade. | 2/28/2022 11:51 AM |
| 5 | there does not appear to be enough space to accommodate a bicycle lane without eliminating an auto lane. Auto traffic is already crowded in the area and eliminating a lane here would be bad. Make other streets in the Rosemary area one way and add bike lanes there. | 2/28/2022 9:03 AM |
| 6 | We need more bike friendly and pedestrian friendly roads. The more bikes we can have, perhaps the less cars we'll have. | 2/27/2022 9:35 PM |
| 7 | better sidewalks | 2/27/2022 5:43 PM |
| 8 | Don't know | 2/27/2022 2:46 PM |
| 9 | Elevated walk way | 2/27/2022 9:08 AM |
| 10 | Shuttle busses from utc or the Meadows area | 2/26/2022 11:20 PM |
| 11 | Bike lanes | 2/26/2022 10:23 AM |
| 12 | Safe bike lanes and a possible overpass. | 2/26/2022 8:14 AM |
| 13 | Designated Lyft/Uber Stops Rental Scooters Safe pedestrian overpass Safety for pedestrian access | 2/25/2022 11:56 PM |
| 14 | Bicycle lanes, bus shelters | 2/25/2022 4:01 PM |
| 15 | Proper bicycle lanes - i.e. w some safety 'barrier' to cars. Covered bus stop seating. High visibility at intersections so cars do see oncoming pedestrians and bicyclists. | 2/25/2022 12:48 PM |
| 16 | Bicycle Lanes, walking promenade, only cars and no trucks or busses | 2/25/2022 10:49 AM |
| 17 | Bike lanes walking access | 2/25/2022 9:37 AM |
| 18 | Bike lanes | 2/25/2022 9:05 AM |
| 19 | Pedestrian bridge over tamiami trail to bay park | 2/25/2022 6:08 AM |
| 20 | bike lanes and bus shelters | 2/24/2022 8:03 PM |
| 21 | All of the above | 2/24/2022 1:17 PM |
| 22 | bicycle lanes, bus shelters, widen if possible | 2/24/2022 11:48 AM |
| 23 | Bike lanes, better visa bill to see pedestrians | 2/24/2022 10:44 AM |
| 24 | X | 2/24/2022 10:01 AM |
| 25 | Bike lanes | 2/24/2022 9:38 AM |

Boulevard of the Arts & 10th Street Complete Streets Survey

| | | |
|----|---|--------------------|
| 26 | Let's not clutter up this area with a garage or bus shelters. There is not enough green space with the hotel construction built so close to the street. | 2/24/2022 9:17 AM |
| 27 | Bike lanes, Bike parking | 2/24/2022 9:07 AM |
| 28 | bicycle lanes | 2/24/2022 8:47 AM |
| 29 | Traffic management | 2/24/2022 8:46 AM |
| 30 | Bike lanes | 2/24/2022 8:36 AM |
| 31 | Bicycle lanes | 2/24/2022 7:11 AM |
| 32 | None | 2/24/2022 2:36 AM |
| 33 | Lighting, bike racks, drinking fountains, bike lanes | 2/23/2022 11:30 PM |
| 34 | Bicycle lanes | 2/23/2022 9:40 PM |
| 35 | bicycle lanes | 2/23/2022 8:44 PM |
| 36 | Food trucks or mobile vendors, water bottle refill spots, bike lanes. | 2/23/2022 8:29 PM |
| 37 | Bike paths, overpasses for bikes and pedestrians, gathering hubs with green spaces | 2/23/2022 7:30 PM |
| 38 | Bicycle lanes; slower and less autos | 2/23/2022 5:30 PM |
| 39 | Sidewalks | 2/23/2022 5:19 PM |
| 40 | Bicycle lanes, access to Van Wezel with ample parking. | 2/23/2022 5:01 PM |
| 41 | Street is already too busy. Safety first. | 2/23/2022 4:52 PM |
| 42 | Bike lanes | 2/23/2022 4:40 PM |
| 43 | bike lanes, bus stops | 2/23/2022 4:35 PM |
| 44 | Bicycle lanes that are visually different from roadways. Trees between cars and pedestrians, cross walks that stand out. | 2/23/2022 4:25 PM |
| 45 | bicycle lanes; wide sidewalks; sitting benches | 2/23/2022 4:04 PM |
| 46 | bicycle lanes | 2/23/2022 3:57 PM |
| 47 | Bicycle lanes | 2/23/2022 3:53 PM |
| 48 | Bicycle lanes, city share bikes /scooters services | 2/23/2022 3:43 PM |
| 49 | Bicycle Lanes | 2/23/2022 3:37 PM |
| 50 | Bike lanes | 2/23/2022 3:27 PM |
| 51 | Eliminate right turn on red. It is too dangerous for peds and bikes. | 2/23/2022 3:03 PM |
| 52 | Bike lanes - roundabouts | 2/23/2022 2:53 PM |
| 53 | Bike lanes, parking | 2/23/2022 2:51 PM |
| 54 | Bike lanes | 2/23/2022 2:50 PM |
| 55 | Not sure | 2/23/2022 2:38 PM |
| 56 | This is great but with all the housing there I think it can not be done safely. This should have been thought of long ago. This city is full of people over 65 who are not riding bikes or buses. | 2/23/2022 2:34 PM |
| 57 | bicycle lanes; bus shelters | 2/23/2022 2:27 PM |
| 58 | Provide adequate parking for people who arrive from distant parts of the county, (I.e. Meadows, Palmer Ranch, Lakewood Ranch, keys) | 2/23/2022 2:10 PM |
| 59 | Pretty much what you mentioned. Decent bicycle lanes and bus shelters with lighting. | 2/23/2022 2:06 PM |
| 60 | NO ROUNDABOUTS/CIRCLES EVER!!!! | 2/23/2022 1:46 PM |
| 61 | Handicap signs warnings | 2/23/2022 1:39 PM |

Boulevard of the Arts & 10th Street Complete Streets Survey

| | | |
|----|---|--------------------|
| 62 | Bus shelters, bike lanes, more sitting places | 2/23/2022 1:35 PM |
| 63 | Making much safer | 2/23/2022 1:31 PM |
| 64 | Bike lanes | 2/23/2022 1:24 PM |
| 65 | Bicycle lanes, bus shelters, bike racks | 2/23/2022 1:08 PM |
| 66 | bicycle lanes, places to park & lock bikes, bus stops & benches | 2/23/2022 12:56 PM |
| 67 | wide bike paths | 2/23/2022 12:50 PM |
| 68 | Bicycle lanes | 2/23/2022 12:42 PM |
| 69 | Bicycle lanes and wide sidewalks for pedestrian walking. | 2/23/2022 12:24 PM |
| 70 | Bike lanes | 2/23/2022 12:16 PM |
| 71 | The area around 41 is so congested now. Creating a space of cycling, pedestrians, public transit and vehicles will be quite a task. | 2/23/2022 12:11 PM |
| 72 | Bicycle lanes | 2/23/2022 12:09 PM |
| 73 | designated bicycle lanes and bus shelters and parking garages | 2/23/2022 11:52 AM |
| 74 | Bike Lanes A bridge across 41 to Bay Front Park | 2/23/2022 11:48 AM |
| 75 | Pedestrian overpasses. | 2/23/2022 11:47 AM |
| 76 | Wide bicycle lanes, outdoor cafe seating | 2/23/2022 11:45 AM |
| 77 | Wide sidewalks. Benches | 2/23/2022 11:38 AM |
| 78 | bridges over us 41 | 2/23/2022 11:29 AM |
| 79 | Dining options | 2/23/2022 11:17 AM |
| 80 | Bike racks, bus benches...NO SHELTERS as they are being overtaken by homeless. | 2/23/2022 11:17 AM |
| 81 | Bicycle lanes and pedestrian walkways | 2/23/2022 11:15 AM |
| 82 | Bicycle lanes separate from the pedestrian walkways | 2/23/2022 11:11 AM |
| 83 | Safety | 2/23/2022 11:08 AM |
| 84 | no bikes | 2/23/2022 11:07 AM |
| 85 | Bike lane, Safe pedestrian walking spaces | 2/23/2022 11:07 AM |
| 86 | Addequate parking facilities. | 2/23/2022 11:04 AM |
| 87 | Keep pedestrians away from vehicles. Make safe walks, similar to Vegas. | 2/23/2022 10:58 AM |
| 88 | Bicycle lanes | 2/23/2022 10:52 AM |
| 89 | Bike lanes | 2/23/2022 10:48 AM |
| 90 | Bike lanes, bus shelters and bus stops that do not impede the traffic flow. Handicapped accessible features. | 2/23/2022 10:47 AM |
| 91 | enhanced cross walks, roundabouts, in addition to stated bicycle lanes, bus shelters | 2/23/2022 10:45 AM |
| 92 | pedestrian overpass on tamiami trail | 2/23/2022 10:44 AM |
| 93 | Bike Lane | 2/23/2022 10:41 AM |
| 94 | Bike trail connection from Payne park. | 2/23/2022 10:39 AM |
| 95 | would not cycle there.. too far from my house | 2/23/2022 10:39 AM |
| 96 | Hi ??? | 2/23/2022 10:38 AM |
| 97 | Bike lanes | 2/23/2022 10:37 AM |
| 98 | No opinion | 2/23/2022 10:29 AM |

Boulevard of the Arts & 10th Street Complete Streets Survey

| | | |
|-----|--|--------------------|
| 99 | I would bike to this area if more road space/sidewalk space was made for people and bikes. A curb separation between car and bike lanes and wider sidewalks. Traffic calming methods so pedestrians/cyclists feel safer out of their cars. | 2/23/2022 10:27 AM |
| 100 | Overpass for walkers and bikers...easy access to walk to downtown... crossing Tamiami is dangerous | 2/23/2022 10:26 AM |
| 101 | Bicycle lanes | 2/23/2022 10:25 AM |
| 102 | bicycle lanes and bicycle sharing stations | 2/23/2022 10:24 AM |
| 103 | Bu=icycle lanes, pedestrian bridge | 2/23/2022 10:23 AM |
| 104 | pedestrian walkways that go over Tamiami is essential. | 2/23/2022 10:23 AM |
| 105 | Bike trails | 2/23/2022 10:23 AM |
| 106 | bike lanes | 2/23/2022 10:21 AM |
| 107 | Separate large lanes for each: Buses, Bicycles Shelters & Benches for both | 2/23/2022 10:21 AM |
| 108 | Wide pedestrian walks. | 2/23/2022 10:20 AM |
| 109 | bike lanes | 2/23/2022 10:19 AM |
| 110 | You tell me. | 2/23/2022 10:17 AM |
| 111 | Pedestrian bridge and bike bridge | 2/23/2022 10:14 AM |
| 112 | Bicycle lanes | 2/23/2022 10:13 AM |
| 113 | Public Transit and Bike Lanes | 2/23/2022 10:13 AM |
| 114 | Don't care. Bike riders will do whatever they want anyway. | 2/23/2022 10:13 AM |
| 115 | Wide sidewalks | 2/23/2022 10:06 AM |
| 116 | More public transit | 2/23/2022 10:06 AM |
| 117 | Having curb "cut-outs" near hotels and restaurants helps ggreatly. suchas.....the Modern has in front of their doors. | 2/23/2022 10:05 AM |
| 118 | Bicycle lanes | 2/23/2022 10:04 AM |
| 119 | My opinion: I can't envision where bicycle lanes could be..It is so crowded with cars and people that the bicyclist are a danger to both. I don't even know about the bus shelters as they could be inhabited by the homeless. Make more "shelters" for the homeless. | 2/23/2022 10:04 AM |
| 120 | Bike lanes | 2/23/2022 10:03 AM |
| 121 | Bike paths, Bus stops, pedestrian bridge X Tamiami | 2/23/2022 10:03 AM |
| 122 | bicycle lanes | 2/23/2022 10:02 AM |
| 123 | Bike lanes, pedestrian bridges | 2/23/2022 10:01 AM |
| 124 | Wider bike lanes, resting spots during the high heat of the day, covered area from sun such as trees for shade,etc., water fountains for drinking for both humans and pets, bike rental or bike lock up areas, scooter rental area for those unable to walk, wheelchair accessible | 2/23/2022 10:00 AM |
| 125 | pedestrian-friendly access to the Bay Front and Van Wezel area | 2/23/2022 9:59 AM |
| 126 | Right now, we access the area by car because of this traffic mess and unsafe drivers that are unaware of the traffic congestion. Eventually we want to be able to walk | 2/23/2022 9:57 AM |
| 127 | Bike lanes | 2/23/2022 9:56 AM |
| 128 | Bicycle lanes | 2/23/2022 9:55 AM |
| 129 | I don't use public transportation but bus shelters are critical for those who do especially during the summer months when heat is extreme and lightening is a daily occurrence. | 2/23/2022 9:55 AM |
| 130 | Bike lanes | 2/23/2022 9:54 AM |

Boulevard of the Arts & 10th Street Complete Streets Survey

| | | |
|-----|---|--------------------|
| 131 | Bike lanes | 2/23/2022 9:53 AM |
| 132 | Pedestrian bridges - there is no reason to put pedestrians in harms way and they are simply not safe crossing over 41 to get to the Bay. | 2/23/2022 9:52 AM |
| 133 | Bike lanes and safe car driving | 2/23/2022 9:52 AM |
| 134 | Bike lanes | 2/23/2022 9:48 AM |
| 135 | Bike lanes. Pedestrian safety amenities. | 2/23/2022 9:48 AM |
| 136 | No bike lanes. Very dangerous here in Florida | 2/23/2022 9:47 AM |
| 137 | Ferry service from Longboat Key plus bike lanes | 2/23/2022 9:46 AM |
| 138 | Bike lanes | 2/23/2022 9:45 AM |
| 139 | bicycle lanes | 2/23/2022 9:45 AM |
| 140 | Bicycle lanes | 2/23/2022 9:45 AM |
| 141 | safe bicycle lanes, safe street crossings for pedestrians and cyclists. | 2/23/2022 9:45 AM |
| 142 | pedestrian overpasses | 2/23/2022 9:43 AM |
| 143 | Bus shelters -- shade is so important! Wayfinding signage identifying the area as a destination for transit users and walkers/bikers. Bike lanes good but not as high a priority on BOTA as on other streets, as traffic speeds are slow and street is narrow, and existing on-street parking has a traffic calming effect I wouldn't want to jeopardize. | 2/23/2022 9:24 AM |
| 144 | Bicycle lanes | 2/22/2022 11:52 PM |
| 145 | 1) Sharing the road (sharrows), protected bike lanes, or green-painted bike lanes. 2) Low vehicle speeds! 3) Bus shelters, trees for shade, landscaping. Public art. Creative lighting. 4) Bike racks. | 2/22/2022 8:59 AM |
| 146 | bicycle lanes | 2/20/2022 7:40 PM |
| 147 | NOT bike lanes- street is too narrow already - if you add bike lanes it will make traffic even worse. It is only street to get out of our neighborhood | 2/20/2022 7:38 PM |
| 148 | Cross walks | 2/20/2022 10:29 AM |
| 149 | bicycle lanes, wide sidewalks | 2/20/2022 5:18 AM |
| 150 | bicycle lanes, bus shelters, among others | 2/17/2022 11:42 AM |
| 151 | bicycle lanes, | 2/16/2022 10:42 AM |
| 152 | Pedestrian Bridge | 2/15/2022 6:25 PM |
| 153 | Bike lanes, wide sidewalks like between Lemon and Orange for pedestrians are awesome | 2/15/2022 3:16 PM |
| 154 | Designated bike lane | 2/15/2022 2:49 PM |
| 155 | bike lanes, safe and aesthetically-pleasant bus stops and shelters | 2/15/2022 1:55 PM |
| 156 | Wide sidewalks, safety measures, landscaping | 2/15/2022 1:15 PM |
| 157 | Bike lanes, nice bus shelter | 2/15/2022 1:04 PM |
| 158 | bicycle lanes and safe pedestrian walkways ad road crossings | 2/15/2022 12:02 PM |
| 159 | Protected bike lanes, micro mobility | 2/15/2022 10:42 AM |
| 160 | Bicycle lanes with plenty of space. Low speed limits through area | 2/15/2022 9:39 AM |
| 161 | Bike share | 2/15/2022 9:24 AM |
| 162 | bike lanes | 2/15/2022 9:22 AM |
| 163 | Easing of traffic | 2/15/2022 8:33 AM |
| 164 | Wide enough bike lanes | 2/15/2022 8:31 AM |

Boulevard of the Arts & 10th Street Complete Streets Survey

| | | |
|-----|---|--------------------|
| 165 | Love Bike Lanes like the existing bike lanes on Cocoanut Ave. Bus shelters on Cocoanut at 9th St and McCown Towers are old and unsightly but heavily used. | 2/15/2022 8:15 AM |
| 166 | Bike lanes | 2/14/2022 11:35 PM |
| 167 | shade trees, wide sidewalks | 2/14/2022 9:49 PM |
| 168 | Protected bike lanes would make people feel more safe cycling and encourage more casual cyclists to explore the area on foot/bike | 2/14/2022 4:22 PM |
| 169 | Bicycle lanes | 2/14/2022 2:12 PM |
| 170 | Bike lanes, signage for cars to share/drive safely with bikes, lighting, bike racks, walk signals for pedestrians, curb cuts | 2/14/2022 2:04 PM |
| 171 | Bike lanes | 2/14/2022 1:58 PM |
| 172 | Outdoor availability to restaurants, artistic establishments, large enough walkway to accommodate bike storage, benches etc. | 2/14/2022 11:50 AM |
| 173 | Bike lanes, public bike racks with locks, mirco-mobility docking stations NOT in the sidewalk. | 2/14/2022 11:39 AM |
| 174 | Bicycle lanes, pedestrian crossings | 2/14/2022 10:39 AM |
| 175 | Bike lane | 2/14/2022 9:52 AM |
| 176 | bicycle lanes, artistically designed bus shelters | 2/14/2022 9:24 AM |
| 177 | bike lanes, medians | 2/14/2022 9:05 AM |
| 178 | large sidewalks | 2/14/2022 8:49 AM |
| 179 | Bike lanes, bike racks, bus shelters, signal pre-emotion for buses (needed system wide!) | 2/14/2022 8:21 AM |
| 180 | Bike lanes | 2/14/2022 8:14 AM |
| 181 | Bike path set off from traffic for safety. Too many times I have encountered bikers on the sidewalk even with a dedicated bike lane. | 2/13/2022 7:57 PM |
| 182 | Bicycle lanes | 2/13/2022 7:04 PM |
| 183 | Bike Lanes | 2/13/2022 6:45 PM |
| 184 | bike lanes | 2/13/2022 12:58 PM |
| 185 | bicycle lanes | 2/13/2022 10:36 AM |
| 186 | Reroute trucks that barrel down Cocoanut and turn left on Blvd of Arts to head west | 2/13/2022 7:50 AM |
| 187 | Bicycle lanes, trees, and saffety. | 2/13/2022 12:31 AM |
| 188 | Please put an overhead walkway over Blvd of the Arts. Like NYC highline | 2/12/2022 5:49 PM |
| 189 | Multi-use trails and trees | 2/12/2022 1:50 PM |
| 190 | Bicycle lanes, pedestrian walkways | 2/12/2022 1:40 PM |
| 191 | Wider, better lit sidewalks. Seating areas. Outdoor dining opportunities. | 2/12/2022 1:17 PM |
| 192 | Bike lanes. You actually take your life in your hands biking on the Blvd of the arts. Whoever designed the (no) parking, no bike lanes, no sidewalk even wide enough for pedestrians, let alone bikers trying to not get hit by traffic or car doors opening, should be.....not allowed to design and build in the city. No future forethought. Shameful. | 2/12/2022 12:46 PM |
| 193 | Bike lane 6th is too narrow | 2/12/2022 12:40 PM |
| 194 | bike lanes sidewalks, less cars | 2/12/2022 12:32 PM |
| 195 | Bike lanes | 2/12/2022 11:33 AM |
| 196 | Bicycle lanes | 2/12/2022 11:16 AM |
| 197 | Bike lanes, bike parking, walking to ample parking lots or street parking. Alternatively, bike and family friendly shuttle service to public parking and transit. | 2/12/2022 11:04 AM |

Boulevard of the Arts & 10th Street Complete Streets Survey

| | | |
|-----|--|--------------------|
| 198 | Bus shelters are needed | 2/12/2022 10:31 AM |
| 199 | Bicycle lanes | 2/12/2022 10:27 AM |
| 200 | More walkable. Cars speed on Central. All central intersections should have 4 way stop signs. It is dangerous crossing streets in the Rosemary District. | 2/12/2022 10:00 AM |
| 201 | Pedestrian bridges on BOTA and 10th Street facilitating pedestrian access to the Rosemary District. | 2/12/2022 9:57 AM |
| 202 | bicycle lanes | 2/12/2022 9:23 AM |
| 203 | bike lanes and curb cuts. | 2/12/2022 9:21 AM |
| 204 | Bike lanes with bollard barriers! | 2/12/2022 8:47 AM |
| 205 | Bicycle lanes, bus shelters, decreased speed limit and speed bumps | 2/12/2022 8:06 AM |
| 206 | Don't know | 2/11/2022 10:08 PM |
| 207 | protected bike lanes, bus shelters, traffic calming measures, street trees | 2/11/2022 5:18 PM |
| 208 | Bicycle lane separated from road traffic, increased public seating, sidewalk and pedestrian space separated from unpleasant and un-walkable roadways | 2/11/2022 3:58 PM |
| 209 | bicycle lanes | 2/11/2022 12:17 PM |
| 210 | Isolated pedestrian and bicycle path. | 2/11/2022 11:40 AM |
| 211 | Bike lanes and bus shelters | 2/11/2022 11:32 AM |
| 212 | BIKE LANES, MOBILITY HUBS AS NODES | 2/11/2022 11:32 AM |
| 213 | A separate bike trail/transit lane | 2/11/2022 11:31 AM |
| 214 | cleaning up the homeless. I always walk on this street but what deters me is my safety. If shops open up it will help gentrify it and make it more safe. | 2/11/2022 11:28 AM |
| 215 | Bike lanes, bike parking | 2/11/2022 11:27 AM |
| 216 | Dedicated Bike lanes physically separated from motorized vehicle traffic. | 2/11/2022 7:41 AM |
| 217 | bicycle lanes | 2/10/2022 11:17 AM |
| 218 | crosswalks, bicycle lanes, please design with the mindset that cars are the least important thing on the road! The future is not cars. Parking garages take up valuable space for community development and are an eyesore. We have an opportunity to correct the mistakes of our big city neighbors that think to much of car travel. We should create an environment of walkability. | 2/10/2022 8:28 AM |
| 219 | bike lanes, bus shelters, more flashing things on the road for nighttime | 2/10/2022 12:32 AM |
| 220 | Bicycle lanes, speed tables | 2/9/2022 4:20 PM |
| 221 | I wish we had more safe and dedicated bike and walking paths throughout the downtown area. | 2/9/2022 4:13 PM |
| 222 | Divided bike / walking trails. Biking on the road is unpleasant/dangerous. | 2/9/2022 2:42 PM |
| 223 | bike lanes, proper light water fountains | 2/9/2022 12:01 PM |
| 224 | bicycle lanes, bus shelters, electric car charging stations, benches. | 2/9/2022 12:24 AM |
| 225 | bike lanes | 2/8/2022 6:01 PM |
| 226 | Lighting and accessibility | 2/8/2022 2:34 PM |
| 227 | bike lane, designated taxi/uber pickup points | 2/8/2022 2:00 PM |
| 228 | Bicycle lanes, Bench seating for public transit | 2/8/2022 1:52 PM |
| 229 | More bus shelters would primarily draw the homeless (who congregate, defecate, and litter) there all day long as they do along Coconut Street. | 2/8/2022 12:58 PM |
| 230 | Bike Lanes | 2/8/2022 9:49 AM |

Boulevard of the Arts & 10th Street Complete Streets Survey

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| 231 | Bike Lanes with 2-sided curb protection | 2/8/2022 9:29 AM |
| 232 | Bike lanes please! | 2/8/2022 8:46 AM |
| 233 | Protected bike lanes that promote safe mobility as an alternative to vehicles | 2/8/2022 8:35 AM |
| 234 | Bike lanes Walking shaded sidewalks with trees | 2/8/2022 8:19 AM |
| 235 | Less space devoted exclusively to cars and more to pedestrians or bikes. (brick pavers, bulb outs, that type of thing) | 2/8/2022 8:03 AM |
| 236 | Above ideas | 2/8/2022 7:43 AM |
| 237 | living space the is affordable for your service people, they wii create a good culture and respect where they serve and live. the biltmore estate understood the staff living on property | 2/8/2022 6:19 AM |
| 238 | Bicycle lanes | 2/8/2022 3:38 AM |
| 239 | Bike lanes, bus shelters | 2/7/2022 11:19 PM |
| 240 | Bicycle lanes | 2/7/2022 10:03 PM |
| 241 | Water filler. Bike repair. Shade. Seating. | 2/7/2022 9:56 PM |
| 242 | Easy access across 41.. Bike lanes all the way to Gillespie | 2/7/2022 9:15 PM |
| 243 | bike lanes that are protected with a barrier. Traffic lights that favor pedestrians. Pedestrian walkways at every block (ex. at Florida and BOTA, there is no stop sign. Hard to cross). Dog park (small and large). | 2/7/2022 8:07 PM |
| 244 | Bicycle lanes, wider walking areas | 2/7/2022 6:58 PM |
| 245 | Bicycle racks, don't need bike lanes. | 2/7/2022 6:28 PM |
| 246 | Bike lane | 2/7/2022 4:22 PM |
| 247 | Bicycle lanes, bicycle racks | 2/7/2022 1:52 PM |
| 248 | green areas, protection from vehicular traffic | 2/7/2022 1:44 PM |
| 249 | bicycle lanes, reprimanding bus drivers that speed through town | 2/7/2022 1:44 PM |
| 250 | Change the "use" factor for Blvd of the Arts by installing mechanisms that will significantly reduce car speeds and frequency of traveling through the area | 2/7/2022 1:01 PM |
| 251 | Bicycle lanes, narrowing traffic lanes to increase sidewalks and trees,making it a route for street cars. | 2/7/2022 12:53 PM |
| 252 | Bike lanes | 2/7/2022 12:40 PM |
| 253 | flashing 4-way stops/crossings | 2/7/2022 12:18 PM |
| 254 | I do not want anymore access. We are a nice quiet neighborhood. Do NOT want more people | 2/7/2022 12:13 PM |
| 255 | bicycle lanes that are safe from moving traffic or parked vehicles, sidewalks wide enough for 2-way ped and with no obstacles such as utilities structures in the sidewalk which makes for not enough room for wheelchairs/walkers (this is a city-wide issue) - also important to make sure visibility for motorists when turning out onto to BOTA i.e. from Florida Ave behind Cityside is difficult due to the parking spots in front of Cafe Arts and the other bldgs along there--larger pick-up trucks/vans when parked make it impossible to see coming traffic in order to pull out to turn. | 2/7/2022 11:40 AM |
| 256 | Bicycle lanes. However, this needs to be coordinated with the development on 10th street and other city plans to route bicycles off of Fruitville. | 2/7/2022 11:29 AM |
| 257 | safe space to travel...bike lanes. visible | 2/7/2022 11:24 AM |
| 258 | Bicycle lanes | 2/7/2022 10:33 AM |
| 259 | Protected bike lanes | 2/7/2022 10:13 AM |
| 260 | Bollards, painting bicycle lanes to be different to the asphalt, traffic calming such as narrower lanes and speed tables. | 2/7/2022 9:25 AM |

Boulevard of the Arts & 10th Street Complete Streets Survey

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| 261 | Walking sidewalks and lights | 2/7/2022 8:44 AM |
| 262 | Bike lanes | 2/7/2022 8:03 AM |
| 263 | bike lanes; bridge over 41 | 2/7/2022 7:53 AM |
| 264 | Bike lanes | 2/7/2022 7:30 AM |
| 265 | Bike lanes, bike kiosks, bike parking, bus shelters, and trees that provide shade. | 2/7/2022 7:30 AM |
| 266 | Bike lanes and a bridge connection somewhere over Tamiami trail. | 2/7/2022 7:30 AM |
| 267 | cycling | 2/7/2022 7:28 AM |
| 268 | Pedestrian and bicycle crossings above the streets | 2/7/2022 7:08 AM |
| 269 | Pedestrian bridge | 2/7/2022 2:48 AM |
| 270 | Bicycle lanes | 2/7/2022 12:07 AM |
| 271 | Pedestrian bridge | 2/6/2022 8:51 PM |
| 272 | Painted bike lanes, not separated bike lanes | 2/6/2022 8:49 PM |
| 273 | bike lanes, what about school drop off traffic? | 2/6/2022 7:52 PM |
| 274 | Protected bike lanes | 2/6/2022 6:54 PM |
| 275 | Bicycle lanes, pedestrian lanes | 2/6/2022 5:30 PM |
| 276 | PROTECTED bike lanes, PROTECTED bike lanes, PROTECTED bike lanes | 2/6/2022 5:20 PM |
| 277 | Bike lanes | 2/6/2022 3:35 PM |
| 278 | Bicycle lanes | 2/6/2022 2:51 PM |
| 279 | Bike lanes, public rest rooms | 2/6/2022 1:09 PM |
| 280 | A well designed pedestrian overpass, designed to accommodate bikes, and micromobility modes | 2/6/2022 12:53 PM |
| 281 | Protected bike lanes | 2/6/2022 12:51 PM |
| 282 | Bike lanes and sidewalks | 2/6/2022 12:10 PM |
| 283 | Bicycle lanes | 2/6/2022 11:11 AM |
| 284 | bike lane | 2/6/2022 10:37 AM |
| 285 | consider homeless problem in all planning | 2/6/2022 10:09 AM |
| 286 | Bicycle lanes that work. | 2/6/2022 10:05 AM |
| 287 | bus shelters | 2/6/2022 9:32 AM |
| 288 | bike lanes and car speed control | 2/6/2022 9:25 AM |
| 289 | Bike lanes | 2/6/2022 9:15 AM |
| 290 | Safety and ease of access for pedestrians, cyclists, merchants | 2/6/2022 9:11 AM |
| 291 | Separate bicycle lanes away from walkers | 2/6/2022 8:44 AM |
| 292 | bicycle lanes, bus lane at stops | 2/6/2022 8:39 AM |
| 293 | Bike lanes | 2/6/2022 8:15 AM |
| 294 | Bicycle trail | 2/6/2022 7:52 AM |
| 295 | Bike lanes throughout connecting to it. | 2/6/2022 7:23 AM |
| 296 | protected bike lane. | 2/6/2022 6:55 AM |
| 297 | Protected bike lane.Bike parking | 2/6/2022 6:45 AM |
| 298 | Pedestrian overpass | 2/6/2022 4:17 AM |

Boulevard of the Arts & 10th Street Complete Streets Survey

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| 299 | Traffic signal lights and more traffic signs before and around Blvd of the Arts. The Street Lamps/Lights should be higher so there is more light spread at night. It's too dark around Blvd of the Arts at night. | 2/6/2022 12:11 AM |
| 300 | Bike lanes. Bridge over 41. | 2/5/2022 10:52 PM |
| 301 | Bike lanes | 2/5/2022 9:56 PM |
| 302 | Secured protective lanes for bikes. Easement between sidewalk and street to accommodate trees and give safety to sidewalk activities. | 2/5/2022 9:35 PM |
| 303 | bike lanes, shaded areas for residents to get out of the hot sun, areas for the food trucks and more parking | 2/5/2022 8:48 PM |
| 304 | Bus shelters | 2/5/2022 8:38 PM |
| 305 | Scooter golf carts | 2/5/2022 8:09 PM |
| 306 | bike lanes | 2/5/2022 7:27 PM |
| 307 | Not sure, maybe bicycle lanes, but the city allowed buildings to be right up to the sidewalk, so there isn't much room for more than one lane each side (this is between Cocanut & 41). | 2/5/2022 5:46 PM |
| 308 | bicycle lanes | 2/5/2022 5:25 PM |
| 309 | Pedestrian bridge over 41 | 2/5/2022 5:14 PM |
| 310 | Wide sidewalks, trees for shade, benches to rest, green areas | 2/5/2022 4:15 PM |
| 311 | That's your job | 2/5/2022 4:11 PM |
| 312 | trees, shade, ways to reduce heat island effects, bus shelters | 2/5/2022 4:05 PM |
| 313 | Sheltered rest areas | 2/5/2022 4:01 PM |
| 314 | Bus shelters pedestrian bridge | 2/5/2022 4:00 PM |
| 315 | bicycle lanes | 2/5/2022 3:53 PM |
| 316 | bicycle lanes | 2/5/2022 3:48 PM |
| 317 | bike lanes that have a curb or protection so cars cant come in them. | 2/5/2022 3:44 PM |
| 318 | bike lane | 2/5/2022 3:37 PM |
| 319 | Bike lanes | 2/5/2022 3:26 PM |
| 320 | Bike | 2/5/2022 3:23 PM |
| 321 | bicycle lanes | 2/5/2022 3:07 PM |
| 322 | Protected lanes, bike shares, stations for rest covered | 2/5/2022 3:07 PM |
| 323 | Dedicated bike lanes, not just lines painted on the road, bus shelters with shade and benches | 2/5/2022 3:05 PM |
| 324 | bikeshare stations | 2/5/2022 2:46 PM |
| 325 | A parking lot in the Rosemary District so that people fRT om outside the immediate area would be able to come to the area restaurants and businesses | 2/5/2022 2:32 PM |
| 326 | Bike lanes, | 2/5/2022 2:27 PM |
| 327 | Wider sidewalks, shade trees | 2/5/2022 2:24 PM |
| 328 | Wider Sidewalks- stop building so close to the street | 2/5/2022 2:14 PM |
| 329 | Bicycle lane | 2/5/2022 2:13 PM |
| 330 | bicycle lanes, wide sidewalks | 2/5/2022 2:11 PM |
| 331 | Protected bike lanes with trees or planters between bikes and any car activity are a must. Would be great to have a omnibus running around the city with stops convenient to the Bay, Marina, St. Armands, downtown...maybe Siesta Key. | 2/5/2022 2:11 PM |

Boulevard of the Arts & 10th Street Complete Streets Survey

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| 332 | Cycling: Protected bicycle lanes. Micro-mobility (scooter/ebike) parking/access/corrals. Adequate bicycle parking. Public Transit: a total overhaul of the transit system to make it come timely and be viable...but that's WAY bigger than this project. | 2/5/2022 2:10 PM |
| 333 | Bicycle lanes | 2/5/2022 1:57 PM |
| 334 | Bicycle lanes definitely | 2/5/2022 1:55 PM |
| 335 | bicycle lanes | 2/5/2022 1:47 PM |
| 336 | Safe bicycle lanes | 2/5/2022 1:33 PM |
| 337 | Bicycle lane easy and safe Crossings | 2/5/2022 1:19 PM |
| 338 | More dedicated bike lanes and trails. | 2/5/2022 1:13 PM |
| 339 | I prefer bike paths over bike lanes where feasible of course. | 2/5/2022 1:09 PM |
| 340 | bicycle lanes, bus shelters | 2/5/2022 1:04 PM |
| 341 | And benches | 2/5/2022 1:01 PM |
| 342 | Sidewalks and safety | 2/5/2022 1:00 PM |
| 343 | Covered covered covered covered bus shelters. It is hot and it rains here. | 2/5/2022 12:55 PM |
| 344 | bicycle lanes | 2/5/2022 12:45 PM |
| 345 | an overhead pedestrian walkway | 2/5/2022 12:23 PM |
| 346 | Bike lanes | 2/5/2022 12:22 PM |
| 347 | Bicycle Lanes, Bus-stops with shelters that are widened to provide vehicle traffic to continue by. | 2/5/2022 12:21 PM |
| 348 | Bicycle lanes, wide sidewalks, park like inviting design.....NO public transportation...BLVD of the Arts should be pedestrian friendly and green ... no polluting buses or trucks | 2/5/2022 12:16 PM |
| 349 | Designated lanes, wide side walks, pedestrian overpass to safely get to the bay park and new pac | 2/5/2022 12:09 PM |
| 350 | Bike lanes, trolley or bus stops | 2/5/2022 12:07 PM |
| 351 | Bike lanes | 2/5/2022 12:04 PM |
| 352 | bike lanes | 2/5/2022 11:56 AM |
| 353 | Dedicated bike lanes Small public transport options | 2/5/2022 11:52 AM |
| 354 | Bicycle lanes | 2/5/2022 11:50 AM |
| 355 | a pedestrian bridge to the Bay Front over Tamiami Trail. Even with the walk light it is unpleasant to cross Tamiami Trail and the light wait times are very long. I cross Tamiami Trail often to go to the park and the symphony hall and Van Wezel. | 2/5/2022 11:33 AM |
| 356 | Bike lanes with barriers from traffic | 2/5/2022 11:30 AM |
| 357 | No opinion | 2/5/2022 11:26 AM |
| 358 | Comprehensive approach...All improvements that evolve from good urban planning | 2/5/2022 11:23 AM |
| 359 | NO motorized vehicles except public transit. | 2/5/2022 11:21 AM |
| 360 | Bike lanes, sufficient lighting | 2/5/2022 11:16 AM |
| 361 | Bicycle lanes | 2/5/2022 11:14 AM |
| 362 | Parking and bicycle lanes | 2/5/2022 11:03 AM |
| 363 | Bike lanes, bus shelters | 2/5/2022 10:59 AM |
| 364 | Proper bike lanes with physical barriers between bikers and cars. | 2/5/2022 10:53 AM |
| 365 | Blvd of the Arts is a bypass of Fruitville. Bike lanes and cross walks are very good but traffic flow is important. | 2/5/2022 10:53 AM |

Boulevard of the Arts & 10th Street Complete Streets Survey

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| 366 | Bus shelters are a hard no because of the homeless problems we have in the rosemary district. They loiter there and cause problems. The more bicycle lanes the better. | 2/5/2022 10:46 AM |
| 367 | bike lane | 2/5/2022 10:42 AM |
| 368 | bike lanes, bus shelters, wider side walks | 2/5/2022 10:41 AM |
| 369 | Green and infrastructure for Stormwater | 2/5/2022 10:38 AM |
| 370 | Bike lanes and wide sidewalks | 2/5/2022 10:26 AM |
| 371 | Protected bicycle lanes PLEASE! | 2/5/2022 10:15 AM |
| 372 | Increase and improve all walking and biking areas | 2/5/2022 10:09 AM |
| 373 | There are bike lanes all over Sarasota and they are virtually unused. As a pedestrian, bikes are my greatest fear. They need to be on the road, not side walks ad that rule needs to be enforced. | 2/5/2022 10:05 AM |
| 374 | Bike lanes | 2/5/2022 10:04 AM |
| 375 | protected bike lanes, plenty of secure bicycle parking, benches/rain shelters | 2/5/2022 10:03 AM |
| 376 | bike lanes, pedestrian bridge | 2/5/2022 10:02 AM |
| 377 | Building a pedestrian overpass over 41. | 2/5/2022 10:00 AM |
| 378 | Bicycle lanes, traffic calming for vehicles, sharrows, signage | 2/5/2022 9:57 AM |
| 379 | specialized lanes bus shelters | 2/5/2022 9:56 AM |
| 380 | Bike lanes Safe walking access across Fruitville road Need more parking | 2/5/2022 9:48 AM |
| 381 | Safe pedestrian and bicycle crossings over Tamiami Trail. | 2/5/2022 9:47 AM |
| 382 | Separate bike paths and safe pedestrian walkways. | 2/5/2022 9:45 AM |
| 383 | Bike lanes | 2/5/2022 9:39 AM |
| 384 | Low speed limits | 2/5/2022 9:38 AM |
| 385 | Bike lanes do not work because drivers do not pay attention or slowdown. | 2/5/2022 9:38 AM |
| 386 | Bicycle lanes. Pedestrian-safe gathering areas. | 2/5/2022 9:37 AM |
| 387 | Safety measures for riders and walkers. | 2/5/2022 9:35 AM |
| 388 | No bus routes go down the Blvd. But a nice bus shelter would be good on Cocoanut would be good. | 2/5/2022 9:32 AM |
| 389 | Bicycle lane | 2/5/2022 9:32 AM |
| 390 | Dedicated bike and separate pedestrian lanes | 2/5/2022 9:31 AM |
| 391 | Widening the road | 2/5/2022 9:30 AM |
| 392 | Bike lanes | 2/5/2022 9:26 AM |
| 393 | bike lanes, bus shelters, public space for gathering | 2/5/2022 9:24 AM |
| 394 | Bus shelters | 2/5/2022 9:21 AM |
| 395 | Bike lanes Safety measures like marked, lighted crosswalks | 2/5/2022 9:20 AM |
| 396 | Pedestrian bridges | 2/5/2022 9:15 AM |
| 397 | wide bicycle lanes | 2/5/2022 9:13 AM |
| 398 | Bike lane | 2/5/2022 9:13 AM |
| 399 | Bicycle lanes | 2/5/2022 9:12 AM |
| 400 | Bicycle trail or MURT | 2/5/2022 9:11 AM |
| 401 | Bike lanes...wider sidewalks or more walkways | 2/5/2022 9:05 AM |

Boulevard of the Arts & 10th Street Complete Streets Survey

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| 402 | Bicycles do not belong on the street, vehicles belong on the roadways. They belong on the sidewalk. | 2/5/2022 8:51 AM |
| 403 | Designated bike lanes please!!! Downtown St Petersburg has done a fantastic job with designated bike lanes through their downtown. This promotes safety #1 and with a safer way to bike, more people would use it, less cars on the road. Also with downtown having rental bikes, having the safer bike lanes makes that more appealing for people to use, feel safer. | 2/5/2022 8:06 AM |
| 404 | Bicycle lanes | 2/5/2022 7:54 AM |
| 405 | Bike lanes | 2/5/2022 7:46 AM |
| 406 | Bike lanes | 2/5/2022 7:36 AM |
| 407 | Road marking, bike lanes, on demand traffic lights for crossing road | 2/5/2022 7:27 AM |
| 408 | Bicycle lane | 2/5/2022 7:23 AM |
| 409 | Bicycle lanes | 2/5/2022 6:27 AM |
| 410 | Narrow street and put green buffer between cars and pedestrians and bicyclists | 2/5/2022 6:14 AM |
| 411 | Barrier bike lanes | 2/5/2022 5:52 AM |
| 412 | protected bicycle lanes | 2/5/2022 1:49 AM |
| 413 | Certainly, bicycle lanes! Better yet, eliminate auto traffic entirely and create closed off streets like in Holland, Germany, etc. | 2/5/2022 12:43 AM |
| 414 | Well defined bicycle lanes. | 2/4/2022 9:58 PM |
| 415 | Designated bike lanes with a buffer between lane and traffic. All way stop for pedestrian crossings. | 2/4/2022 9:41 PM |
| 416 | bike lanes | 2/4/2022 9:34 PM |
| 417 | Bike lanes | 2/4/2022 9:33 PM |
| 418 | bike lanes | 2/4/2022 9:10 PM |
| 419 | Law enforcement of drivers stopping in the intersection at 41/Blvd Of The Arts or a Traffic Circle installed at 41/Blvd Of The Arts. | 2/4/2022 4:36 PM |
| 420 | If you put in bike lanes you MUST enforce bike traffic adherence to traffic rules. As it is now they run lights, cut across lanes, slow traffic and in general believe the road is theirs through their exhaled sense of superiority to drivers. | 2/4/2022 4:28 PM |
| 421 | Protected bike lanes separated from vehicular traffic with preferred flow throughout the City, not just in the project area. The ideal would be to create an environment for bicyclists to feel safe commuting to and from work (and promoting that as a primary form of transportation). Florida is one of the top UNSAFE places to ride bikes and walk in the COUNTRY! We need to change that. Complete Streets does not fully address this. The engineer must go above and beyond the guidelines to make this happen. | 2/4/2022 4:03 PM |
| 422 | Bike lanes and bus shelters | 2/4/2022 3:34 PM |
| 423 | Buffered/Separated bike lanes that have physical separation perhaps via landscaping in between the bike lane and car lanes along with more public bike storage/racks to lock up bikes. | 2/4/2022 3:21 PM |
| 424 | Bicycle lanes and bike racks. | 2/4/2022 3:16 PM |
| 425 | Bike lanes | 2/4/2022 2:07 PM |
| 426 | NO bus shelters. Took loud and buses take away from the peace and calm. I hope the motorized cycle program has bike pickup/drop off in this area. | 2/4/2022 8:52 AM |
| 427 | Bicycle lanes | 2/4/2022 8:34 AM |
| 428 | Bus shelters so SCAT does not block traffic during route. | 2/4/2022 8:29 AM |
| 429 | Ped overpass over 41 | 2/4/2022 8:27 AM |

Boulevard of the Arts & 10th Street Complete Streets Survey

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| 430 | Definitely bicycle lanes | 2/3/2022 10:51 PM |
| 431 | Safe bike lanes, and make room for all the E-gizmos. | 2/3/2022 10:26 PM |
| 432 | bicycle lanes | 2/3/2022 6:59 PM |
| 433 | Sheltered Bus Stations, Greenery, painted bike lanes | 2/3/2022 4:54 PM |
| 434 | bicycle lanes | 2/3/2022 4:50 PM |
| 435 | bike lanes | 2/3/2022 4:39 PM |
| 436 | Safe lighted pitstops for those waiting 4 transit. | 2/3/2022 4:16 PM |
| 437 | Bicycle lanes, trolley stops (if we can ever get that mode of public transportation) | 2/3/2022 3:47 PM |
| 438 | Roundabout at Boulevard of the Arts would improve mobility and safety. | 2/3/2022 2:05 PM |
| 439 | Bus Shelters, either freestanding or built into new buildings. | 2/3/2022 2:03 PM |
| 440 | Bike lanes with ample clearance, light-up signage signaling bikers on the road | 2/3/2022 1:49 PM |
| 441 | Bicycle lanes | 2/3/2022 1:13 PM |
| 442 | Bike lanes!!!! | 2/3/2022 12:13 PM |
| 443 | no buses or bus shelters, they are only used by the homeless | 2/3/2022 11:15 AM |
| 444 | bicycle lanes and well designed crossings | 2/3/2022 11:14 AM |
| 445 | bike lanes | 2/3/2022 8:53 AM |
| 446 | Bike lanes | 2/3/2022 8:50 AM |
| 447 | Bike lanes | 2/3/2022 8:45 AM |
| 448 | wider bike lanes, wider sidewalks, green space, better crossings | 2/3/2022 8:12 AM |
| 449 | Bike lanes, shelters for inclement weather, bike racks | 2/3/2022 8:08 AM |
| 450 | green bike lanes, bus shelter, aesthetic surroundings, appropriate street and sidewalk lighting levels, and implement traffic calm technology | 2/3/2022 7:19 AM |
| 451 | Bicycle lanes and a more consistent bus schedule. | 2/3/2022 6:28 AM |
| 452 | No additional amenities needed. | 2/3/2022 3:47 AM |
| 453 | bicycle lanes | 2/2/2022 11:52 PM |
| 454 | Bicycle lanes, pedestrian walkway, dog poop bag dispensers. | 2/2/2022 11:36 PM |
| 455 | I live in South Gate and regularly bike downtown, but have been apprehensive about trying to cross Fruitville Road, and don't know what good and safe routes are to cross it. | 2/2/2022 10:38 PM |
| 456 | Bike lanes, bus shelters, bike fixit stations (see the ones at Benderson park) | 2/2/2022 8:41 PM |
| 457 | Please no benches or bus shelters. These are just going to become homeless hangouts and make the area less appealing. | 2/2/2022 8:40 PM |
| 458 | Bike lanes, lighted crosswalks | 2/2/2022 8:22 PM |
| 459 | I believe protected bicycle lanes (separated by a median or comparable vegetated space) would be great, but I recognize it's limited space. Generally, narrowing the streets would be a good idea I believe. | 2/2/2022 8:15 PM |
| 460 | bicycle lanes a most, seating area | 2/2/2022 4:48 PM |
| 461 | bicycle lanes protected by some sort of raised curb | 2/2/2022 4:29 PM |
| 462 | Bike lanes and sheltered bike racks. | 2/2/2022 4:19 PM |
| 463 | Bicycle Lanes connecting to Legacy Trail | 2/2/2022 4:18 PM |
| 464 | Bicycle lanes | 2/2/2022 4:06 PM |

Boulevard of the Arts & 10th Street Complete Streets Survey

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| 465 | Feeling safe. Removal of homeless that tend to accost patrons of the downtown area. Not enough is done to keep them out of the away. Many guests I have cannot believe that Sarasota does not do a better job of controlling nuisance. A better job is done in Baltimore, DC and even Philadelphia. | 2/2/2022 2:58 PM |
| 466 | bicycle lanes | 2/2/2022 2:29 PM |
| 467 | stop building roads and build more houses | 2/2/2022 2:19 PM |
| 468 | Bike lanes | 2/2/2022 1:43 PM |
| 469 | Protected bike lanes. Sidewalk widening for a shared use path. Secure bike parking or storage. | 2/2/2022 1:43 PM |
| 470 | Bike parking area | 2/2/2022 1:40 PM |
| 471 | super wide sidewalks wide bike lanes ferry service from Bayfront and Marina | 2/2/2022 1:33 PM |
| 472 | If you want to promote cycling anywhere in the City then move the cycling paths away from the streets or at the very least physically separate them. Not only does it reduce the actual danger but it significantly reduces the perceived danger and associated stress. Look to how efficient the streets of Columbia Maryland are in moving both cyclists and vehicles which is achieved by not having the two anywhere near each other. | 2/2/2022 1:28 PM |
| 473 | unknown | 2/2/2022 1:02 PM |
| 474 | Bicycle lanes, raised crossing areas, lower speed limit, more sidewalks | 2/2/2022 12:21 PM |
| 475 | wide bicycle lanes | 2/2/2022 11:44 AM |
| 476 | Bike lanes and trees for shaded walking | 2/2/2022 11:01 AM |
| 477 | Minimize/eliminate traffic of vehicles. Make blvd a walking and bicycle street only. | 2/2/2022 10:44 AM |
| 478 | Bicycle lanes. | 2/2/2022 10:37 AM |
| 479 | bike lane | 2/2/2022 10:33 AM |
| 480 | Bike lanes in road. | 2/2/2022 10:32 AM |
| 481 | Bicycle lanes | 2/2/2022 10:29 AM |
| 482 | bicycle lanes bus shelters | 2/2/2022 9:36 AM |
| 483 | bike rack, bike lanes, bike & scooter sharing. Landscapping | 2/2/2022 9:34 AM |
| 484 | Please do not add bus shelters, these are not appropriately used throughout Sarasota | 2/2/2022 8:54 AM |
| 485 | Bike lanes | 2/2/2022 8:47 AM |
| 486 | frequent, accessible transit stops; protected bike lanes; | 2/2/2022 8:09 AM |
| 487 | All Florida bus stops should have covered shelters. Bike lanes should be the next priority. We need to get bikes off of sidewalks in Sarasota. | 2/2/2022 7:53 AM |
| 488 | Bike lanes, bike parking, more area bus service | 2/2/2022 7:35 AM |
| 489 | We bike on Blvd of the Arts all the time but there are a lot of cars - more pedestrian areas and wider bike lanes, less parking on the streets would help. | 2/2/2022 7:34 AM |
| 490 | Bike lanes & rideshare pick up points | 2/2/2022 7:30 AM |
| 491 | Bike lanes | 2/2/2022 7:19 AM |
| 492 | Stands to allow bicycles to stop and grab a coffee or look in a shop. More benches — the more benches the better for pedestrians of all kinds. | 2/2/2022 7:09 AM |
| 493 | Cut out for bus lane | 2/2/2022 6:53 AM |
| 494 | Well defined bicycle lanes separated by plastic stantions | 2/2/2022 5:37 AM |
| 495 | Bike lanes | 2/2/2022 5:07 AM |
| 496 | Bus shelters with seats | 2/2/2022 5:00 AM |

Boulevard of the Arts & 10th Street Complete Streets Survey

| | | |
|-----|---|-------------------|
| 497 | Shade and creative seating in form of public art and a pedestrian overpass bridge to the bay park | 2/2/2022 12:50 AM |
| 498 | Bike paths (not lanes). Paths are safer and out of traffic unlike lanes | 2/2/2022 12:48 AM |
| 499 | Small predestined cycle carriers | 2/2/2022 12:19 AM |
| 500 | Protected bike lanes, stripes don't do the job; bike racks | 2/1/2022 11:23 PM |
| 501 | Bike lanes | 2/1/2022 11:05 PM |
| 502 | None | 2/1/2022 10:51 PM |
| 503 | Bike lanes | 2/1/2022 10:33 PM |
| 504 | Dedicated Bike lanes!! | 2/1/2022 10:31 PM |
| 505 | better parking and bike lanes | 2/1/2022 10:21 PM |
| 506 | Shade trees | 2/1/2022 9:52 PM |
| 507 | Bicycle lanes | 2/1/2022 9:50 PM |
| 508 | Why not support the cars? | 2/1/2022 9:46 PM |
| 509 | I believe this area of Sarasota would benefit from wider sidewalks, bike lanes, trees, and possibly bus shelters too. | 2/1/2022 9:28 PM |
| 510 | EV charging. Better pedestrian access from bay front to Rosemary district | 2/1/2022 9:21 PM |
| 511 | Bike lanes and bus shelters | 2/1/2022 9:20 PM |
| 512 | Bike lanes, dedicated side walks, bus shelters, bike shares | 2/1/2022 9:10 PM |
| 513 | Actually having public transportation that works and does not take more time than driving in personal vehicle | 2/1/2022 9:07 PM |
| 514 | Benches | 2/1/2022 8:52 PM |
| 515 | Bike and bus priority lanes, safe and comfortable bus shelters.. | 2/1/2022 8:43 PM |
| 516 | Bike lanes and other safety measures | 2/1/2022 8:31 PM |
| 517 | Bicycle lanes. | 2/1/2022 8:30 PM |
| 518 | Bicycle lanes | 2/1/2022 8:12 PM |
| 519 | Bike lanes, bike racks, sidewalk lighting, bus shelters w/overhang | 2/1/2022 8:06 PM |
| 520 | Bicycle lanes, better lighting | 2/1/2022 8:05 PM |
| 521 | Lots of side walks for walking, bike lane, lot landscaping and green spaces, tear down old dilapidated buildings so it isn't pretty on one corner and scary on the next. Have spaces for concerts and festivals. The area should have a park similar to the one at hart's landing And have FREE Parking instead of parking police who I've witnessed marking tires and passing out tickets. Not a good impression for tourists! Can't wait to see what the City does, next it should tart cleaning up south gate, Arlington and all of the old parts of Sarasota. You are making areas around the water and downtown nice and leaving the rest looking less desirable. I have lived here 36 years and recently left for a few to take care of my mother. I'm back and honestly, if it were not for downtown and our lovely beaches, I wouldn't have moved back because the rest of SRQ is looking dirty and nasty. | 2/1/2022 7:57 PM |
| 522 | Something that won't take away from our ability to drive down 6th. It is a way for us to get through the neighborhood. | 2/1/2022 7:51 PM |
| 523 | LIGHTED CROSSWALKS!!! Like the ground is lit up. They do this in every country except the US. Covered kit bus stops - creative ones like in Tucson Arizona. Also pedestrian bridges such as the diamondback bridge in Tucson, AZ. | 2/1/2022 7:49 PM |
| 524 | Bike lane | 2/1/2022 7:39 PM |
| 525 | Bicycle lanes | 2/1/2022 7:37 PM |

Boulevard of the Arts & 10th Street Complete Streets Survey

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|-----|---|------------------|
| 526 | -Cross walks like those in Marco island (they beep when people cross) -Natural shade like many palms -Transient Boat slips | 2/1/2022 7:19 PM |
| 527 | Wide Bike lanes separated from street | 2/1/2022 7:15 PM |
| 528 | Wide and barrier-lined bike/pedestrian dual purpose lanes. | 2/1/2022 6:41 PM |
| 529 | Wide sidewalks | 2/1/2022 6:29 PM |
| 530 | Travel across Tamiami is still dangerous with cars failing to stop. | 2/1/2022 6:27 PM |
| 531 | Bike lanes, bus shelters, ask transportation planners for the latest trends and innovation. | 2/1/2022 6:25 PM |
| 532 | Separate lane | 2/1/2022 6:08 PM |
| 533 | wide bike lanes | 2/1/2022 5:57 PM |
| 534 | lanes and places to lock up bikes | 2/1/2022 5:48 PM |
| 535 | Circle at 41 and BLVD of the Arts | 2/1/2022 5:39 PM |
| 536 | I see no need for bike lanes or bus shelters in the area. | 2/1/2022 5:39 PM |
| 537 | Bus shelters due to sun & rain should be a given. I am very interested in safe bicycle lanes. | 2/1/2022 5:35 PM |
| 538 | Bike Lanes with barriers rather than just lines, better bus stop areas | 2/1/2022 5:30 PM |
| 539 | Bicycle Lanes | 2/1/2022 5:23 PM |
| 540 | Bicycle parking racks | 2/1/2022 5:20 PM |
| 541 | Slower speeds on streets, traffic calming | 2/1/2022 5:20 PM |
| 542 | vehicular restrictions, there are plenty of alternate routes | 2/1/2022 5:18 PM |
| 543 | Bike lanes | 2/1/2022 5:17 PM |
| 544 | No bus shelters please there are enough homeless people hanging around | 2/1/2022 5:08 PM |
| 545 | Definitely bike lanes. Trolley that runs in the Rosemary district. Scooter, ebike and regular bike rentals. Pedestrian bridge going over 41!! | 2/1/2022 5:07 PM |
| 546 | bicycle lanes | 2/1/2022 5:05 PM |
| 547 | bike lanes and bus shelters. | 2/1/2022 5:03 PM |
| 548 | Parking for cars | 2/1/2022 4:50 PM |
| 549 | Bike lanes | 2/1/2022 4:41 PM |
| 550 | Bike lanes. Covered bus shelters. | 2/1/2022 4:32 PM |
| 551 | Please no public transit | 2/1/2022 4:25 PM |
| 552 | Bus shelter, bicycle lanes. | 2/1/2022 4:25 PM |
| 553 | Bike lanes | 2/1/2022 4:19 PM |
| 554 | Bicycle lanes | 2/1/2022 4:17 PM |
| 555 | Wide sidewalks, shaded areas, increased parking. | 2/1/2022 4:14 PM |
| 556 | The cars are not very respectful of bikes unfortunately, better bike lanes and more signs may help. | 2/1/2022 4:14 PM |
| 557 | walkable | 2/1/2022 4:13 PM |
| 558 | Bicycle lanes & bus access points | 2/1/2022 4:09 PM |
| 559 | Bicycle lanes separated from motor vehicles | 2/1/2022 4:06 PM |
| 560 | a row of parallel parking to shield in bikers and pedestrians | 2/1/2022 4:02 PM |
| 561 | bike lanes | 2/1/2022 3:54 PM |
| 562 | bus shelters and bike lanes, bike racks, public-use bikes, restrooms | 2/1/2022 3:44 PM |

Boulevard of the Arts & 10th Street Complete Streets Survey

| | | |
|-----|--|-------------------|
| 563 | Bike lanes, bike racks | 2/1/2022 3:42 PM |
| 564 | Improved look of bus shelter that would not 'invite' the homeless from camping out on them, as they have been. They can look more appealing (less dingy) to those who choose to ride the bus and need to wait | 2/1/2022 3:31 PM |
| 565 | covered bike spots for the summer rain, protected bike lanes. | 2/1/2022 3:30 PM |
| 566 | Safe access for walker / bikers | 2/1/2022 3:26 PM |
| 567 | Bike lanes, bike "corrals", bus shelters, benches. Pedestrian traffic signals, with countdown. | 2/1/2022 3:10 PM |
| 568 | Lanes | 2/1/2022 3:05 PM |
| 569 | Bicycle lanes Wide sidewalks | 2/1/2022 3:03 PM |
| 570 | Trees between ped/biker and cars On street parking Nice shaded trees in median- would have a preferred the original Boulevard design. | 2/1/2022 3:01 PM |
| 571 | Bicycle lanes | 2/1/2022 3:00 PM |
| 572 | Bike lanes, wide sidewalks, shade trees | 2/1/2022 2:54 PM |
| 573 | bicycle lanes bus shelters that are designed by artists to enhance the streetscape | 2/1/2022 2:52 PM |
| 574 | Bike lanes that actually connect to other bike lanes. Sometimes it feels as if they're built in tiny chunks that don't connect. It's super dangerous and only works if there is a place to bike (for more than three minutes at a time) often if feels like non-bikers plan the routes | 2/1/2022 2:44 PM |
| 575 | Eliminate the gas station at the US 41 intersection | 2/1/2022 2:32 PM |
| 576 | Bicycle lanes, Planters | 2/1/2022 2:31 PM |
| 577 | bicycles lanes and bicycle parking/locking "stations" | 2/1/2022 2:22 PM |
| 578 | Bike lane | 2/1/2022 2:16 PM |
| 579 | Safe bike lanes away from distracted drivers | 2/1/2022 1:47 PM |
| 580 | Bike lanes | 2/1/2022 1:45 PM |
| 581 | Protected bike lanes, bus shelter/benches, wider sidewalks | 2/1/2022 1:44 PM |
| 582 | Protected bike lanes. Clean streets. Limited street parking | 2/1/2022 1:27 PM |
| 583 | bike lanes, elevated pedestrian bridge over 41, bus shelters | 2/1/2022 1:17 PM |
| 584 | Bike lanes, pedestrian paths, street lights, crosswalks | 2/1/2022 1:06 PM |
| 585 | Wide and/or protected bike lanes. Wide sidewalks to encourage walking. They're currently far too narrow. | 2/1/2022 1:06 PM |
| 586 | Personally, I am totally over making everything being designed for cycling. Buses do not have a problem getting around here. Please get rid of the bus stop cages that are taken over by 7am by homeless people to camp out for the day. | 2/1/2022 1:02 PM |
| 587 | Bike lanes, shelter from sun, GREEN it up with Native trees & plant life for butterflies. Garbage bins for recyclables. ect. | 2/1/2022 1:02 PM |
| 588 | More shade over walking and biking lanes. The only appropriate public transit for BotA would be a trolley or jitney that circulated throughout the Downtown area -- and that would need sheltered stops. | 2/1/2022 12:55 PM |
| 589 | bike racks, clear pedestrian walkways | 2/1/2022 12:54 PM |
| 590 | Bicycle lanes, and safe walkways for pedestrians | 2/1/2022 12:41 PM |
| 591 | Defined bike lanes with a barrier, more bus stops, less parking | 2/1/2022 12:40 PM |
| 592 | Trees | 2/1/2022 12:34 PM |
| 593 | PROTECTED bike lanes | 2/1/2022 12:31 PM |

Boulevard of the Arts & 10th Street Complete Streets Survey

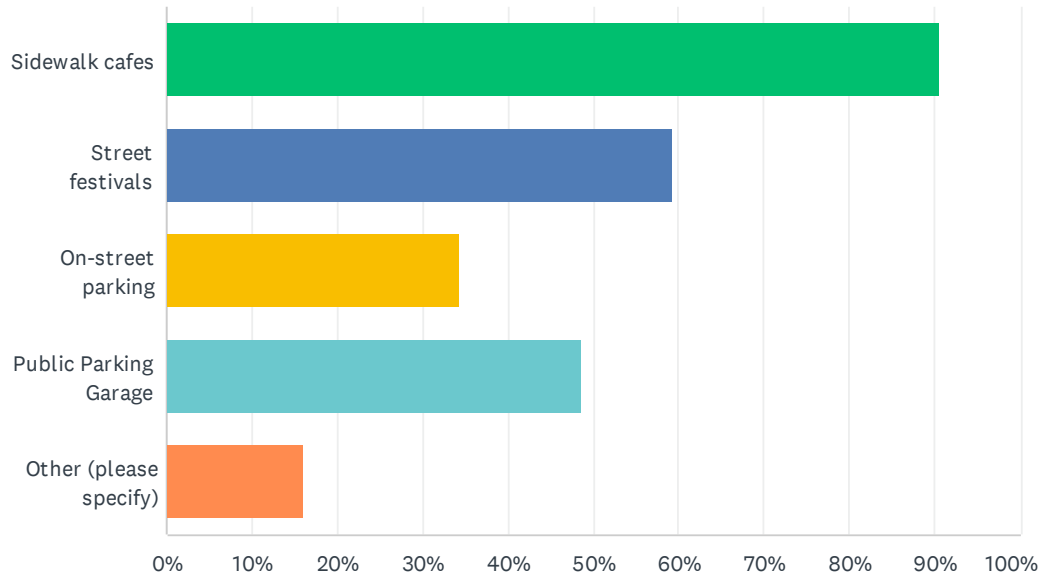
| | | |
|-----|--|-------------------|
| 594 | Bicycle lanes, bike racks | 2/1/2022 12:30 PM |
| 595 | bike lanes separate from car traffic. Wider sidewalks for Ada compliance. | 2/1/2022 12:22 PM |
| 596 | Shade trees and bike lanes. | 2/1/2022 12:19 PM |
| 597 | Pedestrian walk ways bike paths ...less cars | 2/1/2022 12:16 PM |
| 598 | Bike lanes | 2/1/2022 11:56 AM |
| 599 | BOTA will always be used for auto traffic and connection across 41 needs to be maintained for automobile | 2/1/2022 11:56 AM |
| 600 | Water fill stations pet stations cycle racks and lanes EDUCATION ON PEDESTRIAN RIGHTS | 2/1/2022 11:55 AM |
| 601 | Dedicated separate bike lanes with physical barriers from automobile traffic. Train or bus access. | 2/1/2022 11:55 AM |
| 602 | Police or security - that area is still a little but sketch | 2/1/2022 11:55 AM |
| 603 | bike lanes | 2/1/2022 11:52 AM |
| 604 | Bike lanes, traffic calming features, more pedestrian crossings | 2/1/2022 11:38 AM |
| 605 | Safe Bike lanes and pedestrian walkways, benches | 2/1/2022 11:36 AM |
| 606 | Highly visible bicycle lanes (particularly protected) would make a big difference towards promoting bike-ability in the Rosemary District and broader city. Currently, the bike lanes are not very wide, lines are faded, and they are inconsistent. Bus shelters are, unfortunately, a problem in the neighborhood. Given the location of the Salvation Army, bus shelters in the area are used more for illegal activity than waiting for public transit. As such, shelters actually discourage usage of public transit. I strongly recommend that bus shelters be replaced with bus stops to discourage vagrancy. | 2/1/2022 11:18 AM |
| 607 | Street trees, enforce front yard and commercial vehicle parking rules. (No infrastructure matters when cars are allowed to block sidewalks and commercial vehicles ruin the character of the neighborhood.) | 2/1/2022 11:16 AM |
| 608 | Bicycle lane with safety barrier, wide walking paths, tree and shrub plantings as barriers to traffic. | 2/1/2022 11:08 AM |
| 609 | Bike lane, bike rack | 2/1/2022 11:00 AM |
| 610 | Bike lanes/ Rent-a-bike | 2/1/2022 10:51 AM |
| 611 | PEDESTRIAN OVERPASS ON US 41!!!! | 2/1/2022 10:50 AM |
| 612 | Close off the street to traffic. Please build a pedestrian mall. Build a parking garage. | 2/1/2022 10:47 AM |
| 613 | Separate bikes lanes with curbs to keep cars out, traffic lights for pedestrians separate from traffic. | 2/1/2022 10:44 AM |
| 614 | Bicycle lanes, bicycle parking, street trees | 2/1/2022 10:37 AM |
| 615 | better marked bike lane...with BotA narrowed to allow for a bike lane | 2/1/2022 10:37 AM |
| 616 | Bike lanes, raised walking cross walks | 2/1/2022 10:34 AM |
| 617 | Bike lanes in the street that are clearly marked so they don't get run over | 2/1/2022 10:26 AM |
| 618 | Bus shelters, bike lanes | 2/1/2022 10:21 AM |
| 619 | Public transit pickup/dropoff spots that do not impede the flow of vehicular traffic. | 2/1/2022 10:15 AM |
| 620 | Bike lanes... sidewalks.... | 2/1/2022 10:09 AM |
| 621 | protected bike lanes | 2/1/2022 10:09 AM |
| 622 | bicycle lanes, signs warning of bicycles in the area | 2/1/2022 10:02 AM |
| 623 | Put bike lanes on streets other than high volume traffic streets such as Blvd. of the Arts. Bikes and cars don't mix well! Especially when there is on-street parking. | 2/1/2022 10:02 AM |

Boulevard of the Arts & 10th Street Complete Streets Survey

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| 624 | Proper nighttime lighting. | 2/1/2022 10:00 AM |
| 625 | Shuttles from downtown | 2/1/2022 9:43 AM |
| 626 | bike lanes | 2/1/2022 9:40 AM |
| 627 | ? | 2/1/2022 9:38 AM |
| 628 | Bike lanes | 2/1/2022 9:32 AM |
| 629 | bicycle lanes! | 2/1/2022 9:29 AM |
| 630 | No cars or buses | 2/1/2022 9:29 AM |
| 631 | Bike lanes, quality bike racks, good lights, bus shelters, covered walkways | 2/1/2022 9:28 AM |
| 632 | Bike lane, better cross walks | 2/1/2022 9:28 AM |
| 633 | An overpass over mound | 2/1/2022 9:24 AM |
| 634 | Bicycle repair stations, free parking, bike lanes, dog poop bags | 2/1/2022 9:22 AM |
| 635 | Bus shelters , bicycle lanes , safe walking when arriving | 2/1/2022 9:21 AM |
| 636 | Bike lanes, but we need to have safe travel to get there. | 2/1/2022 9:17 AM |
| 637 | Bike lanes Bus shelters Bike secure parking | 2/1/2022 9:16 AM |
| 638 | Build out neighborhood north-south greenways like school street greenway north of Weber so people can bike all the way downtown. People will just drive and walk around if you don't build out the bike network with class four and class one facilities to get to here. Also include building protected intersections that NACTO espouses so that people can cross major arterials and collectors safely. | 2/1/2022 9:13 AM |
| 639 | We don't need that NYC nonsense | 2/1/2022 9:05 AM |
| 640 | What? | 2/1/2022 9:01 AM |
| 641 | Better, easier, safer crossing of Tamiami, Fruitville, 10th St. Less harassment by homeless. | 2/1/2022 8:58 AM |
| 642 | Multi-Use Paths to accommodate micromobility and pedestrians. Lowering speeds to allow for cyclists to share the road. Shelters for buses and trolleys routes. | 1/28/2022 10:17 AM |
| 643 | covered bus shelters with trash cans, cigarette butt containers, bike repair kiosks, water fountains, protected bike lanes, paint/art | 1/28/2022 10:09 AM |

Q7 What types of activities or features would draw more people to Boulevard of the Arts (east of Tamiami Trail)? Select all that apply.

Answered: 1,064 Skipped: 20



| ANSWER CHOICES | RESPONSES |
|--------------------------|------------|
| Sidewalk cafes | 90.60% 964 |
| Street festivals | 59.30% 631 |
| On-street parking | 34.30% 365 |
| Public Parking Garage | 48.59% 517 |
| Other (please specify) | 15.98% 170 |
| Total Respondents: 1,064 | |

| # | OTHER (PLEASE SPECIFY) | DATE |
|---|--|--------------------|
| 1 | We do not need to attract more cars and peoples until we fix the disaster of the Tamiami rotaries. | 2/28/2022 1:42 PM |
| 2 | educational activities, global warming, red tide | 2/28/2022 12:12 PM |
| 3 | Access to the water in terms of rest areas with shade and park benches. | 2/28/2022 11:51 AM |
| 4 | public parking preferred | 2/28/2022 9:03 AM |
| 5 | bike lanes and bike racks that are safe from vandalism. | 2/27/2022 9:35 PM |
| 6 | Pedestrian overpass into Bay Park and new PAC | 2/25/2022 11:56 PM |
| 7 | Keep it lined with trees and open spaces no no businesses | 2/24/2022 2:36 AM |
| 8 | All of the above | 2/24/2022 12:20 AM |
| 9 | Side walk music (not live) creating friendly atmosphere | 2/23/2022 11:30 PM |

Boulevard of the Arts & 10th Street Complete Streets Survey

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| 10 | Walking areas /parks with shady trees /benches like north straub in st Pete downtown | 2/23/2022 3:43 PM |
| 11 | Trees | 2/23/2022 2:53 PM |
| 12 | Is there room for all this? These pictures show much more open space than is available. | 2/23/2022 2:34 PM |
| 13 | There are already too many people. The enjoyment is gone because of them. | 2/23/2022 1:46 PM |
| 14 | picnic areas, children playground | 2/23/2022 1:39 PM |
| 15 | Quality business that are open on the weekend and early evening | 2/23/2022 11:49 AM |
| 16 | Pedestrian overpasses | 2/23/2022 11:47 AM |
| 17 | Trying to get through the mess that you created at Tamiami and Fruitville. The downtown is a hot mess. Correct that before creating new problems. | 2/23/2022 11:22 AM |
| 18 | NONE OF THE ABOVE. You folks are NOT thinking about the homeless population of North Sarasota. | 2/23/2022 11:17 AM |
| 19 | Ferry people to and from LBK | 2/23/2022 10:58 AM |
| 20 | overpass | 2/23/2022 10:44 AM |
| 21 | No more people are needed | 2/23/2022 10:29 AM |
| 22 | Bike racks to "park" bikes at. | 2/23/2022 10:17 AM |
| 23 | Parking garage is desperately needed | 2/23/2022 10:14 AM |
| 24 | Pedestrian accessibility | 2/23/2022 10:13 AM |
| 25 | Food stands (take away, e.g. ice-cream parlour) and seatings | 2/23/2022 10:04 AM |
| 26 | Walk to park from Blvd via Pedestrian bridge X Tamiami | 2/23/2022 10:03 AM |
| 27 | Attractive Restrooms, security, | 2/23/2022 10:01 AM |
| 28 | Scooter or bike rental area, bike lock up area | 2/23/2022 10:00 AM |
| 29 | Please have a locals only parking option - I would pay an annual fee for a parking pass that allows me to have more access to the place I live. | 2/23/2022 9:52 AM |
| 30 | Arts type of businesses | 2/23/2022 9:52 AM |
| 31 | Low traffic greener space | 2/23/2022 9:48 AM |
| 32 | Downtown Sarasota has benefitted with the Parking Garages | 2/23/2022 9:48 AM |
| 33 | Public spaces that are destinations, like parks, that do not require \$\$ and that are nice to hang out in. | 2/22/2022 8:59 AM |
| 34 | more trees | 2/20/2022 7:40 PM |
| 35 | NOT a parking garage | 2/20/2022 7:38 PM |
| 36 | Neighborhood Market deli style , local Artist an artisan type shops(featuring affordable classes) | 2/17/2022 5:03 PM |
| 37 | Additional businesses such as cafes and shops | 2/16/2022 10:42 AM |
| 38 | Easy walking access to cross Tamiami | 2/16/2022 6:49 AM |
| 39 | Well landscape, surface parking lot | 2/15/2022 1:04 PM |
| 40 | public parking garage to get the cars off the street | 2/14/2022 7:09 PM |
| 41 | wider sidewalks and shade trees | 2/14/2022 9:24 AM |
| 42 | No Parking garage! | 2/13/2022 6:45 PM |
| 43 | This is highly residential area and should remain so with the addition of any businesses having regular 9-5 type hours | 2/13/2022 5:18 PM |
| 44 | Close to traffic | 2/12/2022 1:40 PM |

Boulevard of the Arts & 10th Street Complete Streets Survey

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| 45 | Area needs re-vitalizing to make it more pedestrian and visitor friendly and safer | 2/12/2022 1:17 PM |
| 46 | Take over minimal parking for bikes and pedestrians | 2/12/2022 12:46 PM |
| 47 | Parking but not sure if it should be a parking garage | 2/12/2022 12:41 PM |
| 48 | Trees bike lane | 2/12/2022 12:40 PM |
| 49 | Peace and quiet which you are systematically destroying making the area look like a gilded lily ! | 2/12/2022 10:31 AM |
| 50 | No Parking garage. A parking garage should be part of the new Performing Arts Center —right in the Bay Park! | 2/12/2022 10:00 AM |
| 51 | Individualized, locally owned retail | 2/12/2022 9:57 AM |
| 52 | Love to see shops such as clothing and restaurants coffee shops | 2/12/2022 8:06 AM |
| 53 | Free easy parking , bike rack, more coffee/cafes, better hours, new businesses | 2/8/2022 6:10 PM |
| 54 | Improve Mobility options | 2/8/2022 8:35 AM |
| 55 | Literally anything that gets the cars off the street and makes it more comfortable for pedestrians. When people complain that it's hard for them to drive this area, IGNORE THEM. | 2/8/2022 8:03 AM |
| 56 | Varied retail stores | 2/7/2022 6:28 PM |
| 57 | moratorium on high rise construction | 2/7/2022 1:44 PM |
| 58 | not allowing homeless to sleep/live on sidewalks | 2/7/2022 1:44 PM |
| 59 | Thinking about people and eliminating cars. | 2/7/2022 12:53 PM |
| 60 | Do not want more people | 2/7/2022 12:13 PM |
| 61 | NEED A PEDX BRIDGE OVER 41 (and Fruitville) FOR CONNECTING SAFELY THE BAY TO RADD AND BEYOND TO OTHER NEIGHBORHOODS (ie. Gillespie, Central Cocanut, downtown). Problem due to some of the new residential buildings with no set-back and imposing presence do not lend to a comfortable experience. Sidewalks should be wide and allow for landscaping areas that are big enough for the health of the plants/trees. More interesting shops and boutiques. FREE PARKING. On-street parking to coordinate safely with bicycles. | 2/7/2022 11:40 AM |
| 62 | public art | 2/7/2022 9:25 AM |
| 63 | Restaurants, retail | 2/7/2022 8:31 AM |
| 64 | A safe way to cross from the park over Tamiami trail | 2/7/2022 7:30 AM |
| 65 | Pedestrian bridge | 2/6/2022 8:51 PM |
| 66 | trees, plantings, and PROTECTED bike lanes | 2/6/2022 5:20 PM |
| 67 | Connection to rail trail or water trail | 2/6/2022 3:35 PM |
| 68 | Lighting more stores and restaurants | 2/6/2022 2:51 PM |
| 69 | we don't want more people or traffic on our street!!!! | 2/6/2022 12:20 PM |
| 70 | Functioning businesses period. There is a dearth of viable commercial activity in Blvd of the Arts. It needs a mix of restaurants, cafes and other "go-to" shops. | 2/6/2022 10:05 AM |
| 71 | Walkway over route 41. | 2/6/2022 9:11 AM |
| 72 | Pedestrian overpass | 2/6/2022 4:17 AM |
| 73 | With a lot of high rises on Blvd of the Arts, people would be more attracted to lush green spaces, | 2/6/2022 12:11 AM |
| 74 | Bridge over 41. Roundabout | 2/5/2022 10:52 PM |
| 75 | Dog area | 2/5/2022 9:27 PM |
| 76 | food trucks are essential, outdoor cafes and farmers market | 2/5/2022 8:48 PM |

Boulevard of the Arts & 10th Street Complete Streets Survey

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| 77 | Parking is needed for example 6th and orange no parking on 6th. The few spaces available are always filled for design studio building | 2/5/2022 8:09 PM |
| 78 | Living in my townhouse On the Blvd. I'd be less enthusiastic having folks lingering by my window and entranceway | 2/5/2022 4:55 PM |
| 79 | Green spaces | 2/5/2022 4:15 PM |
| 80 | Sidewalks are too narrow for cafes Add trees | 2/5/2022 4:11 PM |
| 81 | SHADE, trees, hanging flower baskets | 2/5/2022 4:05 PM |
| 82 | places to lock bikes, ease of crossing to the water front, parking lot, limited street parking. | 2/5/2022 3:44 PM |
| 83 | More greenery | 2/5/2022 3:05 PM |
| 84 | easier vehicle access for local residents and guests on West Side of Blvd of the Arts | 2/5/2022 2:14 PM |
| 85 | more protected bicycle paths -- well away from cars -- protected with planters / trees. | 2/5/2022 2:11 PM |
| 86 | Visible police presence to give a feeling of safety | 2/5/2022 1:57 PM |
| 87 | Don't want to draw more people | 2/5/2022 1:55 PM |
| 88 | why do you need to drawer people -? Too much money is put into this 1 | 2/5/2022 12:29 PM |
| 89 | Concerts, movies outdoors | 2/5/2022 12:22 PM |
| 90 | outdoor dining | 2/5/2022 12:21 PM |
| 91 | Parks, green spaces, walkable, commercial space designed to fit the neighborhood with interesting high end shops | 2/5/2022 12:16 PM |
| 92 | Pedestrian overpass to safely cross over | 2/5/2022 12:09 PM |
| 93 | Public circular transportation, hop on/off people mover | 2/5/2022 11:52 AM |
| 94 | pedestrian bridge between Bay and Blvd | 2/5/2022 11:48 AM |
| 95 | art work | 2/5/2022 11:33 AM |
| 96 | Pedestrian bridge would provide easy access. I would walk rather than drive because I would feel safe with pedestrian bridge access | 2/5/2022 11:26 AM |
| 97 | Broad selection of businesses & services | 2/5/2022 11:23 AM |
| 98 | Play spaces or splash pad type fountain | 2/5/2022 11:04 AM |
| 99 | More retail outlets | 2/5/2022 11:03 AM |
| 100 | Easy access to The Bay sites for anyone over 50: | 2/5/2022 10:51 AM |
| 101 | Only a parking garage if there are bridges connecting the rosemary district to the Bay. | 2/5/2022 10:46 AM |
| 102 | Secure bicycle parking. | 2/5/2022 10:15 AM |
| 103 | Engaging storefronts, mixed uses, unique restaurants/cafes, thoughtful lighting, art/murals, music | 2/5/2022 9:57 AM |
| 104 | There is no parking. Hurts the businesses there | 2/5/2022 9:48 AM |
| 105 | Retail shops - grocery, hardware - things people need | 2/5/2022 9:31 AM |
| 106 | safe, efficient bridge over Tamiami Trail to The Bay | 2/5/2022 9:24 AM |
| 107 | Please add a pedestrian bridge across 41! | 2/5/2022 9:15 AM |
| 108 | Free parking | 2/5/2022 8:51 AM |
| 109 | Off street parking | 2/5/2022 7:23 AM |
| 110 | More trees! Pocket parks. | 2/5/2022 6:14 AM |
| 111 | E.V. charging. | 2/5/2022 1:49 AM |

Boulevard of the Arts & 10th Street Complete Streets Survey

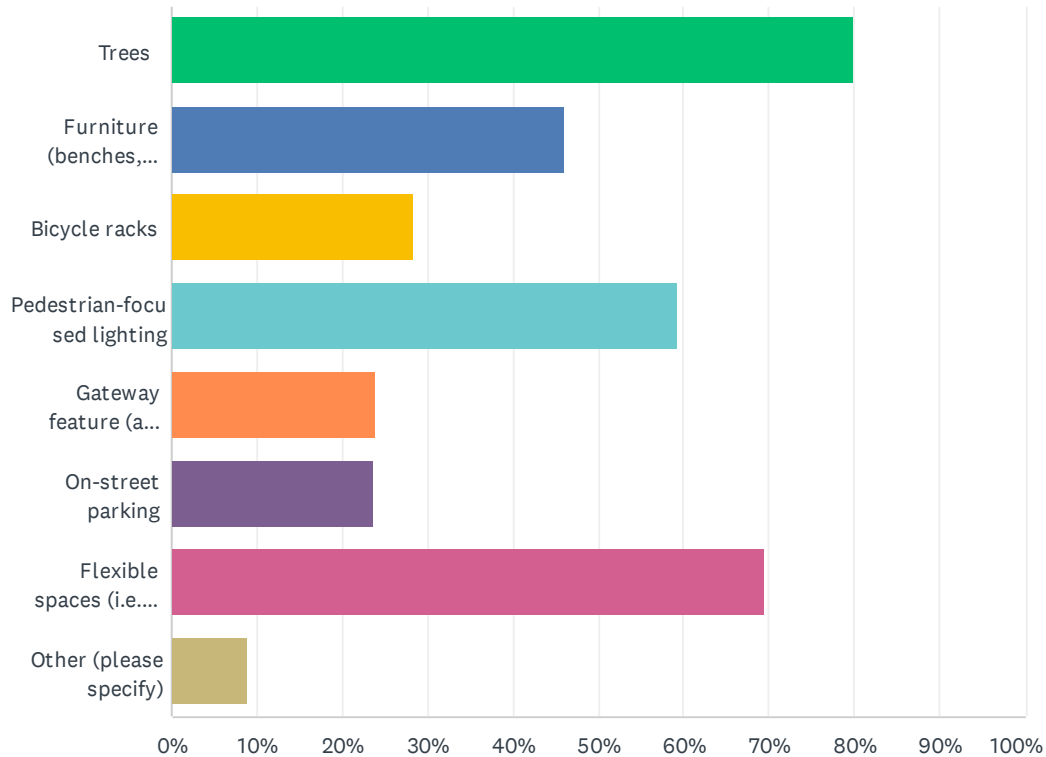
| | | |
|-----|--|-------------------|
| 112 | Simple Software to time street lights like in other cities and counties (Ex: St. Pete). | 2/4/2022 4:36 PM |
| 113 | shopping, interesting landmarks | 2/4/2022 4:03 PM |
| 114 | Address safety for all users. Clean up dilapidated building s | 2/3/2022 10:26 PM |
| 115 | more businesses, mixed residential | 2/3/2022 4:54 PM |
| 116 | Arts | 2/3/2022 2:03 PM |
| 117 | Better sidewalks | 2/3/2022 1:49 PM |
| 118 | bars, restaurants | 2/3/2022 11:15 AM |
| 119 | Assorted public art, landscaping, shade and musical ambiance | 2/3/2022 7:19 AM |
| 120 | Don't draw more people. Enhance the experience for the people already there. We already have a Main street. We don't need another. | 2/3/2022 3:47 AM |
| 121 | A small Art Deco movie theater. | 2/2/2022 11:36 PM |
| 122 | Innovative retail/attractions. | 2/2/2022 2:58 PM |
| 123 | houses | 2/2/2022 2:19 PM |
| 124 | unknown | 2/2/2022 1:02 PM |
| 125 | Bike rack | 2/2/2022 12:21 PM |
| 126 | Trees, greenery, and make street completely pedestrian-no vehicles | 2/2/2022 10:44 AM |
| 127 | Businesses open in evenings | 2/2/2022 8:47 AM |
| 128 | Bike lanes | 2/2/2022 7:35 AM |
| 129 | Shade trees and multiuser zoning, | 2/2/2022 12:50 AM |
| 130 | Public parks and recreational space (dog parks, etc.) | 2/1/2022 10:12 PM |
| 131 | Additional car lane | 2/1/2022 9:46 PM |
| 132 | Programs that offer incentives for older, unique, old Florida properties to be saved, not destroyed, which adds to the charm and appeal of this area | 2/1/2022 9:28 PM |
| 133 | trees and benches | 2/1/2022 8:52 PM |
| 134 | Pedestrian overpass across 41 to the new bay park | 2/1/2022 8:31 PM |
| 135 | Affordable shops and activities | 2/1/2022 7:49 PM |
| 136 | Stop building Residential it's too much. | 2/1/2022 7:39 PM |
| 137 | Closing the street to cars | 2/1/2022 6:41 PM |
| 138 | Parking | 2/1/2022 6:29 PM |
| 139 | More retail | 2/1/2022 5:17 PM |
| 140 | Remove homeless | 2/1/2022 5:08 PM |
| 141 | More people not needed! | 2/1/2022 4:32 PM |
| 142 | Parks and shade | 2/1/2022 4:14 PM |
| 143 | A variety of shops/restaurants/visual appeal to create an interesting walk | 2/1/2022 4:02 PM |
| 144 | shade trees, foliage, small scale events, food trucks, free music | 2/1/2022 3:44 PM |
| 145 | Or 'trolley' type service throughout Rosemary District as more businesses open rather than inviting cars and thus an increased need for parking | 2/1/2022 3:31 PM |
| 146 | pop-up, kiosk-style businesses run/owned by locals | 2/1/2022 3:30 PM |
| 147 | I'm concerned about safety. More lights always reassuring in walk around when alone. | 2/1/2022 3:25 PM |
| 148 | Park! For kiddos to play at between grabbing coffee and shopping | 2/1/2022 2:44 PM |

Boulevard of the Arts & 10th Street Complete Streets Survey

| | | |
|-----|---|--------------------|
| 149 | Street art | 2/1/2022 1:50 PM |
| 150 | Policing of homeless/ panhandling | 2/1/2022 1:27 PM |
| 151 | We already have plenty of activities here in our small neighborhood. Please work on retaining its charm and forget making it Downtown+. | 2/1/2022 1:02 PM |
| 152 | Big art sculptures are missing from the "Arts" | 2/1/2022 1:02 PM |
| 153 | Wider sidewalks, and the planned park. Note the constant activity at Rosemary Square | 2/1/2022 12:55 PM |
| 154 | Park | 2/1/2022 12:34 PM |
| 155 | PROTECTED bike lanes | 2/1/2022 12:31 PM |
| 156 | ability to drive across 41 on BOTA from west end of BOTA | 2/1/2022 11:56 AM |
| 157 | Keeping cars out of the area | 2/1/2022 11:55 AM |
| 158 | More security and fewer homeless | 2/1/2022 11:55 AM |
| 159 | more art from local artists, not pretentious boring sculptures in the middle of a roundabout | 2/1/2022 11:38 AM |
| 160 | Enhanced walkability and pedestrian-friendly features | 2/1/2022 11:38 AM |
| 161 | Taking indigent and homeless off of the streets such as Central Ave, no one really feels safe with people laying on the sidewalks. A parking garage on Fruitville or 10th street would allow people to park at the perimeter of The Rosemary District and businesses to flourish/thrive there as well | 2/1/2022 11:00 AM |
| 162 | More trees/shade! Heat driving people indoors kills sidewalk business. | 2/1/2022 10:25 AM |
| 163 | Art installation, parks, dog friendly, outdoor spaces to eat/drink | 2/1/2022 10:09 AM |
| 164 | park, legacy trail | 2/1/2022 10:09 AM |
| 165 | Sarasota has too many parking garages. Please do not add more. | 2/1/2022 10:02 AM |
| 166 | Stores in general | 2/1/2022 9:28 AM |
| 167 | Keeping up with safety measures to deter transients from drinking/drugging. (Love the new signs and fences to deter vagrancy, good job, I don't feel the need to carry pepper spray anymore when I walk my dog. I was chased twice last year, no issues in 6 months.) | 2/1/2022 9:28 AM |
| 168 | Why? Leave the people on Main Street. | 2/1/2022 9:01 AM |
| 169 | Easier, Faster, Safer crossing of Tamiami & Fruitville | 2/1/2022 8:58 AM |
| 170 | public art, sidewalk art, murals | 1/28/2022 10:09 AM |

Q8 What amenities would you like to see on Boulevard of the Arts (east of Tamiami Trail)? Pick your top three.

Answered: 1,068 Skipped: 16



| ANSWER CHOICES | RESPONSES |
|--|------------|
| Trees | 80.06% 855 |
| Furniture (benches, garbage/recycling bins) | 46.07% 492 |
| Bicycle racks | 28.28% 302 |
| Pedestrian-focused lighting | 59.36% 634 |
| Gateway feature (a formal entrance to Boulevard of the Arts) | 23.78% 254 |
| On-street parking | 23.60% 252 |
| Flexible spaces (i.e. pocket parks, parklets, outdoor market spaces) | 69.48% 742 |
| Other (please specify) | 8.90% 95 |
| Total Respondents: 1,068 | |

| # | OTHER (PLEASE SPECIFY) | DATE |
|---|--|--------------------|
| 1 | Recurring weekend events, farmers markets, fairs... something that St. Armand's Circle does very well. | 2/28/2022 11:51 AM |
| 2 | Pedestrian overpass to Bay Park and new PAC | 2/25/2022 11:56 PM |

Boulevard of the Arts & 10th Street Complete Streets Survey

| | | |
|----|--|--------------------|
| 3 | No business. We need beautification places of relaxation stop the greed and allow calmness and tranquility | 2/24/2022 2:36 AM |
| 4 | mobile vending options, fresh water source | 2/23/2022 8:29 PM |
| 5 | Microforests (dr. Charles Reith) | 2/23/2022 7:30 PM |
| 6 | Pedestrian overpasses | 2/23/2022 11:47 AM |
| 7 | Parking Garage | 2/23/2022 11:17 AM |
| 8 | Water transportation with good harbor auto parking | 2/23/2022 10:58 AM |
| 9 | Pedestrian bridge | 2/23/2022 10:13 AM |
| 10 | Trolley or bus service from downtown | 2/23/2022 10:03 AM |
| 11 | Water fountains for drinking for both humans and pets | 2/23/2022 10:00 AM |
| 12 | Ample parking | 2/23/2022 9:57 AM |
| 13 | Pedestrian bridge linking the proposed redevelopment West of 41 (trail). Very dangerous in current set up. | 2/23/2022 9:48 AM |
| 14 | A safe way to cross Tamiami for pedestrians. | 2/16/2022 6:49 AM |
| 15 | Great landscaping! | 2/15/2022 1:04 PM |
| 16 | Wayfinding to local arts businesses, more unique businesses | 2/15/2022 8:15 AM |
| 17 | Easier east running automobile traffic movement from 41 to 301 | 2/14/2022 3:07 PM |
| 18 | large sidewalks | 2/14/2022 8:49 AM |
| 19 | Keep outdoor furniture to a minimum as to discourage vagrants | 2/13/2022 5:18 PM |
| 20 | Less cars | 2/12/2022 1:40 PM |
| 21 | Bike lane | 2/12/2022 12:40 PM |
| 22 | More retail/restaurants | 2/12/2022 11:16 AM |
| 23 | traffic calming and re-routing | 2/12/2022 11:04 AM |
| 24 | Crosswalks | 2/9/2022 1:02 PM |
| 25 | bridges that reflect our local arts culture | 2/9/2022 12:01 PM |
| 26 | Free easy parking | 2/8/2022 6:10 PM |
| 27 | Safe bike lanes | 2/8/2022 8:35 AM |
| 28 | Brick pavers! | 2/8/2022 8:03 AM |
| 29 | Pedestrian bridge over Tamiami Tr | 2/7/2022 8:05 PM |
| 30 | Something like Towles Court - a small artist colony feel would fit area vibe and name of the street. Would be | 2/7/2022 1:09 PM |
| 31 | Not just any tree, but Royal Palm Trees | 2/7/2022 1:01 PM |
| 32 | parking lot | 2/7/2022 12:22 PM |
| 33 | Pedestrian Bridge | 2/7/2022 12:16 PM |
| 34 | Streetlights that are in good repair and all same color - some are green and some are black - the streetlight bulbs being using downtown area are so bright they hurt the eyes so a better bulb that lends ample lighting but is easy on the eyes - streetlight banner program (with images of the local RADD murals growing collection--I proposed this to RDA a couple years ago). | 2/7/2022 11:40 AM |
| 35 | Bike tool kiosks | 2/7/2022 7:30 AM |
| 36 | Pedestrian and bicycle bridge / overpass | 2/7/2022 7:08 AM |
| 37 | Pedestrian bridge | 2/6/2022 8:51 PM |

Boulevard of the Arts & 10th Street Complete Streets Survey

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|----|--|-------------------|
| 38 | Painted bike lanes, NOT separated bike lanes | 2/6/2022 8:49 PM |
| 39 | PROTECTED bike lanes | 2/6/2022 5:20 PM |
| 40 | Art | 2/6/2022 4:17 AM |
| 41 | Since its Blvd of the ARTS, more Organic Art Sculptures and Art Expressions, i.e. painted rocks, that blend with greenery. | 2/6/2022 12:11 AM |
| 42 | Bike lanes | 2/5/2022 9:27 PM |
| 43 | From 6th and orange to the Overton not one bench.lots of old people can't walk that far without taking a rest. | 2/5/2022 8:09 PM |
| 44 | sitting spaces to build community. if garbage and recycling bins look at the ones i Paris, not the kind usually used in the US | 2/5/2022 4:05 PM |
| 45 | shade is important in a concrete area. Awnings or similar | 2/5/2022 3:44 PM |
| 46 | Love that you are considering all these!!! Congratulations!!! | 2/5/2022 2:11 PM |
| 47 | BENCHES FOR THE HOMELESS ? GATEWAY - WASTE OF MONEY | 2/5/2022 12:29 PM |
| 48 | Food truck locations | 2/5/2022 12:21 PM |
| 49 | Overpass to enter the park and access the new PAC that will be built | 2/5/2022 12:09 PM |
| 50 | pedestrian bridge to the Bay | 2/5/2022 11:48 AM |
| 51 | Children play space | 2/5/2022 11:04 AM |
| 52 | Art/murals | 2/5/2022 9:57 AM |
| 53 | More things to do there. | 2/5/2022 9:48 AM |
| 54 | Something to slow cars down around people. | 2/5/2022 9:38 AM |
| 55 | Adequate parking | 2/5/2022 9:19 AM |
| 56 | Free parking | 2/5/2022 8:51 AM |
| 57 | Bike lanes to travel to and through downtown better and connect to the Bay. | 2/5/2022 8:06 AM |
| 58 | Public restrooms | 2/5/2022 5:52 AM |
| 59 | E.V. charging. | 2/5/2022 1:49 AM |
| 60 | Against Parklets that draw homeless persons to the area. | 2/4/2022 4:36 PM |
| 61 | Bicyclist-focused traffic pattern | 2/4/2022 4:03 PM |
| 62 | A better more permanent and compassionate response to the homeless population | 2/3/2022 10:26 PM |
| 63 | abundant art | 2/3/2022 4:54 PM |
| 64 | Art | 2/3/2022 2:03 PM |
| 65 | no benches for homeless | 2/3/2022 11:15 AM |
| 66 | quality landscaping that includes all plantings beside just trees, like palms, flowering plants and colorful foliage accent plantings that receives a high level of care and maintenance, accent pavers, bricks or stamped concrete walkways and plazas, and either live or recorded music a reasonable low volume levels to not impact people living in the area. | 2/3/2022 7:19 AM |
| 67 | houses | 2/2/2022 2:19 PM |
| 68 | Behind buildings for parking | 2/2/2022 12:21 PM |
| 69 | No vehicles | 2/2/2022 10:44 AM |
| 70 | Green spaces. There are so few in Sarasota | 2/2/2022 7:19 AM |
| 71 | Tropical landscaping and public art | 2/2/2022 12:50 AM |
| 72 | Over pass across 41 | 2/1/2022 8:31 PM |

Boulevard of the Arts & 10th Street Complete Streets Survey

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|----|--|--------------------|
| 73 | Walking paths, nature enough free parking | 2/1/2022 7:39 PM |
| 74 | No gateway! Keep it real. | 2/1/2022 7:39 PM |
| 75 | Fountains to hear water | 2/1/2022 7:19 PM |
| 76 | Public parking garage, or two public parking garages, one for each! | 2/1/2022 5:32 PM |
| 77 | Wide sidewalks, no street parking lighted very well. No homeless allowed for safety purposes | 2/1/2022 4:25 PM |
| 78 | Connecting the Rosemary district to downtown. Maybe a walking path over fruitville road where pedestrians can walk absent of any cars. The lanes on Fruitville are too wide+traffic travels too fast on fruitville for a comfortable walk which disconnects the downtown. The Rosemary district will flourish much more of it connects to downtown better. | 2/1/2022 4:02 PM |
| 79 | fewer car options, in fact, no car options except for handicap and ride-share | 2/1/2022 3:30 PM |
| 80 | I have seen gateway in Carmel. Looks awesome!! | 2/1/2022 3:26 PM |
| 81 | all of the above | 2/1/2022 3:08 PM |
| 82 | A fountain or park. Toddler friendly! Or pedestrian only zones | 2/1/2022 2:44 PM |
| 83 | Shaded park with water fountain | 2/1/2022 2:32 PM |
| 84 | Please! Kindly pull back on turning a historic neighborhood into Downtown+. We're small and have plenty to do. We have our own park in the works. Don't need any more. | 2/1/2022 1:02 PM |
| 85 | safety/security, must address homeless problem | 2/1/2022 11:56 AM |
| 86 | Parking garage | 2/1/2022 11:18 AM |
| 87 | Public art- sculpture | 2/1/2022 11:00 AM |
| 88 | EV charging stations | 2/1/2022 10:54 AM |
| 89 | Pedestrian mall | 2/1/2022 10:47 AM |
| 90 | Better parking solutions. Right now it's always a gamble. Some ppl I know flat-out refuse to go downtown because parking is a pain. | 2/1/2022 10:25 AM |
| 91 | We DO NOT need gateway signs—they are things from the past, and usually visually unappealing. | 2/1/2022 10:02 AM |
| 92 | Outdoor cafes | 2/1/2022 9:43 AM |
| 93 | Skatepark | 2/1/2022 9:28 AM |
| 94 | Easier, Faster, Safer crossing of Tamiami & Fruitville | 2/1/2022 8:58 AM |
| 95 | placemaking - art incorporated into the public space. | 1/28/2022 10:09 AM |

Q9 What improvements would you like to see on Boulevard of the Arts (east of Tamiami Trail)?

Answered: 467 Skipped: 617

| # | RESPONSES | DATE |
|----|--|--------------------|
| 1 | Making it resident friendly. Not everything is about the out-of-towners. | 2/28/2022 3:40 PM |
| 2 | We need pedestrian ramps over Tamiami for pedestrian safety and automobile flow. | 2/28/2022 1:42 PM |
| 3 | It needs to be inviting to all people with attention to diversity and inclusion. Restaurants should offer foods from different countries, signs should be bilingual, festivals should be multicultural. | 2/28/2022 11:51 AM |
| 4 | Making it more pedestrian friendly, and encourage a variety of businesses and restaurants that people will have access to. | 2/27/2022 9:35 PM |
| 5 | Parks | 2/27/2022 5:49 PM |
| 6 | pedestrian bridges; enhanced pedestrian access and safety | 2/27/2022 5:43 PM |
| 7 | I would like to see inviting spaces for the public and a really attractive road that invites people into the area. This urban neighborhood should be elevated with a unique approach. | 2/27/2022 11:58 AM |
| 8 | Bike racks, parklests | 2/27/2022 10:08 AM |
| 9 | Sky bridge | 2/27/2022 9:08 AM |
| 10 | strong support for pedestrian bridges | 2/26/2022 2:24 PM |
| 11 | Outdoor venue for small concerts and theater. Statues and outside art work that is calming and interesting and that makes it a venue where visitors can appreciate the history of the city and that represents all cultures. | 2/26/2022 8:14 AM |
| 12 | Pedestrian friendly sidewalks, safe way to cross 41, pedestrian foot bridge to new PAC on 19th street | 2/25/2022 11:56 PM |
| 13 | traffic slowed | 2/25/2022 8:51 PM |
| 14 | Raised pedestrian path | 2/25/2022 10:49 AM |
| 15 | Pedestrian Bridge over Tamiami trail | 2/25/2022 6:08 AM |
| 16 | The above listed survey improvements would help to promote business and community involvement and participation. Thank you! | 2/24/2022 8:03 PM |
| 17 | safety of pedestrians | 2/24/2022 1:17 PM |
| 18 | Gateway structure, parklets and benches, trees, flowers, parking garage rather than street parking | 2/24/2022 11:48 AM |
| 19 | Where there is on street parking there is inadequate space. I saw an accident where a person opened their car door and was hit by a passing car | 2/24/2022 10:01 AM |
| 20 | More places to sit and enjoy the area. | 2/24/2022 9:38 AM |
| 21 | I think making the area a true arts venue with the PAC, symphony building, and other art studios. | 2/24/2022 9:17 AM |
| 22 | Pedestrian cross over to Bay Park - 10th street is not an appealing alternative for pedestrians | 2/24/2022 8:47 AM |
| 23 | Green space | 2/24/2022 8:46 AM |
| 24 | pedestrian friendly | 2/24/2022 8:07 AM |
| 25 | Make it easy to cross for cars and pedestrians. No more ridiculous roundabouts. | 2/24/2022 2:36 AM |
| 26 | Pedestrian walkway to access Van Wezel and Sarasota Bay. | 2/24/2022 12:20 AM |

Boulevard of the Arts & 10th Street Complete Streets Survey

| | | |
|----|---|--------------------|
| 27 | More small eating establishments with outdoor seating, more retail, better lighting. | 2/23/2022 11:30 PM |
| 28 | A notable Gateway distinguishes the area. | 2/23/2022 9:40 PM |
| 29 | parking garage, parklets. Not enough room for on-street parking | 2/23/2022 8:44 PM |
| 30 | more retail businesses and places to visit along the way and a path to get me across 41 that feels safe whether walking or biking | 2/23/2022 8:29 PM |
| 31 | Fewer cars. More safety for bikes and pedestrians. | 2/23/2022 7:30 PM |
| 32 | Pedestrian walkway | 2/23/2022 5:07 PM |
| 33 | Pedestrian bridge over US 41 to the Van Wezel. | 2/23/2022 4:44 PM |
| 34 | Pedestrian bridges to cross Rt41 | 2/23/2022 4:35 PM |
| 35 | Design elements to indicate it is a gateway and has continuity like the Riverwalk in Bradenton. | 2/23/2022 4:25 PM |
| 36 | Pedestrian Bridges | 2/23/2022 4:16 PM |
| 37 | Address the homeless issues | 2/23/2022 3:53 PM |
| 38 | Calmer traffic , more trees | 2/23/2022 3:43 PM |
| 39 | I would like to see it be safe, pedestrian and bicycle friendly to connect to Main St. the core of downtown Sarasota. It should also have cafe's restaurants and seating outside along the blvd. It should be a street that people will come to as a destination just like Main St. | 2/23/2022 3:37 PM |
| 40 | Parkets | 2/23/2022 3:27 PM |
| 41 | A pedestrian bridge over Tamiami Trail connecting it to The Bay district and the new performing arts hall. This would make it easy for people to have dinner at restaurants on Boulevard of the Arts before attending performances at the new facility. | 2/23/2022 3:10 PM |
| 42 | Eliminate right turns on red for the safety of humans | 2/23/2022 3:03 PM |
| 43 | Slower traffic | 2/23/2022 2:53 PM |
| 44 | Safety, lighting and ease of access | 2/23/2022 2:51 PM |
| 45 | More pedestrian friendly | 2/23/2022 2:50 PM |
| 46 | Not sure | 2/23/2022 2:38 PM |
| 47 | Pedestrian overpass on Hwy 41 | 2/23/2022 2:20 PM |
| 48 | Pedestrian Bridges & safe walkways from parking deck | 2/23/2022 2:10 PM |
| 49 | family friendly areas with convenient parking | 2/23/2022 1:39 PM |
| 50 | Noise restrictions from outdoor concerts | 2/23/2022 1:37 PM |
| 51 | Improvements that would give it an Arts identity | 2/23/2022 1:35 PM |
| 52 | Pedestrian walk way above traffic | 2/23/2022 1:31 PM |
| 53 | Build pedestrian bridges | 2/23/2022 1:28 PM |
| 54 | Control of traffic | 2/23/2022 1:04 PM |
| 55 | Pedestrian bridge over 41 | 2/23/2022 1:00 PM |
| 56 | Better parking opportunities, pedestrian only areas | 2/23/2022 12:56 PM |
| 57 | Pedestrian walkways. | 2/23/2022 12:38 PM |
| 58 | All of the above. | 2/23/2022 12:24 PM |
| 59 | Safe lighting, parklets, garage, street cafes. | 2/23/2022 12:11 PM |
| 60 | Parking accessibility | 2/23/2022 11:53 AM |
| 61 | We must have a pedestrian bridge to cross 41 to Bay Front Park! | 2/23/2022 11:48 AM |

Boulevard of the Arts & 10th Street Complete Streets Survey

| | | |
|----|--|--------------------|
| 62 | Stop light | 2/23/2022 11:47 AM |
| 63 | Raised walkway over 42 | 2/23/2022 11:38 AM |
| 64 | Pedestrian bridges across Tamiami Trail to performing arts venues. | 2/23/2022 11:29 AM |
| 65 | Love gateway ides | 2/23/2022 11:23 AM |
| 66 | Additional lanes to pull the traffic away from Fruitville where it backs up for blocks and create additional lanes on Tamiami and rip up that ridiculous road-about at Fruitville and Tamiami and go back to traffic lights. Or bulldoze the whole area and start over. You have made a terrible mess of things. | 2/23/2022 11:22 AM |
| 67 | More greenery | 2/23/2022 11:17 AM |
| 68 | Get rid of the homeless, drug dealers, drug users and protitutes. You can put all the lipstick in the world on a pig...end result? You still have a pig. | 2/23/2022 11:17 AM |
| 69 | Initiate elevated (sloped) pedestrian access from east to west of Tamiami Trail | 2/23/2022 11:15 AM |
| 70 | Overpass over Tamiami Trail to the waterfront | 2/23/2022 11:11 AM |
| 71 | Clean, trees (not Oak trees either), sidewalk cafes | 2/23/2022 11:08 AM |
| 72 | Police presence | 2/23/2022 10:58 AM |
| 73 | Pedestrian bridge | 2/23/2022 10:52 AM |
| 74 | more parking and info signs telling what & where things are offered. | 2/23/2022 10:49 AM |
| 75 | Pedestrian bridge | 2/23/2022 10:48 AM |
| 76 | Pedestrian bridges and other design features that support pedestrians. Perhaps someone should point out where Sarasota ranks in pedestrian fatalities, injuries to pedestrians | 2/23/2022 10:45 AM |
| 77 | Safety features...good lighting, safe access on foot. | 2/23/2022 10:39 AM |
| 78 | trees and green spaces | 2/23/2022 10:39 AM |
| 79 | Parleys | 2/23/2022 10:38 AM |
| 80 | None | 2/23/2022 10:29 AM |
| 81 | Provide a place for people to park away from the pedestrians and cyclists. Like the State Street garage which is easy to park and walk to restaurants. | 2/23/2022 10:27 AM |
| 82 | I would love to see a pedestrian overhead walkway for people and bikes, a safer and quicker way to access east and west of the Trail | 2/23/2022 10:26 AM |
| 83 | More restaurants and cafés. More art galleries and boutiques | 2/23/2022 10:25 AM |
| 84 | More restaurants and cafes. More art galleries and boutiques | 2/23/2022 10:25 AM |
| 85 | Safe Pedestrian and Bicycle access and crossings. | 2/23/2022 10:23 AM |
| 86 | a pedestrian walkway over Tamiami to connect it to the Bay Front is essential. | 2/23/2022 10:23 AM |
| 87 | Traffic control, longer time to cross 41, more cafes | 2/23/2022 10:23 AM |
| 88 | Parking, lights, EMERGENCY CALL BOXES, sidewalks, | 2/23/2022 10:21 AM |
| 89 | Pedestrian bridge across 41 | 2/23/2022 10:16 AM |
| 90 | pedestrian bridge, please! | 2/23/2022 10:14 AM |
| 91 | Pedestrian bridges over busy streets | 2/23/2022 10:13 AM |
| 92 | The street needs to be rethought! | 2/23/2022 10:13 AM |
| 93 | Pedestrian bridges | 2/23/2022 10:13 AM |
| 94 | Wide sidewalks to encourage walk ability | 2/23/2022 10:06 AM |
| 95 | We totally understand that the streets should be pedestrian friendly, so traffic going slow and being mindful of pedestrians is paramount, BUT, all these speed bumps are just bad for any | 2/23/2022 10:05 AM |

Boulevard of the Arts & 10th Street Complete Streets Survey

automobile or larger vehicle. Our trolleys and buses take a huge beating, even crawling over them, as many are unnecessarily too large.

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| 96 | Safety, affordable housing, green spaces, and infrastructure. | 2/23/2022 10:05 AM |
| 97 | Again pedestrian bridge X Tamiami would lead to many of the other improvements mentioned in this survey. "Build it and they will come" | 2/23/2022 10:03 AM |
| 98 | Homeless, drug abusers, panhandlers, etc. are sitting on benches and lying down on sidewalks and approaching residents, visitors and businesses making it unclean, unattractive, uncomfortable and unsafe. | 2/23/2022 10:02 AM |
| 99 | Grassy areas, pedestrian bridge over waterways | 2/23/2022 10:01 AM |
| 100 | Bike lock up area, benches, wider bike lanes, shading areas, water fountains | 2/23/2022 10:00 AM |
| 101 | Pedestrian bridge/walkway | 2/23/2022 9:59 AM |
| 102 | pedestrian bridge | 2/23/2022 9:59 AM |
| 103 | Enjoy the area in a safe, beautiful environment | 2/23/2022 9:57 AM |
| 104 | pedestrian bridges that cross highway 41. | 2/23/2022 9:55 AM |
| 105 | Pedestrian bridge to other areas | 2/23/2022 9:54 AM |
| 106 | Pedestrian bridges, outdoor dining, local privileges - please prioritize the people who live here...discounts on food/bev, parking passes, special parking spaces for Sarasota plates - we want to utilize the area where we live, work, and thrive all year round! | 2/23/2022 9:52 AM |
| 107 | A pedestrian bridge to cross tamiami | 2/23/2022 9:52 AM |
| 108 | pedestrian bridges | 2/23/2022 9:45 AM |
| 109 | Walkways over the traffic to get to the park. | 2/23/2022 9:45 AM |
| 110 | The most important are features that promote walking comfort and safety: shade trees, bulb-outs, benches, lighting. Beyond that, flexible spaces that create a destination and will help encourage people to venture beyond the currently most active section (in front of The Overton). | 2/23/2022 9:24 AM |
| 111 | Pedestrian Bridge over 41 | 2/21/2022 6:02 PM |
| 112 | more trees and vegetation and bike lanes | 2/20/2022 7:40 PM |
| 113 | Trees Trees Trees! | 2/17/2022 11:42 AM |
| 114 | Flower boxes benches bicycle racks fruit and flowering trees lots of plants make it beautiful | 2/17/2022 8:25 AM |
| 115 | A safe and easy way for pedestrians to cross Tamiami | 2/16/2022 6:49 AM |
| 116 | Pedestrian Bridge | 2/15/2022 6:25 PM |
| 117 | Bike and Ped safety | 2/15/2022 3:16 PM |
| 118 | Decreased traffic velocity. More cafes and local businesses (book stores, confectioners, gift shops, etc.) | 2/15/2022 1:55 PM |
| 119 | Continued building/tenant improvements, more usable outdoor space | 2/15/2022 1:15 PM |
| 120 | safe pedestrian crossings and bike lanes. A pocket park would be a great addition. | 2/15/2022 12:02 PM |
| 121 | No more housing developments | 2/15/2022 9:24 AM |
| 122 | More stores and green spaces | 2/15/2022 8:31 AM |
| 123 | Blvd of the Arts should be like a mini-Main Street with sidewalk cafes, more shopping, angled or on-street parking. . . . Don't forget to preserve/showcase some of the historic "Overtown" buildings. | 2/15/2022 8:15 AM |
| 124 | Clean up the homeless | 2/14/2022 11:35 PM |
| 125 | Personally, I love the rosemary district but I don't feel safe walking around. Unfortunately the homeless community makes me scared for my family. I wish there was a safer and more | 2/14/2022 10:22 PM |

Boulevard of the Arts & 10th Street Complete Streets Survey

popular way to walk from Main Street to the rosemary district area.

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| 126 | Clean up the streets and sidewalks. Planting along the road/sidewalks | 2/14/2022 6:05 PM |
| 127 | Also we need traffic enforcement to prevent blocking the intersection at 41 & Blvd of the Arts. Reduce pedestrian space to provide for freely moving automobile traffic running west to east on Blvd of the Arts. The intersection of 41 & Blvd of the Arts is badly congested and we need the ability for increases automobile traffic west to east on Blvd of the Arts | 2/14/2022 3:07 PM |
| 128 | Landscaping that creates shade | 2/14/2022 2:12 PM |
| 129 | Landscaping | 2/14/2022 1:58 PM |
| 130 | Walkover pedestrian bridge from the Bay to Rosemary Art and Design District, RADD. An artistic, sculptural walkover that speaks to the connecting ART district. | 2/14/2022 11:50 AM |
| 131 | Public parklets would be great! | 2/14/2022 11:39 AM |
| 132 | Small business,restaurants,outside cafes,trees,landscaping, | 2/14/2022 9:52 AM |
| 133 | wider sidewalks and narrower car lanes. Shade trees. Protected bicycle lanes. | 2/14/2022 9:24 AM |
| 134 | more shops and parking | 2/14/2022 9:05 AM |
| 135 | larger sidewalks for pedestrians | 2/14/2022 8:49 AM |
| 136 | Things that make the area more bike and pedestrian friendly and that attract/support business that attract pedestrians and cyclists | 2/14/2022 8:21 AM |
| 137 | A sculpture garden, safe walking, green space | 2/13/2022 6:45 PM |
| 138 | landscaping, ample lighting, wide pedestrian friendly sidewalks, shade structures | 2/13/2022 5:18 PM |
| 139 | Pedestrian bridge to The BAY | 2/13/2022 3:19 PM |
| 140 | love the idea of parklets, lighting, and pedestrian and bicycle friendly features | 2/13/2022 12:31 AM |
| 141 | Parklets would be great and lots of trees and cafes. | 2/12/2022 5:49 PM |
| 142 | Pedestrian mall. No or less cars | 2/12/2022 1:40 PM |
| 143 | General redevelopment and cleanup to make it safer and more pedestrian friendly | 2/12/2022 1:17 PM |
| 144 | Deal with the biking, enhance walking, parking close by but not in the limited street area. | 2/12/2022 12:46 PM |
| 145 | Trees cafes bike lane | 2/12/2022 12:40 PM |
| 146 | Ferris Wheel, obviously. Maybe public gardening space- demonstration food forests, pollinators | 2/12/2022 12:32 PM |
| 147 | Restaurants and galleries | 2/12/2022 12:26 PM |
| 148 | Get rid of all the glitz and stop spending money on eyesore embellishments that look like Naples!!! This (was) Sarasota and you are all misguided and ruining it! | 2/12/2022 10:31 AM |
| 149 | Make it more pedestrian friendly to support The Bay project | 2/12/2022 10:27 AM |
| 150 | Making it a walkable, attractive corridor | 2/12/2022 10:14 AM |
| 151 | A complete walking street —a sculpture garden. | 2/12/2022 10:00 AM |
| 152 | Minimum obstacles to smooth traffic | 2/11/2022 10:08 PM |
| 153 | Rent control | 2/11/2022 5:18 PM |
| 154 | I'd like to see more native plants. It's clear that all of the landscaping here isn't drought tolerant, and the space would look nicer if more of the landscaping attracted native butterflies and had native plants within it. | 2/11/2022 3:58 PM |
| 155 | Improvements for traffic flow while accommodating more space for bicycles and pedestrians as well. A major deterrence for many in these locations is often times the congestion of traffic, which can be hard to avoid. | 2/11/2022 11:40 AM |
| 156 | Make it a more affordable shopping district for the younger generation. Have breweries, cafes, beer gardens be incorporated. Also improve safety. Central ave by blvd of arts gets very | 2/11/2022 11:28 AM |

Boulevard of the Arts & 10th Street Complete Streets Survey

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| | dangerous at night | |
| 157 | Increased parking, pedestrian access | 2/11/2022 11:27 AM |
| 158 | Spend money wisely: no monuments or gateway structures, focus on pedestrian and bicycle safety, improved roadway pavement (smooth), wide sidewalks (no posts on sidewalk where people have to go around), night-time lighting. | 2/11/2022 7:41 AM |
| 159 | As much green space as possible | 2/10/2022 2:38 PM |
| 160 | Parking, street lights and landscaping. | 2/10/2022 11:17 AM |
| 161 | lights on sidewalks | 2/10/2022 12:32 AM |
| 162 | pedestrian path ways | 2/9/2022 12:01 PM |
| 163 | Keep it safe, even at night time, more businesses: cafes, mom & pop shops, police presence | 2/8/2022 6:10 PM |
| 164 | a pedestrian walkway over Tamiami Trail | 2/8/2022 6:01 PM |
| 165 | Move the parking spaces off the Blvd. of Arts and into a parking structure or side street lots. | 2/8/2022 12:58 PM |
| 166 | Safe mobility for vulnerable users | 2/8/2022 8:35 AM |
| 167 | Plant more trees | 2/8/2022 8:19 AM |
| 168 | If you do literally nothing else, just do whatever you have to do to make it extremely undesirable to drive there, and very desirable to walk there. If you do this, it will more than likely be a smashing success. | 2/8/2022 8:03 AM |
| 169 | Parklets, gateway entrance | 2/8/2022 7:43 AM |
| 170 | cool trendy businesses like st pete, on central ave | 2/8/2022 6:19 AM |
| 171 | no abandoned spaces | 2/7/2022 11:34 PM |
| 172 | Roundabout at intersection of 41 and Blvd of the Arts. | 2/7/2022 11:12 PM |
| 173 | Less traffic | 2/7/2022 11:04 PM |
| 174 | Sidewalk cafes | 2/7/2022 10:03 PM |
| 175 | More greenery. | 2/7/2022 8:44 PM |
| 176 | Speed bumps, wider walking, improving street lights and trees. | 2/7/2022 6:58 PM |
| 177 | More shade trees and parking spots | 2/7/2022 4:22 PM |
| 178 | Create art colonies to provide local artists an area to make, exhibit and sell their art. City funded facility. Not galleries but artists | 2/7/2022 1:52 PM |
| 179 | green pocket parks, maintaining low scale older structures to keep streets well lit and to reduce the tunnel wind effect - see city planning of St. Petes | 2/7/2022 1:44 PM |
| 180 | 2 years ago there was very little homeless population in the area, now it seems over run. Stop encouraging it. | 2/7/2022 1:44 PM |
| 181 | Find ways to continue fostering an artistic colony feel to the area. | 2/7/2022 1:09 PM |
| 182 | The revival of this area should be a major upgrade of the overall "hard" look that currently exists. Soften the look by making it desirable to be there with selections marked in this survey and do it ASAP | 2/7/2022 1:01 PM |
| 183 | Any and everything that gives more power to pedestrians. Get over this car centric thinking. Let us be able to walk, bicycle, create electric trolleys. | 2/7/2022 12:53 PM |
| 184 | Need a park! | 2/7/2022 12:18 PM |
| 185 | Pedestrian bridge | 2/7/2022 12:16 PM |
| 186 | Leave it alone. We enjoy our peace and quiet | 2/7/2022 12:13 PM |
| 187 | PEDX BRIDGE OVER 41 REGARDING A GATEWAY IDEA - WOULD LIKE TO SEE A PEDX BRIDGE OVER 41 FIRST AND FAST - THERE ARE ALREADY THE EXISITING BRICK RD | 2/7/2022 11:40 AM |

Boulevard of the Arts & 10th Street Complete Streets Survey

PILLARS AND THESE SHOULD BE PRESERVED AND ENHANCED - THE EXISTING ROAD WOOD SIGNS ARE PRETTY SHABBY AND THE ONE IN FRONT OF ARCOS (Central Ave) IS SO SAD AND NOW WITH THE THE HUGE APTS IT DOESN'T SEEM TO BE IN THE BEST LOCATION NOW.

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| 188 | The pedestrian bridge across US 41 is critical to the interaction between the Rosemary District and the Bay Development. It has been functional but also an "art" piece that engages the public. You used an example of the High Line. I also look at the Pier in St Pete where it is so engaging. | 2/7/2022 11:29 AM |
| 189 | bike friendly, outdoor cafes | 2/7/2022 11:24 AM |
| 190 | Flexible spaces. Overhead bridge for pedestrians. | 2/7/2022 10:33 AM |
| 191 | Wide walkways for pedestrians and their safety. Public art, trees, and street furniture for a beautifully designed area. | 2/7/2022 10:13 AM |
| 192 | PEDESTRIAN BRIDGE TO THE BAY AREA. | 2/7/2022 9:46 AM |
| 193 | More "art" on the boulevard named for the arts | 2/7/2022 9:25 AM |
| 194 | More accessibility for walking | 2/7/2022 8:44 AM |
| 195 | A pedestrian bridge across 41 to the Bay Park. Please! | 2/7/2022 8:03 AM |
| 196 | Lower speed limit | 2/7/2022 7:30 AM |
| 197 | Pedestrian bridge over Tamiami Trail to the Bay. | 2/7/2022 7:30 AM |
| 198 | We live in the Rosemary district and live to walk and the lights and traffic that need to be watched to cross the street are awful. We could stand there for what seems like such a long time to be able to safely cross. We have seen people dart out to try to beat the traffic and it's dangerous. We NEED a pedestrian bridge like they have all over Fort Lauderdale (I think that's where I saw them). They were amazing and you could easily cross. There is going to be a huge increase in population waking to the park and having people not be able to cross safely and quickly is not only frustrating but dangerous for whether we like it or not someone will dart across and there will be an accident. If you want to maximize the use of the park and make it all work together put in a pedestrian bridge. It would be life changing and would make those of us who use it frequently very happy | 2/7/2022 7:30 AM |
| 199 | Improve access to Bayfront for pedestrians and cyclists. Reduce congestion and slow traffic. | 2/7/2022 7:08 AM |
| 200 | PRking,cafes | 2/7/2022 2:48 AM |
| 201 | Close the road and use as marketplace | 2/7/2022 12:07 AM |
| 202 | Trees and landscaping | 2/6/2022 8:51 PM |
| 203 | PROTECTED bike lanes, bike racks | 2/6/2022 5:20 PM |
| 204 | Crossing 41 at Blvd of the arts can be dangerous. Overhead walkway would be wonderful. I have almost gotten run over by motorists not paying attention while I was crossing. | 2/6/2022 2:51 PM |
| 205 | Easier egress across the trail by auto. You forget 1000 people live on this street and need deliveries, access and emergency service. Keep your park in your boundary | 2/6/2022 12:56 PM |
| 206 | DEFINITELY remove the turn lanes, and put the lanes on a street diet. Reclaim the space for pedestrians and to shorten the crossing distance. Those turn lanes favor a very small # of cars over pedestrian crossing safety. | 2/6/2022 12:53 PM |
| 207 | No traffic!!!!!! | 2/6/2022 12:20 PM |
| 208 | Fix the sidewalks | 2/6/2022 12:10 PM |
| 209 | Wider side walks, sidewalk benches or parkbenches and more planted trees | 2/6/2022 11:11 AM |
| 210 | walkable community | 2/6/2022 10:37 AM |
| 211 | More pedestrian-friendly cafe/restaurant/shop access | 2/6/2022 10:24 AM |
| 212 | Making it more pedestrian-friendly | 2/6/2022 10:09 AM |

Boulevard of the Arts & 10th Street Complete Streets Survey

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| 213 | consider the homeless problem in all planning, i.e. park benches downtown can't be used because it is a homeless enclave; consider development goals on Main Street business | 2/6/2022 10:09 AM |
| 214 | Any and all improvements that would make this potentially charming new neighborhood viable and alive instead of being "dead". | 2/6/2022 10:05 AM |
| 215 | Continuing efforts at restoration of Rosemary Cemetery, collaborating with the Rosemary Committee to continue with the New College Public Archaeology Lab efforts in the early 2000s | 2/6/2022 9:32 AM |
| 216 | Close off some side streets to cars and add casual dining | 2/6/2022 9:25 AM |
| 217 | We need a pedestrian bridge over Tamiami! | 2/6/2022 9:15 AM |
| 218 | Walkway across route 41. | 2/6/2022 9:11 AM |
| 219 | Less parking spaces...pavers and center median landscaped better | 2/6/2022 8:44 AM |
| 220 | pedestrian bridge across Tamiami | 2/6/2022 8:39 AM |
| 221 | More outdoor casual dining in a walk only area | 2/6/2022 8:15 AM |
| 222 | Better pedestrian connectivity to downtown. Right now it's isolated by Fruitville. | 2/6/2022 7:52 AM |
| 223 | More public parking. | 2/6/2022 7:23 AM |
| 224 | Pedestrian overpass that also serves as a piece of art | 2/6/2022 4:17 AM |
| 225 | More Greenery. The Bay Park would be more flexible if it would be a Vast Green Space. It has enough pocket features like the Reading Nook, etc. Otherwise, it might be too cluttered. A vast scenic and peaceful Green sanctuary are best, like Central Park in NYC. I'm Not keen on the Amphitheater near the Van Wezel. The cement seating would be Too Hot to sit on. That area should be All Green. Outdoor concerts can be done with pop-up stages and flexible seating like Concerts in the Park in NYC. | 2/6/2022 12:11 AM |
| 226 | Pedestrian overpass at Tamiami and Blvd of the Arts | 2/5/2022 9:56 PM |
| 227 | If cafe's and storefronts are encourage please have a focused intent on keeping the sidewalks clean. It may come in the form of lease agreements or building owner accountability. | 2/5/2022 9:35 PM |
| 228 | Greenery, markets, outdoor dining | 2/5/2022 9:27 PM |
| 229 | Landscaping. | 2/5/2022 8:38 PM |
| 230 | East side of 6th needs a updo. Trees benches parklets flowers brighten lights music | 2/5/2022 8:09 PM |
| 231 | Make sure all future buildings are set back from the sidewalks in order to have more cafe seating and gathering room. | 2/5/2022 5:46 PM |
| 232 | Create a classic style for celebration of the arts and historical Sarasota. | 2/5/2022 5:37 PM |
| 233 | Gateway Structure | 2/5/2022 5:25 PM |
| 234 | I want to be able to move across 41 both ways safely. How about a pedestrian bridge? | 2/5/2022 4:25 PM |
| 235 | Wide, shaded sidewalks that promote walking | 2/5/2022 4:15 PM |
| 236 | Widen the road to accommodate more amenities | 2/5/2022 4:11 PM |
| 237 | Welcoming street cafes - bike/walking paths | 2/5/2022 4:01 PM |
| 238 | Some cafes and coffee shops | 2/5/2022 4:00 PM |
| 239 | Vacant lots improved | 2/5/2022 3:53 PM |
| 240 | Placing of art objects | 2/5/2022 3:48 PM |
| 241 | the arts, boutique shops | 2/5/2022 3:44 PM |
| 242 | need more parking in order for people to get there and stay there and shop etc | 2/5/2022 3:37 PM |
| 243 | None | 2/5/2022 3:26 PM |
| 244 | all of the above | 2/5/2022 3:07 PM |

Boulevard of the Arts & 10th Street Complete Streets Survey

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| 245 | Well lit at night, wide sidewalks. | 2/5/2022 3:07 PM |
| 246 | More greenery, dedicated bike lanes, shaded benches | 2/5/2022 3:05 PM |
| 247 | Recycling bins, pedestrian crosswalks | 2/5/2022 2:27 PM |
| 248 | Get rid of the Homeless and Drug Dealers | 2/5/2022 2:14 PM |
| 249 | Greenery, parking nearby | 2/5/2022 2:13 PM |
| 250 | Low speed limit (15 as on Main Street) Lighting and wide sidewalks | 2/5/2022 2:11 PM |
| 251 | More bicycle and pedestrian friendly areas....would like to see these all over the city. Would be great if it was ALL PEDESTRIAN - ALL THE TIME! | 2/5/2022 2:11 PM |
| 252 | Everything that contributes to the feeling of a clean, safe, and park-like environment while enjoying commercial activities like dining and specialty shopping. | 2/5/2022 1:57 PM |
| 253 | Lighted trees like on Osprey | 2/5/2022 1:55 PM |
| 254 | A pedestrian cross over Tamiami Trl. | 2/5/2022 1:47 PM |
| 255 | Opportunities for people to come together in safety | 2/5/2022 1:33 PM |
| 256 | Safe traffic flow better visibility ie big trucks parking near corner and blocking sight to drivers at intersections. | 2/5/2022 1:19 PM |
| 257 | Easier parking | 2/5/2022 1:01 PM |
| 258 | Sidewalks, trees and parking | 2/5/2022 1:00 PM |
| 259 | TOO MUCH EMPHASIS ON THE WHOLE DESIGN - WHY NOT TAKE CARE OF Main Street FIRST RATHER THAN CONTINUE TO MAKE IT A HAVEN FOR HOMELESS - MEN URINATING ON THE SIDEWALK AND EXPOSING THEMSELVES ETC | 2/5/2022 12:29 PM |
| 260 | More landscaping | 2/5/2022 12:23 PM |
| 261 | overhead crossing for pedestrians and bicyclists | 2/5/2022 12:23 PM |
| 262 | Sign Narratives on the History of Sarasota and the Islands at various locations incorporated in vest-pocket parks | 2/5/2022 12:21 PM |
| 263 | No parking!!!! Green spaces, walkability , outdoor cafes and high end shops and restaurants | 2/5/2022 12:16 PM |
| 264 | Pedestrian friendly access! Overpass into Bay Park and access to future PAC | 2/5/2022 12:09 PM |
| 265 | Pedestrian bridge over Tamiami that connects with the Bay | 2/5/2022 11:52 AM |
| 266 | Upgraded and attractive lighting and landscaping. | 2/5/2022 11:50 AM |
| 267 | pedestrian bridges. One at Fruitville. One at Blvd of the Arts. | 2/5/2022 11:48 AM |
| 268 | Landscaping, widening of sidewalks | 2/5/2022 11:43 AM |
| 269 | Repeated: an artistic walkway over Tamiami Tr at BoTA, | 2/5/2022 11:33 AM |
| 270 | anything that makes it more "inviting" | 2/5/2022 11:32 AM |
| 271 | Parking, nature plantings, accessible walking | 2/5/2022 11:30 AM |
| 272 | All mentioned above | 2/5/2022 11:23 AM |
| 273 | Improved sidewalks, landscaping | 2/5/2022 11:16 AM |
| 274 | More trees, bicycle lanes | 2/5/2022 11:14 AM |
| 275 | A foot bridge to the Bay Park | 2/5/2022 11:03 AM |
| 276 | Clean up sidewalks and improve landscaping. More seating. Rezone so new buildings aren't so close to sidewalks. | 2/5/2022 10:53 AM |
| 277 | A clean, safe bathroom | 2/5/2022 10:51 AM |
| 278 | More pedestrian friendly - a bridge to get to the Bay (the new circle at 10th is dangerous for pedestrians). | 2/5/2022 10:46 AM |

Boulevard of the Arts & 10th Street Complete Streets Survey

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| 279 | Definitely NO speed humps, NO additional traffic lights. There are already way too many! | 2/5/2022 10:41 AM |
| 280 | Trees, green infrastructure for stormwater | 2/5/2022 10:38 AM |
| 281 | Bike lanes and better landscaping. | 2/5/2022 10:15 AM |
| 282 | Walking bridge to the west side of Blvd of the Arts. Much more green space! | 2/5/2022 10:09 AM |
| 283 | Move the Salvation Armu away from the District | 2/5/2022 10:04 AM |
| 284 | more green space. pedestrian bridges. | 2/5/2022 10:02 AM |
| 285 | More places to stop and visit with other people. | 2/5/2022 10:00 AM |
| 286 | Better access across 41 | 2/5/2022 9:45 AM |
| 287 | Pedestrian lights at the intersection of Blvd of the Arts and Coconut. | 2/5/2022 9:41 AM |
| 288 | Smoother flow of traffic. | 2/5/2022 9:38 AM |
| 289 | vehicle-free zones | 2/5/2022 9:37 AM |
| 290 | All of the above. Eventually. | 2/5/2022 9:35 AM |
| 291 | nice even sidewalks without cracks and breaks...no bricks. | 2/5/2022 9:32 AM |
| 292 | Better sidewalks all through out | 2/5/2022 9:32 AM |
| 293 | Wider road | 2/5/2022 9:30 AM |
| 294 | More green spaces and sidewalk cafes | 2/5/2022 9:26 AM |
| 295 | We need to build a pedestrian bridge over Tamiami Trail to the Bay | 2/5/2022 9:24 AM |
| 296 | Pedestrian Bridge over US 41 | 2/5/2022 9:21 AM |
| 297 | Pedestrian bridges over Tamiami Trail | 2/5/2022 9:15 AM |
| 298 | Doggy bags | 2/5/2022 7:23 AM |
| 299 | Bike lanes | 2/5/2022 6:27 AM |
| 300 | Definitely no gateway! Might need clearer signage - yesterday I saw someone turn east off Tamiami into westbound blvd of arts lane. But hard to account for stupidity... street is very wide and not very pedestrian friendly - narrowing it with more trees and walking spaces on either side would help. Or continue island further east - at least gives people a refuge while crossing. | 2/5/2022 6:14 AM |
| 301 | Barrier bike lanes, | 2/5/2022 5:52 AM |
| 302 | E.V. charging. | 2/5/2022 1:49 AM |
| 303 | Canopies that shield pedestrians from rain and summer heat. (Or adequate trees.) | 2/5/2022 12:43 AM |
| 304 | Bike lanes, sidewalk cafes, parklettes, festival areas | 2/4/2022 9:41 PM |
| 305 | unknown | 2/4/2022 9:34 PM |
| 306 | Right Hand Turn Lanes. | 2/4/2022 4:36 PM |
| 307 | No need for a gateway. They make it feel like an amusement park or race track. Inappropriate in ANY part of the city. | 2/4/2022 4:28 PM |
| 308 | Right now, there's nothing to entice me there | 2/4/2022 4:13 PM |
| 309 | Sheltered bike lanes! | 2/4/2022 4:03 PM |
| 310 | Some kind of police presence. | 2/4/2022 3:18 PM |
| 311 | APS SIGNALS at crosswalks | 2/4/2022 10:43 AM |
| 312 | I would like to see more opportunities to "stop and stay a while". Right now it is just too hard to find a place to park. It is hard to see the pedestrians with the way the road is set up and I feel it is unsafe for them. The area just seem not finished and undefined. | 2/4/2022 8:52 AM |

Boulevard of the Arts & 10th Street Complete Streets Survey

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| 313 | Sidewalk cafes | 2/4/2022 8:34 AM |
| 314 | For the most part, a slight extension would go a long way for the city. | 2/3/2022 10:51 PM |
| 315 | The bay | 2/3/2022 10:26 PM |
| 316 | SAFETY!!!! | 2/3/2022 4:16 PM |
| 317 | Removal of on street parking | 2/3/2022 3:47 PM |
| 318 | A roundabout would cut-down on vehicles attempting to reverse direction by pulling a ' u-ee ' into both directions, north and south on 41. | 2/3/2022 2:05 PM |
| 319 | Wider sidewalks. Street trees. Art installations. Roundabouts. | 2/3/2022 2:03 PM |
| 320 | Close the salvation army location. Get rid of the homeless drug addicts and mentally ill. | 2/3/2022 11:15 AM |
| 321 | parklets, the gateway is unnecessary | 2/3/2022 11:14 AM |
| 322 | Remove overhead electric lines as they are unsightly. Fresh landscaping would help a lot. Less homeless people lying on sidewalks would be a big plus--get them the supervisory mental health care they need. | 2/3/2022 9:42 AM |
| 323 | because of lots of homeless folks i would stay away from water features i:e fountains and such it will become a public bath house | 2/3/2022 8:53 AM |
| 324 | How about improving further down | 2/3/2022 8:50 AM |
| 325 | wider bike lanes, wider sidewalks, green space, better crossings | 2/3/2022 8:12 AM |
| 326 | Bike lanes | 2/3/2022 8:08 AM |
| 327 | All of the above mentioned. | 2/3/2022 7:19 AM |
| 328 | More trees. Fewer cops. | 2/3/2022 3:47 AM |
| 329 | I want to be sure the homeless population is not driven from this area. With the Salvation Army there, this is their home an safe space, too. They need to be accommodated. | 2/2/2022 8:41 PM |
| 330 | Find a way to have fewer homeless hanging at the bust stops and sidewalks. | 2/2/2022 8:40 PM |
| 331 | Make more friendly for the homeless population. Just don't run them out. | 2/2/2022 8:22 PM |
| 332 | social activities in order to gather the community | 2/2/2022 4:48 PM |
| 333 | More police presence. | 2/2/2022 4:19 PM |
| 334 | Address homeless population | 2/2/2022 4:06 PM |
| 335 | Street performances without a big crowd gathering. Something where music/ performances can be enjoyed by people passing through. Not something that caused congestion, crowds and chaos. | 2/2/2022 2:58 PM |
| 336 | more houses less roads | 2/2/2022 2:19 PM |
| 337 | Public art installations. Shared streets for cars/cyclists/pedestrians. Water bottle filling stations. Shaded sidewalks. | 2/2/2022 1:43 PM |
| 338 | Decreased homeless presence | 2/2/2022 1:40 PM |
| 339 | I'd like to see vibrant businesses - restaurants, shops, services - and a nice treed street. | 2/2/2022 1:39 PM |
| 340 | Safe pedestrian crosswalks and ENFORCEMENT of pedestrian right-of-way. I get "almost run over" daily. | 2/2/2022 1:33 PM |
| 341 | Note that item 3 should include some options regarding the use of automobiles instead of just including options that are anti automobile. It reminds me of the invalid Sarasota In Motion surveys which also did not include any ranking options to allow people to express their need or preference for the City to be accessible by car. Please remember that people live on the barrier islands also. The more inconvenient it is to travel to and park in an area the less it will be patronized by those who don't live within walking distance. | 2/2/2022 1:28 PM |
| 342 | unknown | 2/2/2022 1:02 PM |

Boulevard of the Arts & 10th Street Complete Streets Survey

| | | |
|-----|---|-------------------|
| 343 | Pedestrian and bicycle safety with green scape | 2/2/2022 12:21 PM |
| 344 | More shade/trees/greenery for walking. | 2/2/2022 11:01 AM |
| 345 | Eliminate vehicle access and make street for pedestrians only | 2/2/2022 10:44 AM |
| 346 | Check out Pearl St. in Boulder, Colorado. Now that is a vision! No cars and it's a lovely place to walk around and not having to worry about getting run over. There is nothing really like it here and yet we have one of the highest rates of pedestrians & bicyclists getting hit by cars. There are plenty of other streets for people to drive on- a pedestrian/bike only street would draw both business and shoppers to have a unique experience without having to worry. It also provides a place for artists and performers to showcase. | 2/2/2022 10:38 AM |
| 347 | Very quaint now but all brick road to discourage cut throughs for people in a hurry to get to work. Room for current and new restaurants to add more outdoor dining areas (you have already succeeded in the one area at the Overton and Spice Station, etc). Parking is a problem on Blvd of the Arts but we just park on Lemon...usually lots of spaces north of Blvd of the Arts and Lemon. Not much room for anything on the north side of Blvd of the Arts but the south side has a junky looking area across the street...chain length fence area. Would make a nice, small park. We like the radio area so no drastic changes there. The corner of Blvd of the Arts and Lemon next to Mandoville could use some sprucing up...maybe another small park area? We love Blvd of the Arts and appreciate the light at 41 and hope no circle is planned there. Maybe a park at the corner of Blvd of the Arts and Orange in that nice big lot. Please no more apt. bldgs. Thanks for asking for opinions! | 2/2/2022 10:29 AM |
| 348 | More shade & greenery, more cafes | 2/2/2022 9:36 AM |
| 349 | Bridge over Tamiami trail to make safer passage to bayfront | 2/2/2022 9:34 AM |
| 350 | Landscaping | 2/2/2022 8:54 AM |
| 351 | I think gateway structures are waste of capital and maintenance money. Sarasota can't even keep the Marina Jacks park gateway intact. | 2/2/2022 7:53 AM |
| 352 | Good bike lanes, fewer cars | 2/2/2022 7:35 AM |
| 353 | Trees, lighting, traffic calming, parklets, wider sidewalks (and cleaner sidewalks) a dog park and lots of sidewalk cafes | 2/2/2022 7:34 AM |
| 354 | More affordable living | 2/2/2022 7:30 AM |
| 355 | Green spaces, cafes, bike lanes | 2/2/2022 7:19 AM |
| 356 | Trees | 2/2/2022 6:53 AM |
| 357 | Parking | 2/2/2022 5:00 AM |
| 358 | Shade trees and greenery and practical att | 2/2/2022 12:50 AM |
| 359 | More green space, better parking and outdoor areas for pedestrians (and bike trails). | 2/2/2022 12:48 AM |
| 360 | More shaded areas with benches. Free Publix parking. | 2/2/2022 12:24 AM |
| 361 | Limited traffic with parking garage | 2/2/2022 12:19 AM |
| 362 | Parklets | 2/1/2022 11:30 PM |
| 363 | Pedestrian friendly gathering. Green space. | 2/1/2022 11:23 PM |
| 364 | More police presence. | 2/1/2022 11:05 PM |
| 365 | more parking | 2/1/2022 10:21 PM |
| 366 | Easy Connection to the bay | 2/1/2022 9:52 PM |
| 367 | The Parklets look great also! | 2/1/2022 9:28 PM |
| 368 | parklets, trees, benches, bike racks | 2/1/2022 8:52 PM |
| 369 | Landscaping, reduced noise, slower vehicle speeds. | 2/1/2022 8:43 PM |
| 370 | Pedestrian walkway across 41 to the Amazing new bay park, additional lighting | 2/1/2022 8:31 PM |

Boulevard of the Arts & 10th Street Complete Streets Survey

| | | |
|-----|--|------------------|
| 371 | Landscaping, benches, recycle and trash bins | 2/1/2022 8:06 PM |
| 372 | More trash receptacles and dog waste station, better lighting, bike lanes | 2/1/2022 8:05 PM |
| 373 | Keep cleaning it up and eliminate all trash and all old structures that no longer serve a purpose and are magnets for the homeless and drug users. | 2/1/2022 7:57 PM |
| 374 | Affordable housing that those of us who work in the local businesses can afford. To not gentrify the area and cater to wealthy out-of-towners | 2/1/2022 7:51 PM |
| 375 | More tickets given to pedestrians who don't use cross walks | 2/1/2022 7:39 PM |
| 376 | -More palm trees -Non parallel street parking -Local coffee shops -Direct access to hotels and marina | 2/1/2022 7:19 PM |
| 377 | Priority for foot and bike travel and safety. A no car center festival area with quarterly rotation of local Florida artists and sculptures-it is called Blvd of the Arts after all. | 2/1/2022 6:41 PM |
| 378 | I'm only going to go that way because of the local businesses. What do the local businesses need to help survive? | 2/1/2022 6:40 PM |
| 379 | Coherent street scape, landscaping pedestrian friendly and attractive | 2/1/2022 6:29 PM |
| 380 | Trees, horseback police or officers on bicycle s as a friendly welcoming presence. | 2/1/2022 6:25 PM |
| 381 | More shade trees and wider sidewalks | 2/1/2022 6:08 PM |
| 382 | More stores | 2/1/2022 5:57 PM |
| 383 | Less homeless | 2/1/2022 5:41 PM |
| 384 | Street level cafes, features that advocate biking, walking, bike/scooter sharing, pedestrian safety measures, appropriately designed parking garage if solid use analysis justifies it. | 2/1/2022 5:37 PM |
| 385 | provide flexible space for various functions while allowing for accessibility. also suggest using of paver bricks for the road portion to be adjoined to the sidewalk | 2/1/2022 5:23 PM |
| 386 | Wide sidewalks | 2/1/2022 5:20 PM |
| 387 | restrictive vehicular travel lanes, there are plenty of opportunities to navigate around the area without using this roadway segment | 2/1/2022 5:18 PM |
| 388 | landscape, pocket park, parklets, dog/trash facilities | 2/1/2022 5:17 PM |
| 389 | Restaurants a flow to enter the new Bayfront Park outdoor cafe's where dogs are invited. Even a doggie Park for walkers. There is one on 11th so possibly use that with a promenade access | 2/1/2022 5:08 PM |
| 390 | Benches, lighting, bike racks and scooter racks, garbage cans. Open spaces with local art structures and spaces for festivals. | 2/1/2022 5:07 PM |
| 391 | trees, bike lanes | 2/1/2022 5:05 PM |
| 392 | improved lighting and landscaping | 2/1/2022 5:03 PM |
| 393 | Palm trees lining the streets | 2/1/2022 5:02 PM |
| 394 | Dog friendly park area | 2/1/2022 5:00 PM |
| 395 | Parking | 2/1/2022 4:50 PM |
| 396 | Landscaping, bike lane | 2/1/2022 4:41 PM |
| 397 | Dress it up, make it walker friendly, well lit. No food trucks please. | 2/1/2022 4:25 PM |
| 398 | Favor the pedestrian first. | 2/1/2022 4:19 PM |
| 399 | Anything to enhance walkable enjoyment. | 2/1/2022 4:14 PM |
| 400 | green spaces | 2/1/2022 4:13 PM |
| 401 | More walkable, slower traffic | 2/1/2022 4:09 PM |
| 402 | Many more trees and park spaces. Cafes that encourage relaxing and enjoying, and shaded spaces | 2/1/2022 4:06 PM |

Boulevard of the Arts & 10th Street Complete Streets Survey

| | | |
|-----|---|-------------------|
| 403 | More pedestrian friendly. | 2/1/2022 4:06 PM |
| 404 | Better landscaping, areas for festivals and markets | 2/1/2022 3:45 PM |
| 405 | More pedestrian friendly | 2/1/2022 3:42 PM |
| 406 | Better sidewalks. Fix where cracked/broken. More 'dramatic' storefront/architecture features. Better lighting. Options for shade-trees, fabric 'sails' for shelter during hot summer & canopies over benches along popular pedestrian paths to rest | 2/1/2022 3:31 PM |
| 407 | connect the two areas better | 2/1/2022 3:30 PM |
| 408 | More of a friendly neighborhood vibe. | 2/1/2022 3:26 PM |
| 409 | Signage about the Gumbo Limbo trees along the block between Central and Lemon. They are unique and FL Native. Also, water filling stations and recycling bins. Walkable areas like Dunedin. Areas with shade. Maybe a trolley that goes to other parts of downtown. | 2/1/2022 3:14 PM |
| 410 | Space for fests. | 2/1/2022 3:05 PM |
| 411 | Make sure that an ambulance can self get me to the hospital, or any of my neighbors should we need it , God Forbid. | 2/1/2022 3:01 PM |
| 412 | Dedicated bike lanes, more trees and public spaces | 2/1/2022 3:00 PM |
| 413 | widen sidewalks and add more shade trees | 2/1/2022 2:52 PM |
| 414 | Improvements that would make it more of a destination. Like the idea of cafe's and Parklets. | 2/1/2022 2:45 PM |
| 415 | What if it was pedestrian only in the evenings? A central place to hang out in. More fun resteraunts and bars. Open containers allowed. | 2/1/2022 2:44 PM |
| 416 | You need to decide what it can best do for the area. Because it's a key artery, it might be difficult to allow a vibrant pedestrian life. | 2/1/2022 2:32 PM |
| 417 | Wider sidewalks for pedestrians with better nighttime lighting for walkers who walk from the areas near Centennial Park/Cocoanut area and use BOA as their feeder road into downtown to get to key areas downtown -- Opera & Florida Studio Theatre using Cocoanut, Selby Library using Central, Mandeville Beer Garden/Whole Foods/Farmers Market using Lemon, and the Shops along Orange. It would also help if there were 5/6 central "depots" of scooter-sharing options (i.e. Lime, Bird etc) where visitors to The Bay could pick up a scooter near the Bay Park/Art Center area and ride down BOA with other depots at/near Selby Library, Whole Foods, and then the Bayfront Park (Marina Jack area). This would allow visitors and residents to park once for the day (near either Bayfront Park or The Bay) and use e-scooters and walking to enjoy both sides of the Bay and all that the Rosemary District and Downtown has to offer. This doesn't mean that we have to turn the entire city into a free-for-all for e-scooters with them left all over town. We'd have to adjust the service provided by either the city or a private company to be a fixed point-to-point or point to multi-point service. Think of the subway/bus spoke system used in major cities -- our equivalent would be a walk/scooter system with each scooter depot being no more than a 5-10 minute walk between depots (no more than a 1/2 mile walk from one scooter depot to another. | 2/1/2022 2:22 PM |
| 418 | Bike lanes | 2/1/2022 1:45 PM |
| 419 | Previously addressed | 2/1/2022 1:09 PM |
| 420 | Parklets look fun. Free Parking; on street and a garage. More landscaping, especially shade providing trees and features. Street lights. | 2/1/2022 1:06 PM |
| 421 | Smaller curb cuts (too wide and dangerous to cross as a pedestrian, especially for a low traffic roadway) and more shade trees (I.e., not palms), | 2/1/2022 1:06 PM |
| 422 | Just make it so people like me can actually walk around the darn block using some kind of mobility assistance! I haven't been able to do so since the Valencia Townhomes overtook the corner of 6th & Cocoanut. | 2/1/2022 1:02 PM |
| 423 | The "Arts" is missing. Spray paintings are simple, but we need big art pieces. Touchable art, something that inspires us to think about our environment. | 2/1/2022 1:02 PM |
| 424 | Trolley or jitney circulating through Downtown area with stops at public parking garages outside neighborhood. No additional parking on BotA; surface parking on adjacent streets. Limit | 2/1/2022 12:55 PM |

Boulevard of the Arts & 10th Street Complete Streets Survey

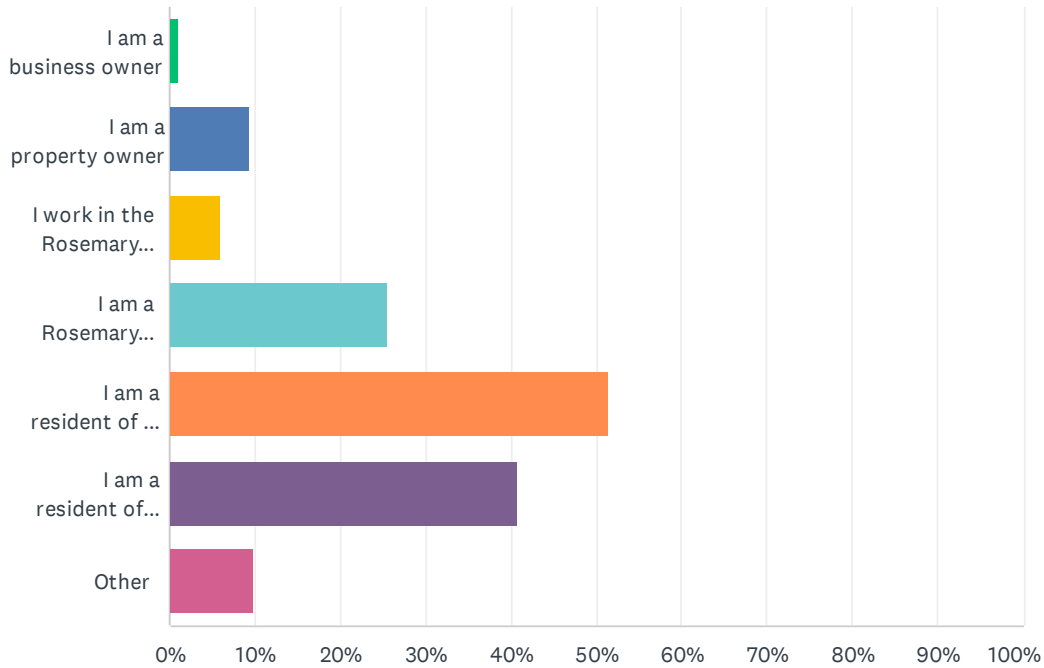
| | | |
|-----|---|-------------------|
| | through-traffic | |
| 425 | All of the above that I mentioned. I DO NOT want a parking garage. | 2/1/2022 12:41 PM |
| 426 | Defined bike lanes with barriers | 2/1/2022 12:40 PM |
| 427 | Microforests | 2/1/2022 12:34 PM |
| 428 | PROTECTED bike lanes | 2/1/2022 12:31 PM |
| 429 | Affordable housing opportunities, | 2/1/2022 12:29 PM |
| 430 | Please make sidewalks wider and more walkable with shade trees. | 2/1/2022 12:28 PM |
| 431 | Better lighting and more protection for cyclists and pedestrians from cars. Safe connection to downtown. | 2/1/2022 12:22 PM |
| 432 | More shade trees | 2/1/2022 12:19 PM |
| 433 | Less cars more pedestrian activity | 2/1/2022 12:16 PM |
| 434 | Must maintain automobile access on BOTA across 41, from west end of BOTA to rosemary district. Need to address homeless problem/security More trees, bigger setback from sidewalk for large buildings | 2/1/2022 11:56 AM |
| 435 | Keep the space as car free and pedestrian and bike friendly as possible. Make it a safe space for pedestrians to shop and enjoy the local businesses and any events the are hosted here. | 2/1/2022 11:55 AM |
| 436 | Get rid of the homeless element, would be a nice place to visit then | 2/1/2022 11:55 AM |
| 437 | Safe areas for homeless people. There are a lot in the area. On the edge of a very rough area. | 2/1/2022 11:46 AM |
| 438 | wider sidewalks, better lighting, more commerce | 2/1/2022 11:40 AM |
| 439 | take responsibility for the gentrification and displacement this is causing, honor the Black historical context of Rosemary District. | 2/1/2022 11:38 AM |
| 440 | Underground utilities A bridge walkway starting between Cocoanut Ave and Tamiami Trail and ending in The Bay Park | 2/1/2022 11:38 AM |
| 441 | Blvd of the Arts could become an active, beautiful, and pedestrian/biker friendly experience. With thoughtfulness and commitment, the city of Sarasota has an amazing opportunity to create world-class street/neighborhood life within an area of the city that is rapidly gaining density. The focus should be on pedestrian, biker, and resident quality of life, as these are the attributes that now draw and retain young and old alike to cities. Think Asheville/Charleston rather than Atlanta/Dallas. | 2/1/2022 11:18 AM |
| 442 | See above | 2/1/2022 11:08 AM |
| 443 | More trees and cleaned up properties that are either owned by individuals, large residential buildings or empty lots. More murals on buildings. Paint McCown Towers and Allow pedestrians to walk through from Cocoanut on 5th street. | 2/1/2022 11:00 AM |
| 444 | Fewer cars, more shade, misting fans/cooling stations | 2/1/2022 10:54 AM |
| 445 | More green space, more community gathering spaces. | 2/1/2022 10:51 AM |
| 446 | Trees, more lighting, cafes and restaurants | 2/1/2022 10:47 AM |
| 447 | Raised walkways over the street so pedestrians don't have to cross busy roads | 2/1/2022 10:44 AM |
| 448 | more pedestrian friendly, more cafes and outdoor seating | 2/1/2022 10:40 AM |
| 449 | As is, BOTA is easy to bike because traffic is slow and drivers expect to see lots of pedestrians. So even as a frequent cyclist, I would prioritize street trees over bike lanes. | 2/1/2022 10:37 AM |
| 450 | Please add canopy trees....lots of these to 'frame' the street and provide more shade | 2/1/2022 10:37 AM |
| 451 | Make it more open to cafes and businesses to attract more pedestrians | 2/1/2022 10:34 AM |
| 452 | safe areas | 2/1/2022 10:26 AM |
| 453 | Be more like a European walking city. | 2/1/2022 10:25 AM |

Boulevard of the Arts & 10th Street Complete Streets Survey

| | | |
|-----|--|--------------------|
| 454 | Much more pedestrian friendly. | 2/1/2022 10:15 AM |
| 455 | Honestly the biggest biggest problem is people experiencing homelessness... The trash left on the streets people sleeping on the street and in front of businesses... The protesters constantly in front of Planned Parenthood... Some other things need to change in order for this to feel more family friendly for the residents of Sarasota. | 2/1/2022 10:09 AM |
| 456 | All of the above | 2/1/2022 10:01 AM |
| 457 | Upgraded consistently spaced street and sidewalk lighting. Vividly apparent stop signs for cross streets. Vividly painted crosswalks with signage to ensure motorists are aware of possible pedestrian activity. | 2/1/2022 10:00 AM |
| 458 | More green space | 2/1/2022 9:30 AM |
| 459 | Space for local artisans perhaps on a rotating basis, antique stores like the ones on fruitville, it's a shame the convenience store closed. More retail less office. | 2/1/2022 9:28 AM |
| 460 | An overpass | 2/1/2022 9:24 AM |
| 461 | Food truck parking, better pedestrian lighting, a place for the homeless to freshen up like public bathrooms | 2/1/2022 9:22 AM |
| 462 | Improved parking and safe walking areas to access businesses after arriving .. cafes and meeting areas with benches , landscaping | 2/1/2022 9:21 AM |
| 463 | Pop up spaces for food trucks | 2/1/2022 9:13 AM |
| 464 | Less spending to keep taxes low | 2/1/2022 9:05 AM |
| 465 | A traffic light that doesn't take 5 minutes to change. | 2/1/2022 9:01 AM |
| 466 | Easier, Faster, Safer crossing of Tamiami & Fruitville | 2/1/2022 8:58 AM |
| 467 | Designated areas for outdoor night markets like kiosks or built in spaces for small businesses to rent for short and long periods of time. | 1/28/2022 10:17 AM |

Q10 What is your connection to 10th Street? Select all that apply.

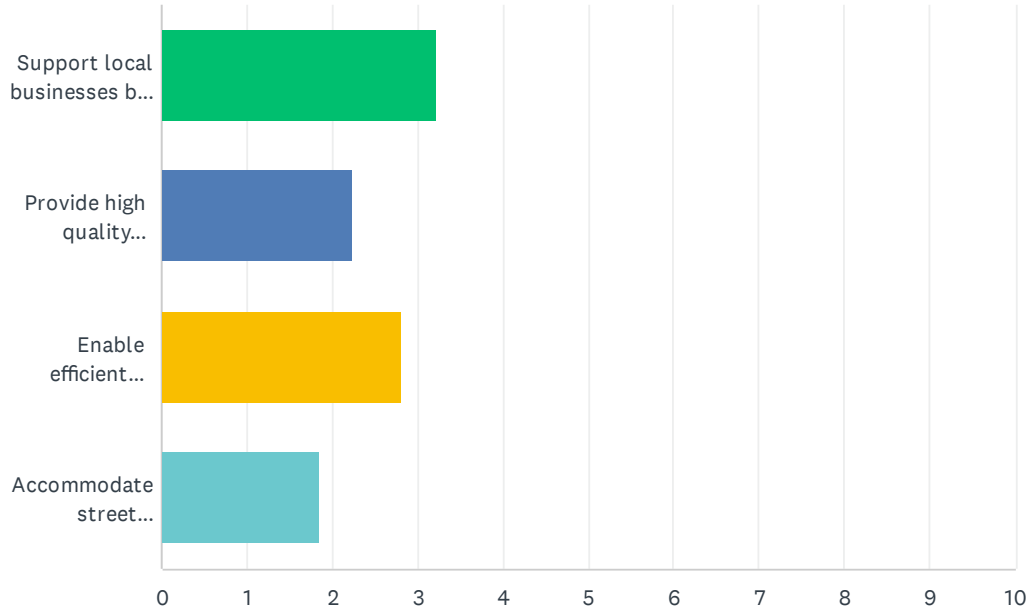
Answered: 942 Skipped: 142



| ANSWER CHOICES | RESPONSES | |
|--|-----------|-----|
| I am a business owner | 1.17% | 11 |
| I am a property owner | 9.45% | 89 |
| I work in the Rosemary District or Central/ Cocoanut | 5.94% | 56 |
| I am a Rosemary District or Central/ Cocoanut resident | 25.69% | 242 |
| I am a resident of the City of Sarasota | 51.38% | 484 |
| I am a resident of Sarasota County | 40.66% | 383 |
| Other | 9.77% | 92 |
| Total Respondents: 942 | | |

Q11 Rank in order of priority what function you think a 'complete street' like 10th Street should serve:

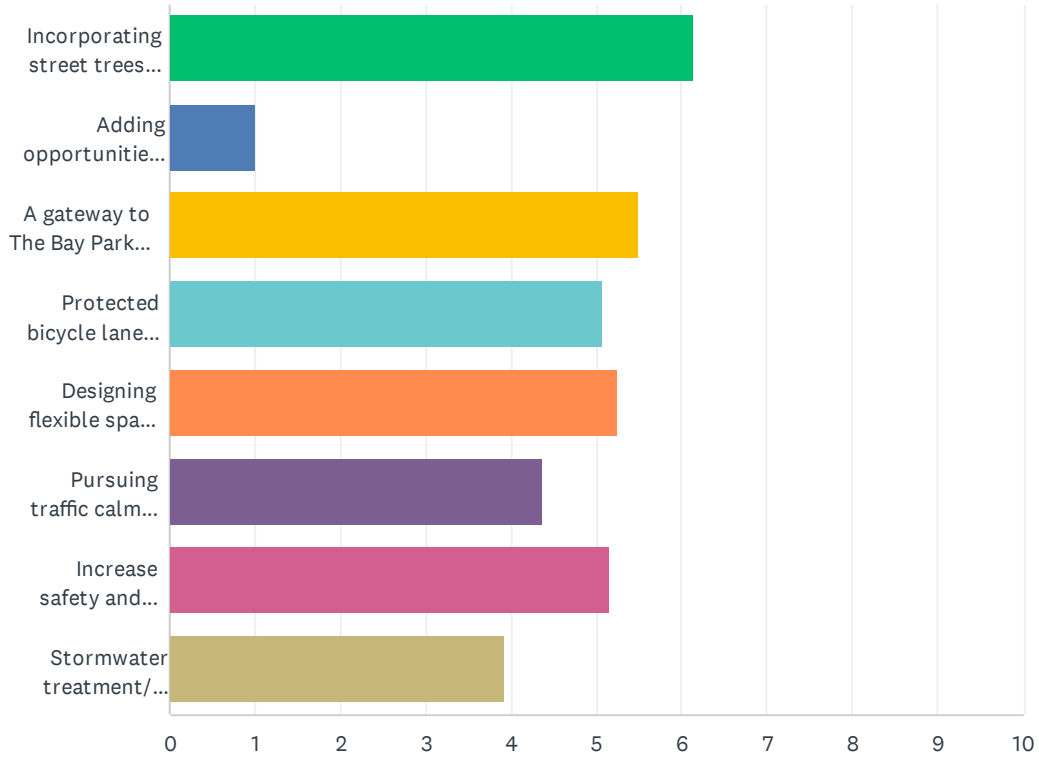
Answered: 903 Skipped: 181



| | 1 | 2 | 3 | 4 | TOTAL | SCORE |
|---|---------------|---------------|---------------|---------------|-------|-------|
| Support local businesses by enhancing the public realm (i.e. providing 'spill over' areas for sidewalk cafes) | 44.16% 378 | 38.43% 329 | 12.97% 111 | 4.44% 38 | 856 | 3.22 |
| Provide high quality placemaking opportunities | 10.24% 85 | 24.82% 206 | 42.29% 351 | 22.65% 188 | 830 | 2.23 |
| Enable efficient traffic flow to destinations | 42.00% 373 | 19.03% 169 | 16.78% 149 | 22.18% 197 | 888 | 2.81 |
| Accommodate street festivals and events | 7.24% 61 | 18.29% 154 | 26.84% 226 | 47.62% 401 | 842 | 1.85 |

Q12 What do you perceive to be the most important for consideration in redesigning 10th Street? Rank in order of priority.

Answered: 905 Skipped: 179

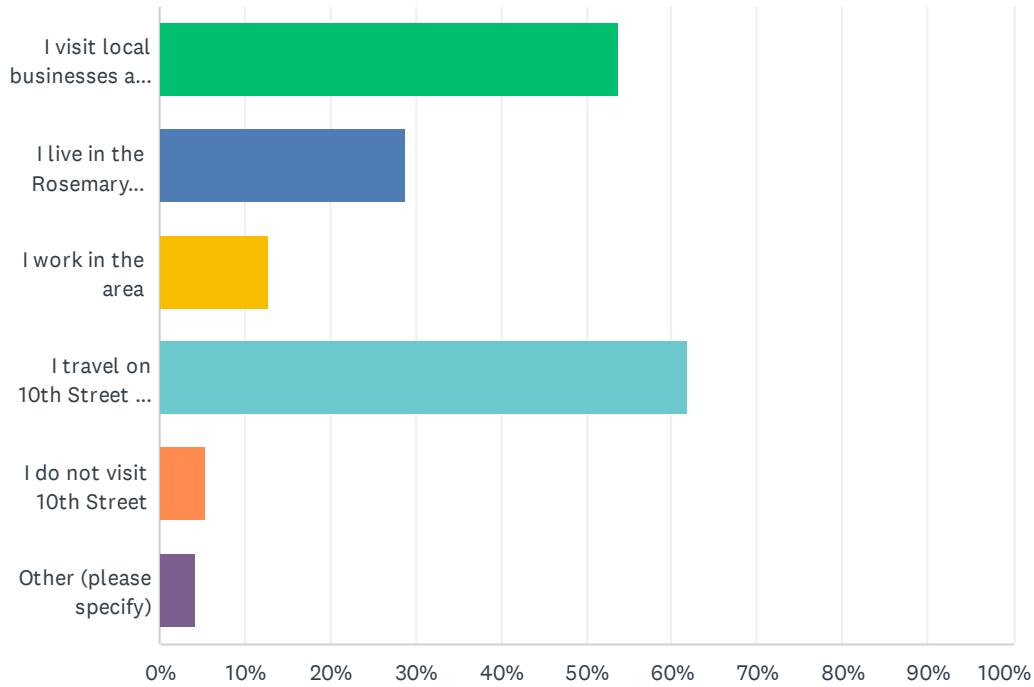


Boulevard of the Arts & 10th Street Complete Streets Survey

| | 1 | 2 | 3 | 4 | 5 | 6 | 7 | 8 | TOTAL | SCORE |
|---|---------------|---------------|---------------|---------------|---------------|---------------|---------------|--------------|-------|-------|
| Incorporating street trees, landscaping, and green space | 30.57% 265 | 18.45% 160 | 17.76% 154 | 14.19% 123 | 9.80% 85 | 5.88% 51 | 3.34% 29 | 0.00% 0 | 867 | 6.15 |
| Adding opportunities for gathering spaces | 0.00% 0 | 0.00% 0 | 0.00% 0 | 0.00% 0 | 0.00% 0 | 0.00% 0 | 0.00% 0 | 100.00% 2 | 2 | 1.00 |
| A gateway to The Bay Park and Van Wezel | 29.03% 252 | 16.01% 139 | 10.60% 92 | 8.87% 77 | 8.06% 70 | 12.10% 105 | 15.32% 133 | 0.00% 0 | 868 | 5.51 |
| Protected bicycle lanes/ infrastructure | 10.62% 89 | 14.92% 125 | 19.21% 161 | 15.51% 130 | 16.47% 138 | 13.72% 115 | 9.55% 80 | 0.00% 0 | 838 | 5.08 |
| Designing flexible spaces for pedestrian activity | 8.27% 71 | 20.37% 175 | 17.69% 152 | 21.19% 182 | 13.27% 114 | 12.81% 110 | 6.40% 55 | 0.00% 0 | 859 | 5.25 |
| Pursuing traffic calming methods (i.e. speed tables or raised intersections) | 5.80% 49 | 11.01% 93 | 11.24% 95 | 12.90% 109 | 22.13% 187 | 18.82% 159 | 18.11% 153 | 0.00% 0 | 845 | 4.37 |
| Increase safety and accessibility (Additional lighting, improved crossings, ADA compliance) | 14.63% 127 | 14.52% 126 | 15.90% 138 | 14.86% 129 | 15.55% 135 | 17.51% 152 | 7.03% 61 | 0.00% 0 | 868 | 5.17 |
| Stormwater treatment/ water quality enhancements | 5.87% 50 | 8.22% 70 | 10.21% 87 | 11.74% 100 | 12.44% 106 | 16.08% 137 | 35.45% 302 | 0.00% 0 | 852 | 3.93 |

Q13 Why do you visit 10th Street? Select all that apply.

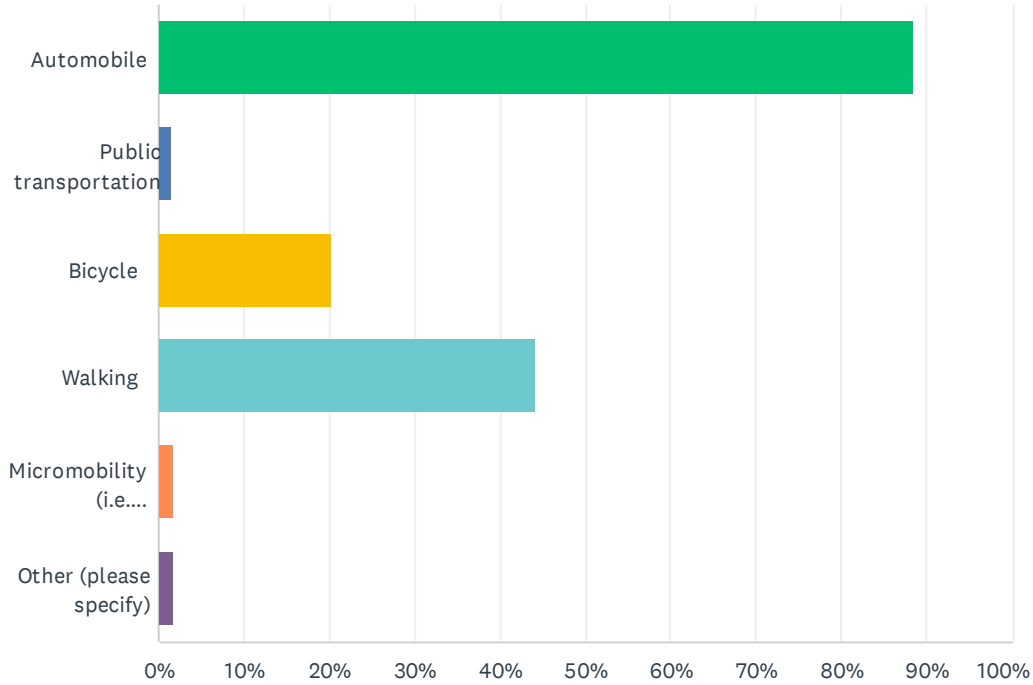
Answered: 918 Skipped: 166



| ANSWER CHOICES | RESPONSES | |
|---|-----------|-----|
| I visit local businesses and services | 53.81% | 494 |
| I live in the Rosemary District or Central/ Coconut | 28.87% | 265 |
| I work in the area | 12.75% | 117 |
| I travel on 10th Street to get somewhere else | 61.87% | 568 |
| I do not visit 10th Street | 5.34% | 49 |
| Other (please specify) | 4.36% | 40 |
| Total Respondents: 918 | | |

Q14 How do you access businesses or services on 10th Street? Select all that apply.

Answered: 905 Skipped: 179



| ANSWER CHOICES | RESPONSES | |
|--|-----------|-----|
| Automobile | 88.51% | 801 |
| Public transportation | 1.44% | 13 |
| Bicycle | 20.33% | 184 |
| Walking | 44.09% | 399 |
| Micromobility (i.e. Bikeshare, scooter, golf cart) | 1.77% | 16 |
| Other (please specify) | 1.66% | 15 |
| Total Respondents: 905 | | |

Q15 What would be appropriate amenities to support cycling and public transit access and travel on 10th Street?

Answered: 454 Skipped: 630

| # | RESPONSES | DATE |
|----|--|--------------------|
| 1 | Bringing back something like the gotcha ride to serve downtown Sarasota and local residents. | 2/28/2022 1:53 PM |
| 2 | Our existing traffic situation will only get worse if we try to attract more people and cars downtown. Fix the rotary problems first | 2/28/2022 1:47 PM |
| 3 | bike lanes.... there's lots of car traffic, and cyclists are afraid | 2/28/2022 12:18 PM |
| 4 | special bike and bus lanes would structure traffic flow better. | 2/28/2022 11:58 AM |
| 5 | Bike lanes would be wonderful, along with bike racks. | 2/27/2022 9:42 PM |
| 6 | pedestrian bridges | 2/27/2022 5:45 PM |
| 7 | pedestrian bridges | 2/26/2022 2:28 PM |
| 8 | landscaping | 2/26/2022 9:01 AM |
| 9 | Safe cycle lanes where cyclist are protected from vehicle traffic. | 2/26/2022 8:20 AM |
| 10 | Designated Lyft/Uber locations, pedestrian overpass bridge, wide sidewalks and appropriate cross lights for pedestrians | 2/26/2022 12:01 AM |
| 11 | Bike lanes | 2/25/2022 10:52 AM |
| 12 | Pedestrian Bridge | 2/25/2022 6:10 AM |
| 13 | can't think but including bike lanes and transit access will help as depicted in figure 12 (below) | 2/24/2022 8:15 PM |
| 14 | Safety | 2/24/2022 1:19 PM |
| 15 | bike lanes and racks, covered bus stops, recycling and trash recepticals | 2/24/2022 11:56 AM |
| 16 | Lanes | 2/24/2022 10:49 AM |
| 17 | C | 2/24/2022 10:04 AM |
| 18 | Clean up corner of Coconut and 10th. Add bicycle lanes. | 2/24/2022 9:02 AM |
| 19 | Pedestrian bridge across tamiami trail!!! | 2/24/2022 8:40 AM |
| 20 | Bicycle lanes | 2/24/2022 7:14 AM |
| 21 | Wider sidewalks | 2/24/2022 6:49 AM |
| 22 | Bicycle lanes. | 2/23/2022 10:08 PM |
| 23 | In general we need a pedestrian mall downtown. More green space. | 2/23/2022 7:34 PM |
| 24 | Lanes with lights | 2/23/2022 5:32 PM |
| 25 | Bicycles are one more hazard. | 2/23/2022 4:56 PM |
| 26 | Pedestrian bridge to cross US 41--safety issues. | 2/23/2022 4:47 PM |
| 27 | bike lane, bus stops area | 2/23/2022 4:38 PM |
| 28 | Bicycle lanes | 2/23/2022 4:00 PM |
| 29 | Bicycle paths | 2/23/2022 3:56 PM |
| 30 | Bike Lanes and pedestrian friendly. | 2/23/2022 3:39 PM |

Boulevard of the Arts & 10th Street Complete Streets Survey

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| 31 | Lanes | 2/23/2022 3:30 PM |
| 32 | pedestrian bridge to VanWezel and the Bay park. Parking facilities. | 2/23/2022 3:23 PM |
| 33 | A pedestrian and cycling overpass over Tamiami Trail to connect the district to The Bay and the new performing arts center. | 2/23/2022 3:13 PM |
| 34 | Eliminate right turn on red. It is too dangerous for pedestrians and bicyclists | 2/23/2022 3:06 PM |
| 35 | Bicycle lane, bike racks to park bikes, | 2/23/2022 2:45 PM |
| 36 | Not sure | 2/23/2022 2:41 PM |
| 37 | Pedestrian bridge & safe walkways from parking deck | 2/23/2022 2:14 PM |
| 38 | bike lanes and lighted bus shelters | 2/23/2022 2:10 PM |
| 39 | Get rid of the roundabout. I avoid 10th St. altogether because of it. | 2/23/2022 1:50 PM |
| 40 | Bus shelters and bike lanes | 2/23/2022 1:40 PM |
| 41 | A pedestrian walk way above street level | 2/23/2022 1:39 PM |
| 42 | Build pedestrian bridges | 2/23/2022 1:30 PM |
| 43 | Bike lanes, bike racks | 2/23/2022 1:12 PM |
| 44 | Pedestrian bridge | 2/23/2022 1:03 PM |
| 45 | bicycle lanes, bike racks, speed bumps, bus shelters | 2/23/2022 1:01 PM |
| 46 | Bicycle lanes | 2/23/2022 12:45 PM |
| 47 | Pedestrian walkways. | 2/23/2022 12:40 PM |
| 48 | Covered by stabs and bicycle racks. | 2/23/2022 12:29 PM |
| 49 | Bike lane Bus shelter Bike stands | 2/23/2022 12:20 PM |
| 50 | Better traffic flow. | 2/23/2022 12:16 PM |
| 51 | No thoughts on this | 2/23/2022 11:58 AM |
| 52 | bicycle lanes and bus shelters | 2/23/2022 11:54 AM |
| 53 | Pedestrian Bridge over 41 is the most important thing to access Bayfront Park | 2/23/2022 11:50 AM |
| 54 | Pedestrian overpasses | 2/23/2022 11:49 AM |
| 55 | Wide bicycle lanes so they don't impede automobile traffic. | 2/23/2022 11:48 AM |
| 56 | Walkover for us 41 | 2/23/2022 11:45 AM |
| 57 | Bike lanes | 2/23/2022 11:40 AM |
| 58 | Pedestrian bridges over Tamiami Trail to access performing arts venues. | 2/23/2022 11:35 AM |
| 59 | Designated bike lane with a physical separation between bikes and cars. | 2/23/2022 11:34 AM |
| 60 | pedestrian bridges | 2/23/2022 11:31 AM |
| 61 | Separate bike lanes, elevated pedestrian walkways to cross Tamiami Trail | 2/23/2022 11:23 AM |
| 62 | Pedestrian/cycling access from 10th St. to Bay Park | 2/23/2022 11:22 AM |
| 63 | Bicycle lanes | 2/23/2022 11:14 AM |
| 64 | Have adequate parking and construct a walking bridge across 41 to access Bay Park and new performing arts center. | 2/23/2022 11:10 AM |
| 65 | don't care | 2/23/2022 11:09 AM |
| 66 | Marked lanes | 2/23/2022 11:02 AM |
| 67 | Wide sidewalks, better crossing lanes, protected bike lanes | 2/23/2022 10:59 AM |

Boulevard of the Arts & 10th Street Complete Streets Survey

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| 68 | Bike lanes | 2/23/2022 10:56 AM |
| 69 | safer bike lanes | 2/23/2022 10:55 AM |
| 70 | obviously bike lanes and bus shelters, crosswalks that alert drivers / protect pedestrians, pedestrian bridges | 2/23/2022 10:51 AM |
| 71 | Bike lanes, bus shelters, | 2/23/2022 10:49 AM |
| 72 | ??? | 2/23/2022 10:43 AM |
| 73 | Protected bike lanes | 2/23/2022 10:41 AM |
| 74 | Shelters | 2/23/2022 10:37 AM |
| 75 | Bike lane and bus shelters | 2/23/2022 10:33 AM |
| 76 | Bike lanes and bus shelters | 2/23/2022 10:33 AM |
| 77 | No comment | 2/23/2022 10:32 AM |
| 78 | Pedestrian iverpass | 2/23/2022 10:29 AM |
| 79 | Bicycle lane | 2/23/2022 10:29 AM |
| 80 | Separate land for bikes/ buses | 2/23/2022 10:28 AM |
| 81 | Safety | 2/23/2022 10:26 AM |
| 82 | there needs to be a safe way to cross over Tamiami- a pedestrian walkway above the street would be ideal. | 2/23/2022 10:26 AM |
| 83 | How frequently and how many busses? | 2/23/2022 10:26 AM |
| 84 | Dedicated Bike lanes | 2/23/2022 10:21 AM |
| 85 | I don't know what options there are other than bike lanes and bus shelters. I guess I'm not very creative or knowledgeable. | 2/23/2022 10:20 AM |
| 86 | Pedestrian bridge and bike bridge | 2/23/2022 10:17 AM |
| 87 | Pedestrian and bicycle bridges over busy streets | 2/23/2022 10:15 AM |
| 88 | Good traffic control, good signage; tourist are unaware of our community and don't always pay attention | 2/23/2022 10:11 AM |
| 89 | Bike lanes, bike parking, bus or trolley stop | 2/23/2022 10:10 AM |
| 90 | Wider bike lanes | 2/23/2022 10:09 AM |
| 91 | Safety and accessibility. | 2/23/2022 10:08 AM |
| 92 | Bicycle lane, sidewalks | 2/23/2022 10:06 AM |
| 93 | pedestrian bridge | 2/23/2022 10:03 AM |
| 94 | Pedestrian bridges, bike lanes | 2/23/2022 9:57 AM |
| 95 | pedestrian bridges that cross highway 41 | 2/23/2022 9:57 AM |
| 96 | Pedestrian bridge | 2/23/2022 9:54 AM |
| 97 | Bike lanes | 2/23/2022 9:53 AM |
| 98 | No bike lanes. Very dangerous here. | 2/23/2022 9:49 AM |
| 99 | bicycle lanes | 2/23/2022 9:48 AM |
| 100 | Access for bike lanes restricted as shown in the picture. | 2/23/2022 9:48 AM |
| 101 | pedestrian bridge over Tamiami trail. | 2/23/2022 9:47 AM |
| 102 | Protected bike lane. Ample shade on sidewalks -- right now it's a brutally hot, uncomfortable walk. Traffic calming to improve safety for those outside vehicles. If within the scope of the | 2/23/2022 9:46 AM |

Boulevard of the Arts & 10th Street Complete Streets Survey

project, fix the intersection at Orange: remove slip lane (a safety hazard) and straighten alignment. A roundabout would be ideal there.

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| 103 | Protected bike lane | 2/22/2022 11:56 PM |
| 104 | Protected bicycle lanes or green-painted bicycle lanes. Bicycle racks. Appropriate street lighting. Low vehicular speeds. Bus shelters with appropriate street lighting, public art, landscaping/trees. | 2/22/2022 9:14 AM |
| 105 | pedestrian bridge over 41 | 2/21/2022 6:04 PM |
| 106 | wide sidewalks for bicycles | 2/20/2022 7:43 PM |
| 107 | Sun shelters and bike lanes | 2/20/2022 5:23 AM |
| 108 | Bike lane, covered bench at bus stop(s) | 2/17/2022 1:43 PM |
| 109 | Benches wide clean sidewalks bicycle Lanes places to drink water beautiful trees flower plants flower boxes | 2/17/2022 8:30 AM |
| 110 | protected bicycle lanes, | 2/16/2022 10:49 AM |
| 111 | A safe way for pedestrians and bikes to cross over the top of Tamiami. There are often some (not all) people in the 10th St. area that make me feel unsafe even during the day. If there is a way to cross over the top of Tamiami that is enclosed, I would not want to feel closed in or have to squeeze by people hanging out "making deals".. Omg.. I sound like an old lady! | 2/16/2022 6:59 AM |
| 112 | Elevated walkway over Tamiami Trail. | 2/15/2022 11:01 PM |
| 113 | Pedestrian Bridge | 2/15/2022 6:29 PM |
| 114 | slow down traffic | 2/15/2022 3:20 PM |
| 115 | Dedicated lanes for cycling | 2/15/2022 1:21 PM |
| 116 | Well marked and protected bike lanes. | 2/15/2022 1:07 PM |
| 117 | Path for bikes and frequent rest area (benches) for pedestrians | 2/15/2022 9:29 AM |
| 118 | bike lanes | 2/15/2022 9:23 AM |
| 119 | Protected bike lanes | 2/15/2022 8:40 AM |
| 120 | There is no public transit service on 10th St or Blvd of the Arts but the service on Coconut and Orange is frequent and deserves nicer shelters. If traffic is slowed using back in angle parking--bike lanes are not really necessary. The space should be allocated to local businesses to create vibrant public spaces like sidewalk cafes and public art. | 2/15/2022 8:21 AM |
| 121 | safety, lighting, and accessibility infrastructure | 2/14/2022 9:58 PM |
| 122 | Protected cycling lanes | 2/14/2022 4:30 PM |
| 123 | Do not encourage bicycle or pedestrian traffic. We need more open automobile routes. | 2/14/2022 3:10 PM |
| 124 | Divided bike lane | 2/14/2022 2:13 PM |
| 125 | separated/protected bike lanes, narrower lanes to slow down traffic, bus shelter, more lighting for pedestrian access at the Coconut Ave. intersection, landscaping to hide the concrete plant at central ave., benches and bike racks near the shelter at central ave. | 2/14/2022 2:10 PM |
| 126 | Bicycle accommodations, benches, attractive pedestrian crosswalk instructions and lights. | 2/14/2022 11:58 AM |
| 127 | Bike lanes , bus loading islands | 2/14/2022 10:02 AM |
| 128 | whatever gets them out of the way of cars | 2/14/2022 9:50 AM |
| 129 | safe and separate bike lane like the Netherlands | 2/14/2022 9:40 AM |
| 130 | connection to the Legacy Trail | 2/14/2022 9:28 AM |
| 131 | large sidewalks and walkway to Bay Park | 2/14/2022 8:52 AM |
| 132 | Bike lanes, bike racks, bus shelters, signal pre-motion for buses (needed system wide!) | 2/14/2022 8:26 AM |

Boulevard of the Arts & 10th Street Complete Streets Survey

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| 133 | Bike lanes | 2/14/2022 8:19 AM |
| 134 | Bike sharing, dedicated bike paths, | 2/13/2022 8:02 PM |
| 135 | Pedestrian focused lighting | 2/13/2022 7:07 PM |
| 136 | Bike lanes | 2/13/2022 6:50 PM |
| 137 | Safe bike lanes | 2/13/2022 3:21 PM |
| 138 | bigger bike lanes. Make 10th street safer in terms of crime and homeless activity | 2/13/2022 1:03 PM |
| 139 | pedestrian bridge | 2/13/2022 10:41 AM |
| 140 | traffic calming devices, ecology concerns (including stormwater drainage), clear delineation of bicycle and pedestrian "lanes." | 2/13/2022 12:36 AM |
| 141 | I wish you could use a raised walkway as it is frightening to cross Tamiami | 2/12/2022 5:53 PM |
| 142 | Monitoring the homeless from sitting on 10th and Cocanut. Making people safe to walk in neighborhood More police presence. | 2/12/2022 2:34 PM |
| 143 | The picture in Figure 12 | 2/12/2022 1:52 PM |
| 144 | Pedestrian overpass, bike lanes | 2/12/2022 1:46 PM |
| 145 | Bus and cycle lanes. Large vehicle and other traffic restrictions. | 2/12/2022 1:20 PM |
| 146 | bike paths | 2/12/2022 12:45 PM |
| 147 | Bicycle Lanes | 2/12/2022 11:21 AM |
| 148 | see answer for Blvd of Arts | 2/12/2022 11:07 AM |
| 149 | I think 10th street is and should remain the major artery to The Bay, Van Wenzel and all the other offerings in the area. I think bicycles should be routed other ways. Bus stops should be cut out so as not to impede traffic flow and be kept to a minimum if necessary at all. | 2/12/2022 10:36 AM |
| 150 | Bike lanes Bike racks Bike rentals | 2/12/2022 10:34 AM |
| 151 | Need a pedestrian bridge across 41 | 2/12/2022 9:39 AM |
| 152 | Bike lanes,racks | 2/12/2022 8:50 AM |
| 153 | Bicycle lanes, bus shelters, pedestrian friendly space | 2/12/2022 8:20 AM |
| 154 | protected bike lanes, street trees, reduced lane width | 2/11/2022 5:21 PM |
| 155 | Isolated bicycle/pedestrian areas. | 2/11/2022 11:44 AM |
| 156 | MOBILITY HUBS, BIKE LANES | 2/11/2022 11:36 AM |
| 157 | bike lanes | 2/11/2022 11:32 AM |
| 158 | lighting for safety. also benches and bike racks | 2/11/2022 11:31 AM |
| 159 | I would like to see something like Figure 12. | 2/11/2022 7:46 AM |
| 160 | bicycle lanes and street lights. | 2/10/2022 11:20 AM |
| 161 | bike lanes, covered bus stops. Ample space away from cars. | 2/10/2022 8:32 AM |
| 162 | bike rack rest stops with bathrooms water fountains | 2/9/2022 12:22 PM |
| 163 | Pedestrian bridge over Tamiami Trail | 2/9/2022 12:17 PM |
| 164 | Feeling safe walking or biking. I would not walk alone in day or at night at all. | 2/8/2022 6:16 PM |
| 165 | Bike lanes | 2/8/2022 2:03 PM |
| 166 | Covered Bus stops, Protected bicycle lanes | 2/8/2022 1:56 PM |
| 167 | Bike Lane | 2/8/2022 9:55 AM |
| 168 | Protected bike lanes | 2/8/2022 9:43 AM |

Boulevard of the Arts & 10th Street Complete Streets Survey

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| 169 | Wide bike lanes, preferably separated from the traffic lanes | 2/8/2022 8:49 AM |
| 170 | Protected bike lanes and bike racks to provide an alternative to vehicle travel | 2/8/2022 8:38 AM |
| 171 | Exactly what's in the picture below. Do that! | 2/8/2022 8:05 AM |
| 172 | move the salvation army out east or north, out of that area, its made homeless everywhere lemon is gross i mean gross | 2/8/2022 6:24 AM |
| 173 | Bike lanes | 2/7/2022 11:11 PM |
| 174 | Bike lane | 2/7/2022 10:04 PM |
| 175 | Bike lanes | 2/7/2022 9:16 PM |
| 176 | I would like to see a bridge over Tamiami Trail to the park and Van Wezel. | 2/7/2022 8:37 PM |
| 177 | protected bike lanes. | 2/7/2022 8:09 PM |
| 178 | Created walk ways, and cycling paths. | 2/7/2022 7:04 PM |
| 179 | Bike lane | 2/7/2022 4:25 PM |
| 180 | Bike lanes and racks | 2/7/2022 2:01 PM |
| 181 | bicycle lanes, street lights | 2/7/2022 1:48 PM |
| 182 | pedestrian bridges to tie waterfront perimeter to city shops and amenities | 2/7/2022 1:47 PM |
| 183 | Fix and upgrade sidewalks, add greenery/water management | 2/7/2022 1:14 PM |
| 184 | Streetcars, separate lanes for walking, cycling, sitting. | 2/7/2022 1:05 PM |
| 185 | Safe space | 2/7/2022 1:05 PM |
| 186 | Pedestrian Bridge | 2/7/2022 12:22 PM |
| 187 | Don't want more people | 2/7/2022 12:14 PM |
| 188 | A PEDX BRIDGE OVER 41 EITHER HERE OR BOTA | 2/7/2022 11:46 AM |
| 189 | bike lanes | 2/7/2022 11:32 AM |
| 190 | lanes and safe spaces to travel about | 2/7/2022 11:27 AM |
| 191 | Overhead bridge over 41 for pedestrians. | 2/7/2022 10:35 AM |
| 192 | Protected bike lanes | 2/7/2022 10:16 AM |
| 193 | BIKE LANES | 2/7/2022 9:52 AM |
| 194 | Painted bike lanes, bollards, other ways to separate traffic from bicycle lane | 2/7/2022 9:29 AM |
| 195 | sidewalks and lighting | 2/7/2022 8:47 AM |
| 196 | Lower speed limit | 2/7/2022 7:37 AM |
| 197 | Protected bike lanes, bike tool kiosks, bike racks, shade producing trees, and covered bus stop. | 2/7/2022 7:36 AM |
| 198 | Safe passageways for pedestrians and cyclists. | 2/7/2022 7:15 AM |
| 199 | Dedicated bike lanes | 2/7/2022 12:09 AM |
| 200 | Pedestrian bridge | 2/6/2022 8:58 PM |
| 201 | Painted bike lanes, NOT separated bicycle lanes | 2/6/2022 8:52 PM |
| 202 | biggest challenge is the Salvation Army clients - how to create a safe, desirable area with that challenge | 2/6/2022 7:56 PM |
| 203 | PROTECTED bike lanes, traffic calming, bus shelters | 2/6/2022 5:26 PM |
| 204 | Bike lanes | 2/6/2022 3:03 PM |
| 205 | Definitely need a street diet; and reallocate the space into better pedestrian areas, and a | 2/6/2022 1:00 PM |

Boulevard of the Arts & 10th Street Complete Streets Survey

dedicated safe bike/micromobility lane if room. Needs to reallocate the space so cars, pedestrians, and bikes/micromobility can safely and EQUALLY use the corridor, as the primary access way to the Bay Park and new PAC.

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| 206 | Protected bike lanes | 2/6/2022 12:54 PM |
| 207 | Bus-waiting booths and bicycle lanes | 2/6/2022 11:14 AM |
| 208 | safety in bike lane | 2/6/2022 10:40 AM |
| 209 | Figure 12 is a great example, except I believe that traditional bus service has no future in Sarasota. It's too cumbersome too costly and too inefficient. No one would ride a bus here unless it's their last resort of getting anywhere and the distances required for bus coverage are too great abs too numerous to make the service practical. | 2/6/2022 10:16 AM |
| 210 | Dedicated bicycle lanes, Shelter kiosks at Transit stops | 2/6/2022 10:14 AM |
| 211 | cycling in Sarasota seems dangerous | 2/6/2022 10:13 AM |
| 212 | bus shelters | 2/6/2022 9:33 AM |
| 213 | Bike lanes | 2/6/2022 9:27 AM |
| 214 | bicycle lanes, bus pullover space | 2/6/2022 8:43 AM |
| 215 | You have to deal with homeless issue | 2/6/2022 8:22 AM |
| 216 | Protected bike lane | 2/6/2022 7:58 AM |
| 217 | Bike lanes | 2/6/2022 7:26 AM |
| 218 | Protected bike lane | 2/6/2022 6:50 AM |
| 219 | Pedestrian overpass | 2/6/2022 4:20 AM |
| 220 | Safety measures: Signs and Lights. Safety First Always. | 2/6/2022 12:18 AM |
| 221 | Protected bike lanes | 2/5/2022 9:38 PM |
| 222 | Areas for families to gather & picnic Bus stops w/benches & covers Beautiful flowers and bushes Cycle pathways not with pedestrians | 2/5/2022 9:36 PM |
| 223 | Protected Bicycle Lanes | 2/5/2022 8:53 PM |
| 224 | enhance the parks already there and get the water department to clean up their mess too | 2/5/2022 8:52 PM |
| 225 | restaurants, shops | 2/5/2022 8:48 PM |
| 226 | Make it friendly walkable. Now it is just a barren looking and feeling street. I use it all the time to go to Publix. It is not pretty. Just functioning street. Golf carts in the Rosemary district would be terrific. The on loan or free carts like in the Villages. | 2/5/2022 8:20 PM |
| 227 | Bus shelters, shade trees | 2/5/2022 8:06 PM |
| 228 | bicycle lane, trees | 2/5/2022 5:34 PM |
| 229 | Make sure the traffic rotary works for traffic to Van Wezel | 2/5/2022 4:19 PM |
| 230 | bike lanes | 2/5/2022 4:07 PM |
| 231 | Cycle paths bus shelter pedestrian bridge | 2/5/2022 4:03 PM |
| 232 | bike lanes, wider sidewalks | 2/5/2022 3:56 PM |
| 233 | see figure 12 | 2/5/2022 3:48 PM |
| 234 | bike lane | 2/5/2022 3:41 PM |
| 235 | Bike lane | 2/5/2022 3:35 PM |
| 236 | Dedicated bike lanes, shaded bus stops with benches | 2/5/2022 3:09 PM |
| 237 | More public parking. | 2/5/2022 2:36 PM |
| 238 | Bike lanes | 2/5/2022 2:31 PM |

Boulevard of the Arts & 10th Street Complete Streets Survey

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| 239 | Pedestrian Bridge across 41 | 2/5/2022 2:18 PM |
| 240 | wide bicycle lanes | 2/5/2022 2:17 PM |
| 241 | Getting rid of the Salvation Army | 2/5/2022 2:17 PM |
| 242 | but I would love to visit on bicycles if there were protected bike lanes. | 2/5/2022 2:17 PM |
| 243 | Bicycle lane | 2/5/2022 2:15 PM |
| 244 | Same as for blvd of the arts, but more emphasis on protected bike lanes...10th is more of a transit corridor, where I see blvd of the arts as more of a shopping/eating/pedestrian street. | 2/5/2022 2:14 PM |
| 245 | The homeless population needs to be off the street. | 2/5/2022 1:58 PM |
| 246 | Bike rack safe lanes for bikes | 2/5/2022 1:38 PM |
| 247 | Bicycle lanes and frequent benches | 2/5/2022 1:14 PM |
| 248 | Parking and safety | 2/5/2022 1:04 PM |
| 249 | bicycle lanes, bus shelters | 2/5/2022 12:48 PM |
| 250 | Safe Cycling paths with locking bike stands at key locations. Pull-off or bus waiting for areas that are separated from vehicle traffic | 2/5/2022 12:35 PM |
| 251 | overhead pedestrian crossings | 2/5/2022 12:26 PM |
| 252 | Bike lanes, walking lanes , NO public transit | 2/5/2022 12:22 PM |
| 253 | Designated ride share locations, safe pedestrian crossings, overpass on 10th street to access the Bay Park and future Performing Arts center | 2/5/2022 12:14 PM |
| 254 | Bike lanes | 2/5/2022 12:06 PM |
| 255 | Safety, bike lanes | 2/5/2022 11:56 AM |
| 256 | Bus shelters & bicycle lanes | 2/5/2022 11:55 AM |
| 257 | a separated bike lane. It is a very busy street and biking seems treacherous to me--I do not do it. | 2/5/2022 11:37 AM |
| 258 | Bike lanes with barriers, Skyway walkway to The Bay Park and Municipal Auditorium | 2/5/2022 11:36 AM |
| 259 | Dumb question. The answer to this is well established through good urban planning principles. | 2/5/2022 11:30 AM |
| 260 | NO motorized vehicles except public transit. | 2/5/2022 11:27 AM |
| 261 | bicycle lanes, sufficient lighting, roundabouts | 2/5/2022 11:20 AM |
| 262 | Bike lanes | 2/5/2022 11:01 AM |
| 263 | Move Rt 41 east away from the marina and Bay development park area. | 2/5/2022 10:59 AM |
| 264 | Proper bike lanes. | 2/5/2022 10:56 AM |
| 265 | Once again, I'd like to see a safer way for pedestrians to get from 10th to the bay. I'm a big fan of traffic circles, but it's not working for pedestrians to easily access the bay from 10th. | 2/5/2022 10:50 AM |
| 266 | see above, public transit access incl. shelters, separate bike lanes | 2/5/2022 10:45 AM |
| 267 | Protected bike lane (not just the traditional painted white stripe) | 2/5/2022 10:40 AM |
| 268 | Protected bike lane | 2/5/2022 10:32 AM |
| 269 | Protected bike lanes. | 2/5/2022 10:17 AM |
| 270 | Improve and enhance current structure. | 2/5/2022 10:13 AM |
| 271 | Bike lanes need to be protected and then people might actually use them. A lot of people who bike here only bike here on vacation so they are not very good at it. But since Florida has so stupidly put trees in the middle of streets there is little space left for protected bike lanes. Florida needs to quit building for the 1960's | 2/5/2022 10:12 AM |

Boulevard of the Arts & 10th Street Complete Streets Survey

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| 272 | Lanes | 2/5/2022 10:08 AM |
| 273 | Bridge over 41. | 2/5/2022 10:05 AM |
| 274 | protected bike lanes, pedestrian bridges | 2/5/2022 10:03 AM |
| 275 | protected bike lane, secure bicycle parking, benches/shelters | 2/5/2022 10:03 AM |
| 276 | Bicycle lanes, sharrows | 2/5/2022 10:01 AM |
| 277 | Safety for pedestrians and bicyclists | 2/5/2022 9:51 AM |
| 278 | Separated bike lane | 2/5/2022 9:49 AM |
| 279 | Keeping bikes and people away from all the speeding cars. | 2/5/2022 9:43 AM |
| 280 | None | 2/5/2022 9:42 AM |
| 281 | Limit automobile access. Create more greenspace. | 2/5/2022 9:42 AM |
| 282 | Safe crossing and walking ability. | 2/5/2022 9:42 AM |
| 283 | Security | 2/5/2022 9:38 AM |
| 284 | Separate lanes for bicycles and pedestrians | 2/5/2022 9:34 AM |
| 285 | Pedestrian bridge across 41 | 2/5/2022 9:33 AM |
| 286 | Bike lanes | 2/5/2022 9:29 AM |
| 287 | pedestrian bridge over Tamiami Trail to The Bay | 2/5/2022 9:28 AM |
| 288 | Bike lanes, bus shelters | 2/5/2022 9:24 AM |
| 289 | Pedestrian bridge over Tamiami Trail | 2/5/2022 9:19 AM |
| 290 | Bicycles do not belong on the roadways, they belong on the sidewalk. Public transit is fine the way it is | 2/5/2022 8:57 AM |
| 291 | Designated bike lanes please! | 2/5/2022 8:10 AM |
| 292 | Protected bike lane (barrier between cars) | 2/5/2022 7:59 AM |
| 293 | Bike lanes | 2/5/2022 7:39 AM |
| 294 | Same as previous section | 2/5/2022 7:29 AM |
| 295 | Bike lanes, slower traffic | 2/5/2022 7:28 AM |
| 296 | Bike lanes | 2/5/2022 6:29 AM |
| 297 | Protected lanes separated from traffic by green strip | 2/5/2022 6:18 AM |
| 298 | protected bicycle lanes. | 2/5/2022 1:53 AM |
| 299 | Bike locking stations. Shielding from rain and summer heat, such as canopies, or trees. | 2/5/2022 12:49 AM |
| 300 | Protected bike lane, pedestrian areas, festival and sidewalk cafes | 2/4/2022 9:45 PM |
| 301 | Add Simple Software For Timing traffic lights for automobile traffic flow. | 2/4/2022 4:40 PM |
| 302 | See earlier comments about cyclists and their disdain for traffic rules and pedestrians | 2/4/2022 4:31 PM |
| 303 | Sheltered bike lanes | 2/4/2022 4:07 PM |
| 304 | Bike lanes, bus shelters | 2/4/2022 3:44 PM |
| 305 | Buffered/Protected Bike lanes | 2/4/2022 3:24 PM |
| 306 | No Bus Stops. Bus tear up the roads, bog down traffic flow and make things too industrial when you are looking for a relaxing experience. What would be good is have the bus stops at the cross roads and the available to rent a motorized bike for accessing 10th Street and Blvd of arts. | 2/4/2022 9:00 AM |
| 307 | Bike lanes | 2/4/2022 8:38 AM |

Boulevard of the Arts & 10th Street Complete Streets Survey

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|-----|--|-------------------|
| 308 | Bus pockets off main road to not impede traffic. | 2/4/2022 8:31 AM |
| 309 | A designated are for bicyclists and pedestrians and a small median separating traffic from them in addition to the traffic median. | 2/3/2022 11:12 PM |
| 310 | Safe bike lanes or wider sidewalk | 2/3/2022 10:28 PM |
| 311 | safety | 2/3/2022 6:06 PM |
| 312 | sheltered bus stops, better bus service | 2/3/2022 4:59 PM |
| 313 | Bus shelters. Protected Bike Lanes. | 2/3/2022 2:06 PM |
| 314 | Safe bikel anes and sidewalks - creating connectivity between neighborhoods on North and South sides of 10th St | 2/3/2022 2:00 PM |
| 315 | Close the salvation army location and get rid of the homeless drug addicts | 2/3/2022 11:21 AM |
| 316 | good bike lanes | 2/3/2022 11:17 AM |
| 317 | 10th Street is not very appealing or welcoming now due to the "hangout" near the Salvation Army facilities. People lying everywhere with their belongings spread out all around is unsightly. The pretty Rosemary Cemetery nearby is too scary to walk near. | 2/3/2022 9:52 AM |
| 318 | protected bike lanes | 2/3/2022 8:53 AM |
| 319 | fix the current road, mark the lanes for 10th St, and Orange Ave, better sidewalks, and bike lanes | 2/3/2022 8:15 AM |
| 320 | Bus shelters, green bike lanes, appropriate traffic calming technology and landscaping | 2/3/2022 7:23 AM |
| 321 | More consistent bus services. | 2/3/2022 6:31 AM |
| 322 | safer bike lanes | 2/3/2022 3:52 AM |
| 323 | Bike lanes, racks, mobile phone charging station. | 2/2/2022 11:39 PM |
| 324 | cross walks, lighting, signage | 2/2/2022 11:31 PM |
| 325 | Bike service stations. Bus shelters. Bike lanes. | 2/2/2022 8:46 PM |
| 326 | Bike lanes and lighted crosswalks | 2/2/2022 8:25 PM |
| 327 | safer sidewalks | 2/2/2022 5:05 PM |
| 328 | protected bicycle lanes | 2/2/2022 4:31 PM |
| 329 | Covered Bike racks. | 2/2/2022 4:21 PM |
| 330 | Bicycle lanes, racks | 2/2/2022 4:09 PM |
| 331 | Clear lines for bike lanes. They're all fading in the area. More safety measures from the onslaught of homeless people. There's no feeling of safety. They're unpredictable. | 2/2/2022 3:07 PM |
| 332 | High visibility bike lanes and protective barriers. | 2/2/2022 1:52 PM |
| 333 | Bike, pedestrian, scooter, lanes should be physically separated from vehicle lanes. This includes crossings. Think of using overpasses. Remove the pedestrian crossings on circles...when a driver is looking left to enter a circle they simply cannot be expected to also be looking right to see if a pedestrian has chosen to cross. | 2/2/2022 1:45 PM |
| 334 | unknown | 2/2/2022 1:06 PM |
| 335 | Wide sidewalks, bicycle lanes, garbage cans, good lighting | 2/2/2022 12:27 PM |
| 336 | lighting wide bicycle lanes | 2/2/2022 11:49 AM |
| 337 | Better/safer bike lanes. More trees for shade. | 2/2/2022 11:05 AM |
| 338 | Protected bicycle lanes. | 2/2/2022 10:56 AM |
| 339 | Make 10th street for vehicles and blvd strictly pedestrian | 2/2/2022 10:49 AM |
| 340 | bike lane | 2/2/2022 10:46 AM |

Boulevard of the Arts & 10th Street Complete Streets Survey

| | | |
|-----|--|-------------------|
| 341 | Think about small electric vehicles rather than City buses. City buses should only be on outer ring of district with electric vehicles running through and to connecting areas. | 2/2/2022 10:41 AM |
| 342 | Shelter for bus passengers. | 2/2/2022 10:41 AM |
| 343 | Again, please consider closing one of these roads and making it pedestrian marketplace. | 2/2/2022 10:40 AM |
| 344 | Bike lane, racks | 2/2/2022 10:35 AM |
| 345 | Racks of sharable bikes and scooters | 2/2/2022 9:41 AM |
| 346 | Bike/scooter lanes/racks, bus routes and shelters | 2/2/2022 8:56 AM |
| 347 | frequent, accessible public transit; protected bike lanes | 2/2/2022 8:12 AM |
| 348 | I don't feel safe biking on 10th street currently, too much traffic and narrow bike lanes. Right now I would pick Blvd of the Arts to bike on because there are businesses there I visit. There need to be more trees and greenery on 10th also. | 2/2/2022 7:40 AM |
| 349 | Good bike lanes, bike parking, broader public transport system | 2/2/2022 7:39 AM |
| 350 | Bike stations , safer traffic patterns and decent separation between cars, bikes, and pedestrians | 2/2/2022 7:34 AM |
| 351 | Bicycle lanes, landscaping, benches | 2/2/2022 7:13 AM |
| 352 | Travel lanes | 2/2/2022 6:58 AM |
| 353 | Well defined and separate bicycle lane | 2/2/2022 5:40 AM |
| 354 | Shade | 2/2/2022 12:55 AM |
| 355 | Actual bike paths out of traffic abs green space | 2/2/2022 12:50 AM |
| 356 | Parking Garage | 2/2/2022 12:24 AM |
| 357 | Dedicated bike lanes like figure 12. | 2/1/2022 11:27 PM |
| 358 | Better police presence | 2/1/2022 11:11 PM |
| 359 | bike lanes and sidewalks | 2/1/2022 10:31 PM |
| 360 | Bicycle lanes | 2/1/2022 9:53 PM |
| 361 | No | 2/1/2022 9:48 PM |
| 362 | Bike lanes, benches, | 2/1/2022 9:33 PM |
| 363 | Yes but safely | 2/1/2022 9:23 PM |
| 364 | Fig 12 | 2/1/2022 8:56 PM |
| 365 | Safe and comfortable bus stops. Protected areas for pedestrians, good lighting. | 2/1/2022 8:55 PM |
| 366 | Pedestrian overpass for safe access across 41. Improve public transit in the area. | 2/1/2022 8:35 PM |
| 367 | Bike lanes | 2/1/2022 8:22 PM |
| 368 | Bike racks, bus stop with overhang, sidewalk lighting | 2/1/2022 8:10 PM |
| 369 | Bicycle lanes, better lighting, creating safety around the Salvation Army | 2/1/2022 8:07 PM |
| 370 | All of the ↑ | 2/1/2022 8:06 PM |
| 371 | See the same Response I wrote for the Boulevard of the arts | 2/1/2022 7:51 PM |
| 372 | Bike lane | 2/1/2022 7:44 PM |
| 373 | Eliminate blindspot for bikers and cars pulling out of parking | 2/1/2022 7:21 PM |
| 374 | Wide bike lanes away from traffic | 2/1/2022 7:18 PM |
| 375 | A wide and barrier-lined 2 lane road way. Trees by bus stops! And benches with trashcans. | 2/1/2022 6:49 PM |
| 376 | Protected bike lane and protected place to access public transportation | 2/1/2022 6:48 PM |

Boulevard of the Arts & 10th Street Complete Streets Survey

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| 377 | Safe lanes Good stores, cafes with outside tables | 2/1/2022 6:36 PM |
| 378 | It is still frightening to cross Tamiami St because the cars do not stop but often roar through red lights. | 2/1/2022 6:31 PM |
| 379 | Safe lane | 2/1/2022 6:13 PM |
| 380 | I would love to see 10th street become a beautiful avenue to connect to Gillespie Park. It is only about a mile from US 41, but mentally it seems lightyears away. | 2/1/2022 5:41 PM |
| 381 | Bike lanes that have a barrier between the bike and the traffic | 2/1/2022 5:33 PM |
| 382 | Pedestrian safety enhancement | 2/1/2022 5:25 PM |
| 383 | Protected bike lanes | 2/1/2022 5:25 PM |
| 384 | appropriate bike paths | 2/1/2022 5:24 PM |
| 385 | bike racks | 2/1/2022 5:21 PM |
| 386 | Protected bike lanes and proper lighting, bike racks and scooter and bike rentals | 2/1/2022 5:13 PM |
| 387 | dedicated bike lanes | 2/1/2022 5:06 PM |
| 388 | Cross bridge for pedestrians at roundabouts | 2/1/2022 5:02 PM |
| 389 | Nothing | 2/1/2022 4:56 PM |
| 390 | Safe lanes, speed control of vehicles | 2/1/2022 4:51 PM |
| 391 | Opening 12th Street from Orange Avenue to US41 | 2/1/2022 4:36 PM |
| 392 | Since I live just south of 10th I would love to walk, run and bicycle to the park. The cars traveling north at the 10th street circle do not respect pedestrians. I am afraid to walk across the crossings so I quit risking my life to get across. | 2/1/2022 4:26 PM |
| 393 | Bike lanes | 2/1/2022 4:25 PM |
| 394 | Wide sidewalks, safety, good lighting | 2/1/2022 4:21 PM |
| 395 | walkways | 2/1/2022 4:16 PM |
| 396 | wider bike lanes | 2/1/2022 4:13 PM |
| 397 | Bike paths separate from motor vehicles | 2/1/2022 4:09 PM |
| 398 | bike racks, bus shelters, bike lanes, public-use bikes | 2/1/2022 3:51 PM |
| 399 | Better design of bike lanes than currently in place. Protected bike lanes...not lanes where bicyclists have to travel in a lane that is flanked by car lanes on either side. Drivers do NOT respect bicyclists! | 2/1/2022 3:39 PM |
| 400 | Safe well lit spaces. It is an unsafe area and avoid it if possible. | 2/1/2022 3:34 PM |
| 401 | covered bike areas for rainy season | 2/1/2022 3:32 PM |
| 402 | Trees, crosswalks on street parking, stop signs, bus stops | 2/1/2022 3:04 PM |
| 403 | wider sidewalks, bicycle lanes, shade trees | 2/1/2022 2:56 PM |
| 404 | Better pedestrian lighting Safety concerns. | 2/1/2022 2:51 PM |
| 405 | Shade! It's a super open space when you're biking | 2/1/2022 2:47 PM |
| 406 | Build bicycle parking stations with plug in recharging stations for e-bikes (like what has happened with car charging stations in some public places). | 2/1/2022 2:44 PM |
| 407 | bookstores, cafes with outdoor seating, mini-parks, | 2/1/2022 2:35 PM |
| 408 | Bike share Trolleys | 2/1/2022 2:04 PM |
| 409 | No cars | 2/1/2022 1:49 PM |
| 410 | Bike lanes | 2/1/2022 1:46 PM |

Boulevard of the Arts & 10th Street Complete Streets Survey

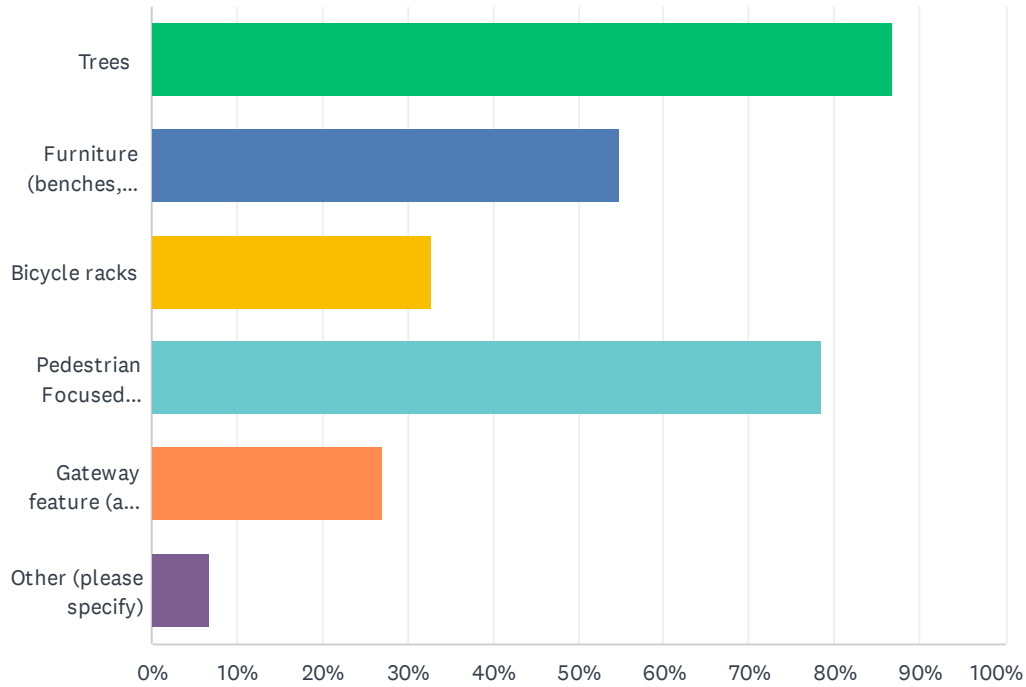
| | | |
|-----|---|-------------------|
| 411 | Protected bike lanes, shade trees, and bus shelters/benches | 2/1/2022 1:46 PM |
| 412 | Protected bike lanes | 2/1/2022 1:30 PM |
| 413 | Protected bike lanes | 2/1/2022 1:11 PM |
| 414 | Protected bike lanes, shaded by day & well-lit at night. Attractive public transit shelters (with plants). | 2/1/2022 1:11 PM |
| 415 | Street lights. Safety measures; especially near the Salvation Army. | 2/1/2022 1:10 PM |
| 416 | It seems like cycling and public transit are the only stinking things you're concerned with. Knock it off. This is a neighborhood not a mini-city. | 2/1/2022 1:09 PM |
| 417 | SAFETY Bike paths. Lights to help bikes cross intersections. | 2/1/2022 1:03 PM |
| 418 | bike rack, bike lane | 2/1/2022 12:59 PM |
| 419 | Safety first...street lights. Improved maintenance of public areas | 2/1/2022 12:45 PM |
| 420 | Protected bike lanes | 2/1/2022 12:42 PM |
| 421 | Park | 2/1/2022 12:36 PM |
| 422 | Bike racks/lanes | 2/1/2022 12:35 PM |
| 423 | Bike lanes that are wider than those on Cocoanut. No one uses the bike lanes on Cocoanut. They use the sidewalks for bikes scooters etc. Pedestrians have to move to the bike lanes. Not good. | 2/1/2022 12:34 PM |
| 424 | PROTECTED bike lanes | 2/1/2022 12:33 PM |
| 425 | Safe cycling on north 41. Why are there lamp posts in the middle of the sidewalk and no bike lane? Make a multi use path along 41 and ped/bike bridge across. | 2/1/2022 12:27 PM |
| 426 | Shade trees to rest | 2/1/2022 12:23 PM |
| 427 | Less traffic and parking | 2/1/2022 12:18 PM |
| 428 | Physically separated dedicated bike Lanes, that take more direct routes than cars. | 2/1/2022 12:01 PM |
| 429 | I would not cycle or use public transit on 10th st | 2/1/2022 12:00 PM |
| 430 | More security and less homeless | 2/1/2022 11:58 AM |
| 431 | I don't feel safe here unless I am in my car. | 2/1/2022 11:47 AM |
| 432 | Buffered bike lanes | 2/1/2022 11:43 AM |
| 433 | Bike lanes, protected pedestrian walkways | 2/1/2022 11:38 AM |
| 434 | Bus shelters in this area are used for illegal activity (generally not waiting for the bus) given the location of the Salvation Army. Bus stops (not shelters) should be utilized where necessary. There is an incredible opportunity to make this a "bike-way" to the Bay. | 2/1/2022 11:24 AM |
| 435 | Protected bike lane, wide sidewalk, plantings | 2/1/2022 11:10 AM |
| 436 | Getting indigent and homeless individuals off of the street, I would never want to wait for public transportation on 10th street, I don't feel safe there outside of my car. | 2/1/2022 11:09 AM |
| 437 | Covered bus stops, bike lanes, bike repair, bike lockers | 2/1/2022 11:00 AM |
| 438 | safety of bikers and walkers | 2/1/2022 10:59 AM |
| 439 | Bicycle lane. Need 10th Street to be major East-West connector street. | 2/1/2022 10:56 AM |
| 440 | Bike lanes. Rent a bike. Bus/trolley stops | 2/1/2022 10:55 AM |
| 441 | Bike racks, pedestrian mall, parking garage | 2/1/2022 10:51 AM |
| 442 | Separated bikes lanes with curbs to keep cars out | 2/1/2022 10:46 AM |
| 443 | Bicycle lanes already exist and I feel safe cycling on 10th street. Street trees and/or bus shelters would be an improvement. | 2/1/2022 10:42 AM |

Boulevard of the Arts & 10th Street Complete Streets Survey

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|-----|--|--------------------|
| 444 | Wider sidewalk....canopy trees and bike lane | 2/1/2022 10:41 AM |
| 445 | Covered bus stops | 2/1/2022 10:25 AM |
| 446 | Create safe lanes away from pedestrians and other vehicular traffic. | 2/1/2022 10:18 AM |
| 447 | Crosswalks, lighting | 2/1/2022 9:35 AM |
| 448 | Separate bike path, covered bike parking, business requirement to have bike racks, bike repair stations | 2/1/2022 9:31 AM |
| 449 | Accessible, safe areas , for walking and cycling | 2/1/2022 9:29 AM |
| 450 | Wider streets with dedicated bike lanes | 2/1/2022 9:26 AM |
| 451 | I don't understand the question. | 2/1/2022 9:04 AM |
| 452 | Easier, Faster, Safer crossing of Tamiami | 2/1/2022 9:01 AM |
| 453 | Bus stops, shelters, and trolley routes. Incorporating complete streets elements. | 1/28/2022 10:17 AM |
| 454 | protected bicycle lanes, bike repair kiosk, water fountains, sheltered transit stops with trash cans for garbage, recycling, and cigarette butts | 1/28/2022 10:15 AM |

Q16 What amenities would you like to see on 10th Street? Pick your top three.

Answered: 897 Skipped: 187



| ANSWER CHOICES | RESPONSES | |
|--|-----------|-----|
| Trees | 86.73% | 778 |
| Furniture (benches, garbage/recycling bins) | 54.74% | 491 |
| Bicycle racks | 32.78% | 294 |
| Pedestrian Focused lighting | 78.37% | 703 |
| Gateway feature (a formal entrance to 10th Street) | 27.09% | 243 |
| Other (please specify) | 6.91% | 62 |
| Total Respondents: 897 | | |

Q17 What improvements would you like to see on 10th Street?

Answered: 375 Skipped: 709

| # | RESPONSES | DATE |
|----|---|--------------------|
| 1 | Make it resident friendly. | 2/28/2022 3:48 PM |
| 2 | Cafés, public art, trees, landscape , shops that serve the local area | 2/28/2022 1:53 PM |
| 3 | Better automobile traffic flow. | 2/28/2022 1:47 PM |
| 4 | slow traffic downnnnn | 2/28/2022 12:18 PM |
| 5 | Street art would make the area more inviting. I'm thinking of the graffiti walls in the Design District in Miami. Also itinerant public art exhibits like the umbrellas exhibits, or snails sculptures that preceded Art Basel in Miami a few years back. ART will make the street more exciting and inviting. | 2/28/2022 11:58 AM |
| 6 | Making it pedestrian and bike friendly would be wonderful. And, having small local business would be great to access. | 2/27/2022 9:42 PM |
| 7 | A better feeling of the area being connected. Paths, trees and cycle/walk lanes and sidewalks. | 2/26/2022 8:20 AM |
| 8 | Pedestrian overpass bridge | 2/26/2022 12:01 AM |
| 9 | Helping the homeless | 2/25/2022 3:54 PM |
| 10 | Raised pedestrian path over 41 | 2/25/2022 10:52 AM |
| 11 | Pedestrian Bridge | 2/25/2022 6:10 AM |
| 12 | I would like 10th street to be free from the homeless and addicts laying around. perhaps if there was more build out in general of landscaping, lighting and activity that would help | 2/25/2022 4:55 AM |
| 13 | Above listed improvements would go a long way.....towards making progress in a less developed area of Sarasota. | 2/24/2022 8:15 PM |
| 14 | Safety | 2/24/2022 1:19 PM |
| 15 | outdoor parklets and cafes, trees and flowers, parking or parking garage | 2/24/2022 11:56 AM |
| 16 | Lighting | 2/24/2022 10:04 AM |
| 17 | Clean up closed business & empty lots. Clean up the bus stops near 10th | 2/24/2022 9:02 AM |
| 18 | Pedestrian friendly | 2/24/2022 6:49 AM |
| 19 | Ease of traffic paths. | 2/23/2022 10:08 PM |
| 20 | See above | 2/23/2022 7:34 PM |
| 21 | pedestrian bridges | 2/23/2022 4:38 PM |
| 22 | Traffic flow | 2/23/2022 3:30 PM |
| 23 | Pedestrian bridge to the Bay from 10th street and from Blvd of the Arts | 2/23/2022 3:23 PM |
| 24 | No right turn on red | 2/23/2022 3:06 PM |
| 25 | Lights | 2/23/2022 2:53 PM |
| 26 | Better traffic control. Circle is a gridlock nightmare after an event at Van Wezel | 2/23/2022 2:41 PM |
| 27 | At least there is some space on 10th St. that can be utilized for some different ideas. To encourage bike riding in the downtown area is very dangerous to me. There has been too much development and very little space in downtown to make it biker-friendly. The building of the Westin should tell you no one was thinking about the future when designing this town. | 2/23/2022 2:39 PM |

Boulevard of the Arts & 10th Street Complete Streets Survey

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| 28 | Remove the dangerous roundabout. | 2/23/2022 1:50 PM |
| 29 | Places to sit | 2/23/2022 1:40 PM |
| 30 | Outdoor cafes,more trees, bike racks, garbage cans, better traffic flow, parking garage and above ground pedestrian walkways and better lighting | 2/23/2022 1:39 PM |
| 31 | pedestrian lighting, trees, landscaping | 2/23/2022 1:01 PM |
| 32 | Lights | 2/23/2022 12:45 PM |
| 33 | If you accomplished all that it's offered above, that would be outstanding. | 2/23/2022 12:29 PM |
| 34 | Better lighting and traffic relief | 2/23/2022 12:16 PM |
| 35 | Trees and benches | 2/23/2022 11:58 AM |
| 36 | Pedestrian Bridge over 41 | 2/23/2022 11:50 AM |
| 37 | I don't want to see any more "improvements" like those that you have made to downtown Sarasota. It's a gridlock. It is so bad that we now have to avoid the downtown area and St. Armands Circle which we used to enjoy. I don't want to see anymore of your so called improvements. | 2/23/2022 11:30 AM |
| 38 | Parking garage. | 2/23/2022 11:10 AM |
| 39 | streetscape | 2/23/2022 11:09 AM |
| 40 | More visible police | 2/23/2022 11:02 AM |
| 41 | Pedestrian bridge | 2/23/2022 10:56 AM |
| 42 | pave the road! | 2/23/2022 10:55 AM |
| 43 | update this street to look like it "belongs" in the City of Sarasota including trees, more green, less step child street of the City | 2/23/2022 10:51 AM |
| 44 | trees and green spaces | 2/23/2022 10:44 AM |
| 45 | Trees | 2/23/2022 10:43 AM |
| 46 | Safe walking and riding amenities | 2/23/2022 10:41 AM |
| 47 | Cafés,restaurant's small business | 2/23/2022 10:33 AM |
| 48 | Cafes restaurants stores | 2/23/2022 10:33 AM |
| 49 | Able to drive thru from US 301 on the way to Van Wezel parking lot. | 2/23/2022 10:32 AM |
| 50 | Pedestrian overpass | 2/23/2022 10:29 AM |
| 51 | I live on 10th street, I can hardly cross the street in front of my house where there is a pedestrian walk way and sign. Cars DO NOT respect the signs and always fly by 10th street. We need a much better way to ensure the safety of residents. There are lots of kids and people with pets that walk by. | 2/23/2022 10:29 AM |
| 52 | Remove the Salvation Army to another spot. | 2/23/2022 10:28 AM |
| 53 | Better lighting and cleanliness | 2/23/2022 10:26 AM |
| 54 | Emergency Call boxes | 2/23/2022 10:26 AM |
| 55 | Pedestrian bridge, please! | 2/23/2022 10:17 AM |
| 56 | Circle is a mess! | 2/23/2022 10:17 AM |
| 57 | Safety improvements for bicycles and pedestrian | 2/23/2022 10:15 AM |
| 58 | Good lighting, good signage; good traffic barriers | 2/23/2022 10:11 AM |
| 59 | More control over the homeless that camp out. People need to feel safe when visiting businesses located in the area. If you want them to walk and bike and partake in local businesses, they need to feel welcome and safe. NOT looking over their shoulders as they go to nad from their cars or homes. | 2/23/2022 10:10 AM |

Boulevard of the Arts & 10th Street Complete Streets Survey

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| 60 | Improved security. Not a walk friendly area at the moment. | 2/23/2022 10:10 AM |
| 61 | Things to increase ped safety. With the boat ramp across the street, boaters are always darting across 41 to get to the Publix. | 2/23/2022 10:09 AM |
| 62 | There is a DESPERATE need for safe pedestrian passage in the form of a pedestrian bridge to access the Bayfront property/Van Wezel from both east of 41/downtown and from the Ringling Bridge road. | 2/23/2022 10:09 AM |
| 63 | Safety | 2/23/2022 10:08 AM |
| 64 | Must keep traffic moving. Downtown Sarasota is a mess with the circles. Your picture above with the truck shows how difficult it is now. | 2/23/2022 10:03 AM |
| 65 | Ensure good lighting and safety. It's not a very safe area. | 2/23/2022 9:57 AM |
| 66 | pedestrian bridges | 2/23/2022 9:57 AM |
| 67 | Better traffic flow, cleaner streets and sidewalks | 2/23/2022 9:53 AM |
| 68 | pedestrian bridges | 2/23/2022 9:48 AM |
| 69 | Right now 10th is extremely unacceptable for basic walking and biking comfort. Safety features, lighting, and SHADE are needed. | 2/23/2022 9:46 AM |
| 70 | Protected bikeway, trees and landscaping, street furniture | 2/22/2022 11:56 PM |
| 71 | pedestrian bridge over 41 | 2/21/2022 6:04 PM |
| 72 | Plenty of lighting beautiful flowering trees flower boxes by cracks bike Lanes places to drink water and benches | 2/17/2022 8:30 AM |
| 73 | Just making it safe for pedestrians including the people who cannot move very quickly when it comes to crossing traffic. | 2/16/2022 6:59 AM |
| 74 | Elevated walkway over Tamiami Trail. | 2/15/2022 11:01 PM |
| 75 | Pedestrian Bridge | 2/15/2022 6:29 PM |
| 76 | Connection to the Bay | 2/15/2022 3:20 PM |
| 77 | I would like to see a pedestrian bridge across Tamiami Trail. | 2/15/2022 1:58 PM |
| 78 | Re: #16- 10th Street will need additional security due to homeless population. Difficult balancing act. | 2/15/2022 1:21 PM |
| 79 | More benches | 2/15/2022 9:29 AM |
| 80 | Green spaces | 2/15/2022 8:40 AM |
| 81 | 10th Street has a long way to go to become a pedestrian street--its really more of a traffic sewer/gateway into Downtown Sarasota. Maybe its OK that way with nice roundabouts at Coconut and Central to make those areas bike/ped friendly gateways. | 2/15/2022 8:21 AM |
| 82 | Clean up the homeless | 2/14/2022 11:40 PM |
| 83 | trees and lighting | 2/14/2022 9:58 PM |
| 84 | Open more lanes to make traveling by automobile easier | 2/14/2022 3:10 PM |
| 85 | Landscaping | 2/14/2022 2:13 PM |
| 86 | More business and landscaping | 2/14/2022 2:00 PM |
| 87 | Trees, pedestrian accommodating facilities ie cafes, benches | 2/14/2022 11:58 AM |
| 88 | Remove all homeless people who squat,sleep,lay down,or gather PLEASE REMOVE , THIS WILL ALLOW MOTHERS WITH BABIES,SMALL CHILDREN , WOMEN,MEN,TO WALK,SHOP,WITHOUT FEAR OF SAFETY OR ISSUES ,OR GARBAGE CREATED AND LEFT ON STREETS | 2/14/2022 10:02 AM |
| 89 | a better traffic light system | 2/14/2022 9:50 AM |
| 90 | It has no identity. Make it a unique destination | 2/14/2022 9:40 AM |

Boulevard of the Arts & 10th Street Complete Streets Survey

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| 91 | wider sidewalks and shade trees | 2/14/2022 9:28 AM |
| 92 | walkway and/or safer crossing for pedestrians to Bay Park | 2/14/2022 8:52 AM |
| 93 | Things that encourage foot traffic and pedestrian friendly business | 2/14/2022 8:26 AM |
| 94 | Trshcans and improved pedestrian lighting | 2/13/2022 7:07 PM |
| 95 | Make it easier for pedestrians to cross 10th. Slow cars down | 2/13/2022 6:50 PM |
| 96 | Elimination of salvation army clients | 2/13/2022 5:28 PM |
| 97 | find housing for the homeless | 2/13/2022 1:03 PM |
| 98 | pedestrian bridge | 2/13/2022 10:41 AM |
| 99 | Help for homeless lying on the street near Salvation Army | 2/13/2022 7:54 AM |
| 100 | see above | 2/13/2022 12:36 AM |
| 101 | Cafes and trees and elevated crosswalk across Tamiami. | 2/12/2022 5:53 PM |
| 102 | Many more trees. I like the pedestrian walkway between the Trail and Cocanut. Do not feel safe walking to Publix. I always drive. Any way to make the public feel safe in this area. It does not have a good reputation. | 2/12/2022 2:34 PM |
| 103 | Pedestrian overpass | 2/12/2022 1:46 PM |
| 104 | Area need redevelopment that creates a safe and welcoming environment for pedestrians | 2/12/2022 1:20 PM |
| 105 | see answer for Blvd of Arts | 2/12/2022 11:07 AM |
| 106 | Support and maintain good traffic flow | 2/12/2022 10:36 AM |
| 107 | Benches Leave open space with shade. | 2/12/2022 10:34 AM |
| 108 | Pedestrian access from The Bay | 2/12/2022 10:02 AM |
| 109 | Pedestrian bridge to The Bay | 2/12/2022 9:39 AM |
| 110 | Resturants, shops, cafes improved lighting, speed bumps, decreased speed limit | 2/12/2022 8:20 AM |
| 111 | Better traffic flow | 2/11/2022 10:15 PM |
| 112 | protected bike lanes | 2/11/2022 5:21 PM |
| 113 | Would prefer to have this accommodate greater traffic flow. The traffic between 41 and 301 can be severe at times, and providing more accessible routes could help alleviate it. | 2/11/2022 11:44 AM |
| 114 | breweries, beer gardens, cafes, etc | 2/11/2022 11:31 AM |
| 115 | Bicycle focused mobility for people from the Legacy trail to visit, relax, sit down for a cup of coffee and pastries, chat with neighbors, be able to safely walk through the area even at night (it's not a very safe neighborhood at night). | 2/11/2022 7:46 AM |
| 116 | green space - good pedestrian access | 2/10/2022 2:41 PM |
| 117 | Streets lights and landscaping. | 2/10/2022 11:20 AM |
| 118 | Design an area that doesn't depend on car travel. Our city should support pedestrian, public transit, and bicycle travel. | 2/10/2022 8:32 AM |
| 119 | trees flowers | 2/9/2022 12:22 PM |
| 120 | Pedestrian bridge over Tamiami Trail | 2/9/2022 12:17 PM |
| 121 | Safety | 2/8/2022 6:16 PM |
| 122 | removal of all the old, abandoned buildings | 2/8/2022 6:04 PM |
| 123 | Any safety improvements | 2/8/2022 2:03 PM |
| 124 | Less loitering near Central Avenue and the Salvation Army. | 2/8/2022 1:06 PM |

Boulevard of the Arts & 10th Street Complete Streets Survey

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| 125 | seeking more police on duty in this area, assisting with the homeless activity. | 2/8/2022 9:55 AM |
| 126 | Protected bike lanes | 2/8/2022 8:38 AM |
| 127 | More pedestrian friendly | 2/8/2022 8:05 AM |
| 128 | move salvation army, its extremely dirty and not a safe area to walk alone | 2/8/2022 6:24 AM |
| 129 | safety and provide an alternate safe haven for the homeless | 2/7/2022 11:38 PM |
| 130 | I think it's a good idea to have 10th Street and Fruitville to be high traffic lanes and use Blvd of Arts (between them) as low speed and low vehicular traffic thorofares. | 2/7/2022 11:11 PM |
| 131 | N/a | 2/7/2022 10:04 PM |
| 132 | Emergency call boxes | 2/7/2022 8:51 PM |
| 133 | Bring in more leisure business, make it pedestrian friendly. | 2/7/2022 8:37 PM |
| 134 | tear down cement plant and have tenants in vacant buildings on north side | 2/7/2022 8:36 PM |
| 135 | Safety--I do not feel comfortable walking there. Dog friendly parks would be great. | 2/7/2022 8:09 PM |
| 136 | More cafes. | 2/7/2022 7:04 PM |
| 137 | 10th St should be the main east-west vehicular roadway on the northern side of Rosemary District. I don't see it as a pedestrian-enhanced walkway or location for street festivals. | 2/7/2022 6:34 PM |
| 138 | More security and keeping homeless from laying on sidewalks | 2/7/2022 4:25 PM |
| 139 | Basically empty space now so develop a mixture of an entertainment and residential district | 2/7/2022 2:01 PM |
| 140 | implementation of parks in place of stuffing tall buildings into vacant lots | 2/7/2022 1:47 PM |
| 141 | I live downtown and I walk to the Bay, sometimes stopping at Publix, and would love to have a cafe to stop for a glass of wine, and be free from cars. The only way we get to sit outside is on the sidewalk next to traffic and parked cars. This city will never be truly walkable without attractions like stores , shops, cafes and relaxing areas that aren't tertiary to cars and parking. It's segmented by Fruitville and 10th Sts, not to mention 41. | 2/7/2022 1:05 PM |
| 142 | Pedestrian Bridge over Tamiami | 2/7/2022 12:22 PM |
| 143 | None | 2/7/2022 12:14 PM |
| 144 | A PEDX BRIDGE OVER 41 HERE OR BOTA | 2/7/2022 11:46 AM |
| 145 | bike friendly and safe | 2/7/2022 11:27 AM |
| 146 | Pedestrian crossing overhead bridge. | 2/7/2022 10:35 AM |
| 147 | safety for pedestrians. | 2/7/2022 9:52 AM |
| 148 | Traffic calming and other business amenities which would improve the transition into downtown and also to The Bay Park. | 2/7/2022 9:29 AM |
| 149 | speed bumps near Gillespie Park | 2/7/2022 7:37 AM |
| 150 | A pedestrian bridge over Tamiami Trail to the Bay. | 2/7/2022 7:36 AM |
| 151 | Homelessness is definitely a safety problem for pedestrians in this area. | 2/7/2022 7:15 AM |
| 152 | PrdestrIN bridge | 2/7/2022 2:52 AM |
| 153 | Wider sidewalks | 2/7/2022 12:09 AM |
| 154 | Painted bike lanes, NOT separated bike lanes | 2/6/2022 8:52 PM |
| 155 | hard to imagine - but the changes in RoseMary district give me hope that this could be a safe area to walk and for more businesses to be located here | 2/6/2022 7:56 PM |
| 156 | PROTECTED bike lanes | 2/6/2022 5:26 PM |
| 157 | Bike and water amenities. Sup access | 2/6/2022 3:39 PM |
| 158 | I rarely go down to 10 th street. I cross at 41 and Blvd of the arts to get to the bay which I walk | 2/6/2022 3:03 PM |

Boulevard of the Arts & 10th Street Complete Streets Survey

the path at least 2 times per week. I avoid the circles on 41 and take the less traveled back streets to get back on 41 when I am traveling north .

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| 159 | Additionally, a pedestrian overpass at 10th or 9th to access the new PAC and park. This overpass should likely be designed to also accommodate narrow rubber tire transit, as there is plenty of ROW in either location. | 2/6/2022 1:00 PM |
| 160 | 10th st has always seemed to me to be an absolute waste. It's wide, it's traffic free and most importantly, It has no eastern destination. It is not a major thru street between SR 41 and I-75 like Fruitville. Instead it's a kind of weird Blvd if the Arts without any existing charm. Improvements to the street itself need to follow or help to enable the creation of a neighborhood or a destination-specific activity like public parking for The Bay. The function(s) of 10th St need to be better defined before one worries about planting trees or adding signage. | 2/6/2022 10:16 AM |
| 161 | There's a big problem with homelessness that needs to be addressed | 2/6/2022 9:27 AM |
| 162 | a pedestrian bridge to the Bay park | 2/6/2022 8:43 AM |
| 163 | Clean up Salvation Army area and homeless issue | 2/6/2022 8:22 AM |
| 164 | Traffic calming | 2/6/2022 7:26 AM |
| 165 | Relocate Salvation Army complex | 2/6/2022 6:50 AM |
| 166 | More businesses, festivals, pedestrian overpass, | 2/6/2022 4:20 AM |
| 167 | More traffic flow. The roundabouts seem difficult for elderly and young speed freaks to navigate. You'll need better traffic signaling lights and bolder signs. FYI, in London, they got rid of their roundabouts. Please study that. | 2/6/2022 12:18 AM |
| 168 | Neighborhood friendly Dog area Great storm water filtered runoff Interesting Boating shops, food, bars | 2/5/2022 9:36 PM |
| 169 | Widen it | 2/5/2022 8:53 PM |
| 170 | more spd checking on the homeless i see sleeping on the sidewalk | 2/5/2022 8:52 PM |
| 171 | restaurants and shops, sidewalk trees; Same on BOTA, plus a pedestrian overpass since a roundabout will not fit at BOTA and US 41. | 2/5/2022 8:48 PM |
| 172 | Flowers benches trees lighting | 2/5/2022 8:20 PM |
| 173 | Safety! | 2/5/2022 5:49 PM |
| 174 | trees, green spaces - grass, flowers | 2/5/2022 5:34 PM |
| 175 | Ensure that the rotary works | 2/5/2022 4:19 PM |
| 176 | Landscaping better lighting | 2/5/2022 4:03 PM |
| 177 | increased businesses, a homeless shelter available during day hours not only evening, wider sidewalks, parks | 2/5/2022 3:56 PM |
| 178 | safety measures. it seems not as safe of an area | 2/5/2022 3:41 PM |
| 179 | Safe bike lanes & traffic calming | 2/5/2022 3:35 PM |
| 180 | More residential areas. | 2/5/2022 2:36 PM |
| 181 | Security to get rid of the Homeless and Drug Dealers | 2/5/2022 2:18 PM |
| 182 | Getting rid of the homeless | 2/5/2022 2:17 PM |
| 183 | protected bike lanes -- using planters or trees to add beauty and safety for the million bikes that will be visiting the city very soon. | 2/5/2022 2:17 PM |
| 184 | 10th street is pretty ugly but fairly functional, esp. between orange & 41. Would like it to be more bike friendly, I do not feel safe biking on it now. Would be nice to develop the area to be more connected with 41, esp. as The Bay takes off. | 2/5/2022 2:14 PM |
| 185 | Safety and reduced homelessness | 2/5/2022 2:02 PM |
| 186 | We need to find a way to decrease the concentrated number of homeless that hang out in the | 2/5/2022 1:58 PM |

Boulevard of the Arts & 10th Street Complete Streets Survey

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| | area. | |
| 187 | Safe walking areas | 2/5/2022 1:38 PM |
| 188 | Homeless people not allowed to sleep around Everyone should be helped to get back on their feet, but if they don't want to, should be committed for mental health care | 2/5/2022 1:14 PM |
| 189 | Trees and parking | 2/5/2022 1:04 PM |
| 190 | Focused Lighting for pedestrian walkways | 2/5/2022 12:35 PM |
| 191 | CITY IS SPENDING TOO MUCH MONEY | 2/5/2022 12:31 PM |
| 192 | overhead pedestrian crossings | 2/5/2022 12:26 PM |
| 193 | Move Salvation Army and homeless encampments further north east ...perhaps around 17 ST and 301 where a large number of social services are located! | 2/5/2022 12:22 PM |
| 194 | Overpass!! | 2/5/2022 12:14 PM |
| 195 | Green space | 2/5/2022 11:56 AM |
| 196 | New businesses | 2/5/2022 11:55 AM |
| 197 | Pedestrian bridge across Tamiami Trail | 2/5/2022 11:48 AM |
| 198 | landscaping | 2/5/2022 11:37 AM |
| 199 | Making it more of a destination | 2/5/2022 11:36 AM |
| 200 | Wider sidewalks | 2/5/2022 11:30 AM |
| 201 | Another foolish question. All of the above improvements would be welcome. | 2/5/2022 11:30 AM |
| 202 | No motorized vehicles except public transit | 2/5/2022 11:27 AM |
| 203 | Improved sidewalks and lighting | 2/5/2022 11:20 AM |
| 204 | More retail outlets & cafes | 2/5/2022 11:10 AM |
| 205 | Kids area splash fountain or integrated nature play space | 2/5/2022 11:05 AM |
| 206 | 10th is a very functional street. I don't see how amenities of the usual kind, besides safety, will help the street. The idea of "improving" the street imply moving some parts out of there. | 2/5/2022 11:03 AM |
| 207 | Improve pedestrian visibility at 10th and Rt 42. It is a very busy area and pedestrians are not easy to see with all the other activity. 10th St was narrowed down near the roundabout. Traffic is backing up there. Restore it. | 2/5/2022 10:59 AM |
| 208 | Improved landscaping | 2/5/2022 10:56 AM |
| 209 | Clean up the homeless issues on 10th and central. It is not safe to walk there. | 2/5/2022 10:50 AM |
| 210 | see above | 2/5/2022 10:45 AM |
| 211 | Protected bike lanes | 2/5/2022 10:32 AM |
| 212 | Restaurant development between Coconut and Orange Ave. | 2/5/2022 10:25 AM |
| 213 | Bike lanes. | 2/5/2022 10:17 AM |
| 214 | Wide sidewalks, more bike lanes, much more greenspace. | 2/5/2022 10:13 AM |
| 215 | Protected bike lanes | 2/5/2022 10:12 AM |
| 216 | Relocation of the Salvation Army to 17th street with the other services | 2/5/2022 10:08 AM |
| 217 | Pedestrian bridge over 41. | 2/5/2022 10:05 AM |
| 218 | pedestrian bridge and bike lanes | 2/5/2022 10:03 AM |
| 219 | Something to make drivers slowdown and pay attention to people and bikes. | 2/5/2022 9:43 AM |
| 220 | None | 2/5/2022 9:42 AM |

Boulevard of the Arts & 10th Street Complete Streets Survey

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| 221 | More greenspace. | 2/5/2022 9:42 AM |
| 222 | Safety. Lighting. Easy walking. Events. Trees. | 2/5/2022 9:42 AM |
| 223 | Elevated pedestrian/bike path over Tamiami trail to connect to The Bay | 2/5/2022 9:34 AM |
| 224 | Cafes | 2/5/2022 9:29 AM |
| 225 | Items to discourage the homeless from congregating | 2/5/2022 9:24 AM |
| 226 | Pedestrian bridge over Tamiami Trail | 2/5/2022 9:19 AM |
| 227 | Pocket parks, sidewalk cafes, more businesses | 2/5/2022 7:59 AM |
| 228 | Speed tables and move garbage at daycare from 10th side to orange side | 2/5/2022 7:28 AM |
| 229 | Bike lane | 2/5/2022 6:29 AM |
| 230 | More trees and green scape. | 2/5/2022 6:18 AM |
| 231 | E.V. charging. | 2/5/2022 1:53 AM |
| 232 | Create an atmosphere that will draw pedestrians and give them things to do, such as cafes, restaurants, entertainment venues, etc. | 2/5/2022 12:49 AM |
| 233 | Protected bike lanes and crosswalks | 2/4/2022 9:45 PM |
| 234 | green space | 2/4/2022 9:30 PM |
| 235 | Beautification and Gentrification. | 2/4/2022 4:40 PM |
| 236 | 10th st for me is a short cut over to 301 and that's the same for many who want to avoid going further south on the trail but need to get over to 301..... | 2/4/2022 4:36 PM |
| 237 | Landscaping | 2/4/2022 4:31 PM |
| 238 | bike and pedestrian focused transportation...cars third | 2/4/2022 4:07 PM |
| 239 | The homeless issue has to be addressed first. The number of homeless on 10th by the Salvation Army discourage walking (especially in the evening) and any other pedestrian activities. I live In Gillespie Park and walk to the bay but I only walk there on Blvd of the Arts. 10th street is too unpleasant. | 2/4/2022 3:44 PM |
| 240 | Traffic control and police presence. | 2/4/2022 3:25 PM |
| 241 | Easier parking, overflow areas and it appear to be an extension from the Bay into downtown. Use lighting and movement to draw curiosity of "what is down that way". | 2/4/2022 9:00 AM |
| 242 | No homeless people. They are everywhere. | 2/4/2022 8:38 AM |
| 243 | The area from the N Orange Ave stoplight headed towards N Tamiami Trail is very ill-lit. Especially in front of the ABC News Building and across the Salvation Army. People nonchalantly cross that street even in moving traffic. I have seen a couple unfortunate accidents in the particular area and came very close to one myself and it is frightening. It is definitely a potential liability area which should have more lighting added to it. A couple solar lighting posts should do the trick. Aesthetic wise, the flexible spaces for pedestrians, lighting included, would be a good fit although less economic. | 2/3/2022 11:12 PM |
| 244 | More residential homes | 2/3/2022 7:05 PM |
| 245 | more festival's | 2/3/2022 6:06 PM |
| 246 | Relocation of the Salvation Army facility to elsewhere in the County to reduce the number of vagrants in the area | 2/3/2022 3:51 PM |
| 247 | Roadway repurposing. Street trees. Wider sidewalks and bike lanes. | 2/3/2022 2:06 PM |
| 248 | More connection to Newtown area | 2/3/2022 2:00 PM |
| 249 | Close the salvation army location and get rid of the homeless drug addicts | 2/3/2022 11:21 AM |
| 250 | sidewalks & bike lanes | 2/3/2022 11:17 AM |
| 251 | Move the Salvation Army barracks and a lot of problems and expense would decrease. | 2/3/2022 9:52 AM |

Boulevard of the Arts & 10th Street Complete Streets Survey

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| 252 | fix the current road, mark the lanes for 10th St, and Orange Ave, better sidewalks, and bike lanes | 2/3/2022 8:15 AM |
| 253 | Quality landscaping that includes all plant material besides just trees, public art and musical ambiance. | 2/3/2022 7:23 AM |
| 254 | safer bike lanes, fewer cops | 2/3/2022 3:52 AM |
| 255 | Better traffic management at peak hours. Better signs | 2/2/2022 11:31 PM |
| 256 | Keep it from being a place where homeless scare away pedestrians | 2/2/2022 8:44 PM |
| 257 | Please don't make it where the homeless are punished for their presence. | 2/2/2022 8:25 PM |
| 258 | NONE needed! Driving between 41 and orange (that's the weeny distance designated by this limited , narrow survey, right?) Is quick and easy. Fyi I drive 10th (from East St) to access Coconut and go north. | 2/2/2022 5:57 PM |
| 259 | address the homeless problem so people actually feel comfortable walking and biking | 2/2/2022 4:31 PM |
| 260 | More police presence. | 2/2/2022 4:21 PM |
| 261 | Place homeless in shelters or programs other than lying on the sidewalks....not good for them, the community, or businesses in the area. | 2/2/2022 4:15 PM |
| 262 | Address homeless population problem | 2/2/2022 4:09 PM |
| 263 | Demolition of old buildings and remodeling of structures all along 10th street. One of the ugliest streets coming into the area. | 2/2/2022 3:07 PM |
| 264 | Wider sidewalks and shade trees. Decorative pavers at intersections. Pocket parks. | 2/2/2022 1:52 PM |
| 265 | An attractive, clean environment - shops, restaurants, other businesses; trees, pedestrian friendly. | 2/2/2022 1:47 PM |
| 266 | Why must we always be building, building and building more? I left Miami after 49 years because it was over built. Please don't so that here. | 2/2/2022 1:45 PM |
| 267 | Design it from the start of the project to physically separate cyclists/pedestrians etc from vehicles. Worry about where to put the trees only after you've got a workable vehicle vs. pedestrian solution. | 2/2/2022 1:45 PM |
| 268 | There's a lot of dust in the area. There happens to be a concrete recycle company thing at 14th and Central creating a mess in the whole area. That needs to be cleaned up so its safer for citizens and guests | 2/2/2022 1:14 PM |
| 269 | unknown | 2/2/2022 1:06 PM |
| 270 | Wide sidewalks, bicycle lanes, lighting | 2/2/2022 12:27 PM |
| 271 | anything | 2/2/2022 11:49 AM |
| 272 | Wider/safer bike lanes and more trees/landscaping for shade | 2/2/2022 11:05 AM |
| 273 | Improve pedestrian safety, with lighting, open spaces, remove trash and hidden areas. | 2/2/2022 10:56 AM |
| 274 | Clean up the street, eliminate loitering outside of shelter, eliminate homeless gatherings and people sleeping/camping on sidewalks and streets between 10th and blvd of the arts. | 2/2/2022 10:49 AM |
| 275 | better lighting, bike lane, more local cafes, businesses | 2/2/2022 10:46 AM |
| 276 | A larger crosswalk and blinking lights from the apt. Bldg to the Publix near the roundabout. | 2/2/2022 10:41 AM |
| 277 | The biggest issue is finding a way to deal with the homeless hangout at Central & 10th. Any accommodations for benches and public space will be overwhelmed with the homeless and scare people away. The area does not feel safe after dark and it is impossible to walk down the sidewalks on Central. I would prefer the money be spent to help address the homeless issue that on street improvements. There are no businesses on 10th that attract people like there are on other east west streets | 2/2/2022 9:41 AM |
| 278 | Clean up the garbage. Eliminate the homeless camps. | 2/2/2022 8:49 AM |

Boulevard of the Arts & 10th Street Complete Streets Survey

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| 279 | My husband and I own the old Ice House and our property is accessed from Florida. Right now you can't turn left onto Florida traveling west. I'd like to see a middle turn lane or roundabout so that people can access our property without going all the way to the light or roundabout at 41. | 2/2/2022 7:44 AM |
| 280 | It needs a total overhaul including cleaning up the Broadway Promenade. New businesses, trees - something to draw me there. | 2/2/2022 7:40 AM |
| 281 | Make it safer to walk around with our being harassed. | 2/2/2022 7:34 AM |
| 282 | More focused business with cafes | 2/2/2022 7:13 AM |
| 283 | Travel lanes | 2/2/2022 6:58 AM |
| 284 | Shade trees, landscaping and practical art | 2/2/2022 12:55 AM |
| 285 | Cycle carriers (transportation) for pedestrians | 2/2/2022 12:24 AM |
| 286 | Mix of business, retail, restaurants and greenspace | 2/1/2022 11:27 PM |
| 287 | More police presence to reduce crime | 2/1/2022 11:11 PM |
| 288 | It's a shame that so many homeless people sleep on the sidewalks and store fronts. With the increased pan-handling , it often feels unsafe. | 2/1/2022 11:05 PM |
| 289 | the street repaved by the park | 2/1/2022 10:31 PM |
| 290 | The intersection at Orange and tenth needs to be reconfigured to accommodate pedestrian traffic. And safety. The street needs to consider access of service vehicles as well as public transportation. | 2/1/2022 9:59 PM |
| 291 | More Car lanes | 2/1/2022 9:48 PM |
| 292 | Maintain current Old Florida charm while improving safety conditions for shops, restaurants, parks, businesses | 2/1/2022 9:33 PM |
| 293 | lighting | 2/1/2022 8:56 PM |
| 294 | Please remove all the homeless people | 2/1/2022 8:55 PM |
| 295 | Pedestrian friendly spaces, good lighting, traffic calming, landscaping to reduce pollution and noise. | 2/1/2022 8:55 PM |
| 296 | Bike lanes, connect to Legacy trail and the bay | 2/1/2022 8:35 PM |
| 297 | Sidewalk lighting, garbage and recycling bins, more landscaping | 2/1/2022 8:10 PM |
| 298 | Have to clean up the Salvation Army situation with so many homeless people sleeping on 10th St sidewalks. | 2/1/2022 8:07 PM |
| 299 | Clean, clean, clean and remove anything and everything that isn't appealing to the eye or you will never draw attention to the area. You have been trying for years but I for one rarely feel safe walking there even in the daylight | 2/1/2022 8:06 PM |
| 300 | I would like to see affordable housing for low and middle income families. I would for those of latinx and black backgrounds to not be pushed out with these projects. It appears to be a thinly veiled approach to moving wealthy out-of-towners into the area. This would push those of us who work in the city out of the district. It is an example of systematic racial and socioeconomic discrimination. | 2/1/2022 7:58 PM |
| 301 | Parking | 2/1/2022 7:40 PM |
| 302 | More local business with outdoor space in front of buildings. More palms! (We are in Florida ya know ;) | 2/1/2022 7:21 PM |
| 303 | Community focused businesses and cafes for culture and a meeting place for all the new residents. As of right now I am uncomfortable riding my bike on 10th street bike lane, and especially not at night. | 2/1/2022 6:49 PM |
| 304 | Connecting the Rosemary District and the neighborhood north of 10th Street | 2/1/2022 6:48 PM |
| 305 | Cohesive attractive streetscape Greenery, parklike Sidewalks wide not bland concrete Art installation | 2/1/2022 6:36 PM |

Boulevard of the Arts & 10th Street Complete Streets Survey

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| 306 | Good automobile traffic flow | 2/1/2022 6:32 PM |
| 307 | nothing is going to improve on 10th St. until you get the homeless to stop living there and leaving trash all over the sidewalks | 2/1/2022 6:02 PM |
| 308 | More stores and businesses | 2/1/2022 5:59 PM |
| 309 | A lot more greenery, wide sidewalks all the way from 41 to Gillespie Park and safe cycling lanes. | 2/1/2022 5:41 PM |
| 310 | public parking garage | 2/1/2022 5:34 PM |
| 311 | As above | 2/1/2022 5:25 PM |
| 312 | Balanced approach and safety | 2/1/2022 5:25 PM |
| 313 | brick for street pavement would enhance overall look | 2/1/2022 5:24 PM |
| 314 | Remove chainlink fence around the large retention basin at Orange Avenue, and replace with a more attractive aluminum picket fence if a fence is really needed. Nothing makes an environment feel unfriendly quite like chainlink. | 2/1/2022 5:22 PM |
| 315 | landscape, trees, homeless off street | 2/1/2022 5:21 PM |
| 316 | All the above mentioned. Also, control of the homeless population not taking over the areas | 2/1/2022 5:13 PM |
| 317 | pedestrian lighting | 2/1/2022 5:06 PM |
| 318 | Palm trees lining the streets | 2/1/2022 5:04 PM |
| 319 | More active businesses. | 2/1/2022 4:57 PM |
| 320 | Walk bridge from east side of 41 to CENTENNIAL PARK | 2/1/2022 4:56 PM |
| 321 | Move Salvation Army to current fairgrounds property and move fairgrounds east of I75 | 2/1/2022 4:36 PM |
| 322 | The 10th street crossing on Tamiami is not safe to cross. The cars traveling north on Tamiami do not respect pedestrians. You take your life in your hands when you try to cross. It sucks and needs to be addressed. | 2/1/2022 4:26 PM |
| 323 | Favor the pedestrian. | 2/1/2022 4:25 PM |
| 324 | Sidewalks and cafes | 2/1/2022 4:21 PM |
| 325 | greenery | 2/1/2022 4:16 PM |
| 326 | Better bike lanes | 2/1/2022 4:13 PM |
| 327 | Making it more walkable friendly | 2/1/2022 4:09 PM |
| 328 | More art elements. More restaurants. Pedestrian lighting and safer crossing. Pervious surfaces. Shade from trees and/or fabric canopies/awnings over buildings. | 2/1/2022 3:39 PM |
| 329 | Increased security | 2/1/2022 3:34 PM |
| 330 | significant monitoring of traffic and pedestrian activity | 2/1/2022 3:12 PM |
| 331 | wider sidewalks, shade trees | 2/1/2022 2:56 PM |
| 332 | More businesses to open along 10th street | 2/1/2022 2:48 PM |
| 333 | More retail businesses | 2/1/2022 2:47 PM |
| 334 | Many of the same things that I mentioned about BOA. I believe what you have to do is find a different mix/intensity of transport options for each of three streets. For instance, Fruitville might be the primary high volume route used by cars/buses, with cycling & pedestrian movement being a less important focus. But BOA may be designed with 50% of the car load capacity as Fruitville so that the other 50% could be focused on some public event spaces (mini-parks), pedestrian-friendly pathways & improved pedestrian lighting. And 10th would be designed with the lowest car capacity and designed to be the primary west-east connector path between 41 and Orange for e-scooters, e-bikes, and at some point -- self-driving e-(golf)carts. I don't know exactly how I would "re-program" the focus of 10th street. Maybe it becomes re-positioned as an entrepreneurial zone for business starts-ups. We need to do something that | 2/1/2022 2:44 PM |

Boulevard of the Arts & 10th Street Complete Streets Survey

bridges the transition from Rosemary to Cocoanut -- right now, it is just sorta "blah" with no defined purpose or vision for one of what will be three key entrance points to the Bay/Centennial Park re-development.

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| 335 | Eliminate the gas station. | 2/1/2022 2:35 PM |
| 336 | The Salvation Army to be relocated | 2/1/2022 2:04 PM |
| 337 | Bike lanes | 2/1/2022 1:46 PM |
| 338 | Definitely needs more shade trees if people are going to walk or bike along the road | 2/1/2022 1:46 PM |
| 339 | Protection/planting strip between sidewalk and roadway and crosswalks with appropriate widths | 2/1/2022 1:11 PM |
| 340 | Parking garage serving The Bay, Rosemary District, Central Cocoanut and Westcoast Black Theatre Troupe. Streetscape improvements to encourage walking from garage to destination. | 2/1/2022 1:11 PM |
| 341 | Safety measures. As a female, I do not feel 100% safe walking 10th street by myself even during the day. | 2/1/2022 1:10 PM |
| 342 | I see people in wheelchairs having a very difficult time navigating street to sidewalk across the yellow curbs. The angle seems to be hard to use. I would stop my car to help them, but I use a walker... so I cannot help to assist them! | 2/1/2022 1:09 PM |
| 343 | More NATIVE species to help our environment, lights that only go on when peds are in that specific area. No big business, keep it local. No Starbucks ect. | 2/1/2022 1:03 PM |
| 344 | I often walk on 10th street, but I do not always feel safe. Anything that would improve the safety of residents I would appreciate | 2/1/2022 12:45 PM |
| 345 | Trees | 2/1/2022 12:36 PM |
| 346 | Bike lanes separated by median. Have seen too many fatalities of bike and peds in that area. | 2/1/2022 12:27 PM |
| 347 | More shade trees | 2/1/2022 12:23 PM |
| 348 | Less car traffic ..more pedestrian space | 2/1/2022 12:18 PM |
| 349 | Physically separated bike Lanes that have priority over cars and take more direct routes. | 2/1/2022 12:01 PM |
| 350 | I would never go to 10th St because there is a real problem with the homeless who intimidate and harass pedestrians. | 2/1/2022 12:00 PM |
| 351 | Less homeless | 2/1/2022 11:58 AM |
| 352 | more commerce, lighting | 2/1/2022 11:44 AM |
| 353 | Underground utilities Enhanced lighting More pedestrian crossings | 2/1/2022 11:43 AM |
| 354 | less gentrification | 2/1/2022 11:40 AM |
| 355 | The location of the Salvation Army is the most significant challenge to making pedestrians and bikers feel comfortable traveling via 10th street. The city needs to enforce laws that minimize vagrancy in order for any infrastructure investment on 10th street to truly benefit the city. | 2/1/2022 11:24 AM |
| 356 | See above | 2/1/2022 11:10 AM |
| 357 | Agreement with the human Services and organizations located there to prevent the indigent and homeless from laying on the street and instead, doing that within the confines of their property | 2/1/2022 11:09 AM |
| 358 | Fewer pedestrians, parking garage, times traffic lights | 2/1/2022 11:00 AM |
| 359 | Bring the Trolley into the area | 2/1/2022 10:59 AM |
| 360 | Better access for increased automobile traffic activity | 2/1/2022 10:56 AM |
| 361 | Enhanced safety | 2/1/2022 10:51 AM |
| 362 | Vagrants | 2/1/2022 10:46 AM |
| 363 | more shops, cafes, more lighting | 2/1/2022 10:44 AM |
| 364 | Street trees | 2/1/2022 10:42 AM |

Boulevard of the Arts & 10th Street Complete Streets Survey

| | | |
|-----|---|--------------------|
| 365 | narrow to two lanes...this is all that is necessary....improved pedestrian experience with canopy trees | 2/1/2022 10:41 AM |
| 366 | All of the above | 2/1/2022 10:18 AM |
| 367 | Pedestrian bridge over Tamiami Trail | 2/1/2022 10:06 AM |
| 368 | Landscaping to improve unsightly buildings. | 2/1/2022 10:05 AM |
| 369 | Clean up the empty lots, more restaurants and services. There isn't anything that draws me to 10th Street currently. Lots of homeless people there, don't feel comfortable. | 2/1/2022 9:49 AM |
| 370 | Lighting, landscaping | 2/1/2022 9:35 AM |
| 371 | Better lighting , accessible,common areas to gather | 2/1/2022 9:29 AM |
| 372 | A place for the homeless to take care of themselves | 2/1/2022 9:26 AM |
| 373 | Please rip out the round about. | 2/1/2022 9:04 AM |
| 374 | Easier, Faster, Safer crossing of Tamiami & Fruitville | 2/1/2022 9:01 AM |
| 375 | Complete streets filled with shade trees, open spaces, art, and kiosks or built in spaces for small businesses to rent for short and long periods of time. | 1/28/2022 10:17 AM |

| ID | Start time | Completion time | Email | Name | Language | Name2 | Email address | Phone number | Comments: |
|----|------------------|------------------|-----------|------|-------------------------|---------------------|------------------------------|--------------|---|
| 1 | 5/25/22 9:37:56 | 5/25/22 9:48:29 | anonymous | | English (United States) | Josh Weiner | jweiner@thelongboatgroup.com | 5037842485 | It is very exciting to have some momentum behind a city-led investment in our neighborhood. Three comments: 1. The intersection of Central Ave. and Blvd of the Arts is a phenomenal opportunity to "place-make" with a unique street treatment and/or other features. The Rosemary District has a distinct vibe which would support something truly edgy/exceptional in this location. 2. I would recommend replacing the benches located on the sidewalk at the aforementioned intersection with bike racks. The adjacent park will be a wonderful place for people to sit/rest. This would be an ideal location for people to park their bikes and/or micro-mobility means of transportation. 3. The Blvd of the Arts streetscape could also include unique place-making street lights, which could support the District's identity has different from downtown Main Street. Thank you for all that you do! |
| 2 | 5/25/22 9:50:17 | 5/25/22 10:00:31 | anonymous | | English (United States) | Maria L. Haber | maria@marialhaber.com | 202-236-6683 | I love your redesign for Blvd. of the Arts. If implemented, it will have 1-lane sharrows in each direction. Is this correct? What will the signage be? As to benches, I would put them only in bus stop structures. Blvd. of the Arts has a park and restaurants where people can sit. Curbside management zones should also be signed as to what they are for. Trees should be of a size that they will provide shade soon. I'd like to see elevation drawings of the crossing at Tamiami and Blvd. of the Arts. Last, but not least - what is the timeline for this work? thanks again for a great first step! |
| 3 | 5/25/22 10:40:20 | 5/25/22 10:51:17 | anonymous | | English (United States) | Nicola Szibbo | nszibbo@gmail.com | 941-780-4780 | Protection for bikes in the intersection is needed for safety (a raised intersection is insufficient) especially at US 41 and Lemon where 5 lanes are crossed. Protected curb medians and specific treatments for cyclists (eg elephant tracks/green paint illustrating conflict zones) should go through the intersection. See NACTO guide for protected intersections. Section showing 3' buffer needs vertical delineation for safety, need to illustrate the buffer on the concept plan. Include K-71 vertical delineators OR concrete barrier with vegetation and delineation. |
| 4 | 5/25/22 10:32:39 | 5/25/22 10:57:18 | anonymous | | English (United States) | Christine | 750 N. Tamiami Trail #313 | 248-299-9805 | Currently, benches at SCAT stops in our neighborhood are filled with homeless indigents and as a result many residents choose not to stroll past them. The indigents often shout obscenities & sometimes relieve themselves in public. Many are substance abusers or have mental health issues. They leave their litter strewn about even when trash cans are provided... I suspect that a "Complete Street" would encourage more homeless to loiter there unless this problem can be addressed. |
| 5 | 5/25/22 10:56:10 | 5/25/22 11:17:51 | anonymous | | English (United States) | Michael Riebe | riebem@gmail.com | 9413201634 | 10th St bikeway needs to be actually protected- armadillos are a cheap temporary fix for cities just learning what a complete street is. Flip the bike lane and bioswale, add the buffer to the bioswale for up to a 10' planted area. Breaks in the new median-style bioswale will allow for additional drainage to the curb (1st Ave, St Pete). Curb cuts can allow drainage into the swale itself without any underground work. If you can't figure that out, raise the bikeway to sidewalk level if debris is a concern and there aren't small enough sweepers. If folks are triggered by a sidewalk bikeway next to sidewalk, split the 10' into a 6-7' bioswale and a 3' buffer (simple landscaped or hardscaped). Yesler Way, 7th/8th Ave in Seattle. With the offset bikeway, you'd then have full bikeway protection at the intersections where the highest safety risks are. Be world class, I know you can do it! |
| 6 | 5/25/22 12:42:51 | 5/25/22 12:47:04 | anonymous | | English (United States) | Abraham Morgentaler | amorgent@yahoo.com | 6178758865 | The most important issue is addressing the traffic issue at 41/BOA. I strongly urge the city to reconsider the option for a roundabout at the intersection. It is the single best option. Will reduce congestion, allow greater throughput, reduce risk to pedestrians, and reduce the "temperature" of residents and motorists in that area. |
| 7 | 5/25/22 13:03:05 | 5/25/22 13:23:28 | anonymous | | English (United States) | Victor Simpkins | victorsimpkins@gmail.com | 917-459-8780 | The BOTA complete street and related presentation seems much more appropriate and successful than the similar treatment on 10th St. which to me has always been a roadway anomaly. The 10th St. traffic circle at Orange Ave is a great improvement to the existing difficult, unusual intersection. But insofar as the remaining 10th St treatment moving W, it seems without specific purpose, as does the existing width of the street itself. There are no existing restaurants or cafes, only open lots, parking lots and the salvation army complex, not exactly where one will want to be sitting in some future outdoor cafe. If the intention of the 10th St "complete street" concept is to motivate pedestrian-friendly development on 10th St., perhaps this will help. And don't get me wrong, the bike lanes, raised intersections and added greenery are all terrific, but I'm not sure if it's worth the cost just to make this street a bit more friendly and greener. To me, the biggest unresolved issue that could affect future planning on 10th St is the issue of parking for The Bay. As of now, when The Bay converts the massive Van Wezel parking lot to parkland, where will all those expected visitors who are driving to The Bay park? A parking structure or two on 10th St nearer to 41 might be the answer, in which case, that development might better define the street configuration on those blocks closest to 41. Finally, please make this reply box more of a paragraph box; it's impossible to write at this length in an area one line deep. Thx. |
| 8 | 5/25/22 13:44:06 | 5/25/22 14:04:13 | anonymous | | English (United States) | Janet Zipper | Janzipper@yahoo.com | 941-302-0828 | As I wasn't able to attend I spoke to several of those who did. It seems they and I expected a different agenda While it's very nice to plant foliage, benches etc. I think ...that is not nearly as important as another current & future problem which is vehicle and pedestrian safety on our local streets as well as 41. With the new buildings and the Bay project I expect there to be a lot more traffic making crossing 41 more difficult and dangerous. I believe, rather than spending time, energy & money on curb appeal ,this to be much more important . |
| 9 | 5/27/22 8:49:53 | 5/27/22 8:52:30 | anonymous | | English (United States) | Alex Theis | alextheis@aol.com | 9704716300 | It would be great if there was a way to connect the proposed bike lanes on Blvd. of the Arts to the Legacy Trail. |
| 10 | 5/27/22 9:16:24 | 5/27/22 9:21:52 | anonymous | | English (United States) | Alex Theis | alextheis@aol.com | 9704716300 | It would be great to have a way for the bike lanes to connect to the Legacy Trail. It is still dangerous to get from The Rosemary District to The Legacy Trail. |
| 11 | 5/27/22 11:22:38 | 5/27/22 11:24:44 | anonymous | | English (United States) | Kathleen Scholl | kkscholl@aol.com | | I talked with neighbors who attended the session and I was not happy with what they relayed to me. I cannot tell from the power points what exactly is proposed without a better written description. Sorry I did not attend. |

| | | | | | | | | |
|----|------------------|------------------|-----------|-------------------------|---------------------|-----------------------------|--------------|--|
| 12 | 5/27/22 16:29:23 | 5/27/22 16:44:12 | anonymous | English (United States) | Peter Blanton | blanton.peter@gmail.com | 914-646-3982 | Nice plans. But in general, I am a fan of shade trees where people walk (along sidewalks) and decorative trees (such as palm trees) where people don't walk (landscaped strips in the middle of the road). Palm trees provide minimal shade. In a perfect world, I would strive to achieve mature shade trees with Spanish moss, similar to what you find in Savannah, GA. |
| 13 | 5/28/22 7:26:26 | 5/28/22 7:30:32 | anonymous | English (United States) | Marshall Stanton | marshallstanton24@gmail.com | 612-723-7276 | Please make sure to keep the intersection of Blvd of the Arts and US 41 (Tamiami Trail) open for vehicles in all directions. There are rumors that it could be partially closed to prevent flow of cars across US 41 while driving on Blvd of the Arts. This would be very disruptive to traffic flow for those living in the area or wanting to access the new Bayfront Park. Thank you. Traffic backup on 6th street in the afternoon when parents are picking up their children from Sarasota School of the Arts. They completely obstruct the westbound lane. Would also like to recommend planting Ornamental Peanut Grass in all of the median beds going down 6th street to the bay. It is a low maintenance drought tolerant ground cover, eliminating the need to re mulch. Perennial peanut is a versatile groundcover that can be planted statewide and blooms year round with cheerful golden flowers. |
| 14 | 5/29/22 13:23:43 | 5/29/22 13:33:56 | anonymous | English (United States) | Elizabeth Hinkelman | bethwhnp@comcast.net | 941-400-4911 | Please consider this input from a full-time Resident of BLVD of the ARTS. Yes...to the raised intersection WITH the Traffic Light, enhanced crosswalks and free flow of traffic North, South, East & West to maintain access to Blvd of the Arts for Emergency Response Vehicles and our ability to enter/exit our property to access the city/airport as we do today. This was the most favorable option for Residents of the choices being considered by the City. PLEASE Keep the Eastbound dedicated Right Turn Lane (as we have it today) that is critical due to high volumes of traffic during events at the Park & Van Wezel. This lane is eliminated in the most recent plan to give more free curb space to the new ONEPark Condos where they already violated the agreement to provide that space for the Roundabout in exchange for more space 100 meters to the West! No...to the pedestrian walkway extending west after the COTB Entrance serves no purpose and creates a dead-end for pedestrian traffic that could be a safety and congestion issue. Pedestrian Traffic (Other than COTB residents) needs to be funneled North across BLVD of the ARTS into the park before that point. |
| 15 | 6/3/22 12:17:34 | 6/3/22 12:28:33 | anonymous | English (United States) | Mark Lynch | mjlal1963@gmail.com | 4047133946 | Thank you for your consideration of Full-time Residents, Mark Lynch RE : BLVD OF THE ARTS Yes : to Raised intersection and Trees. NO: To the pedestrian walkway extending west past the COTB Entrance could be a safety and congestion issue. Pedestrian Traffic (Other than COTB residents) needs to be funneled North across BLVD of the ARTS into the park before that point. |
| 16 | 6/3/22 12:55:48 | 6/3/22 13:00:41 | anonymous | English (United States) | Henry VonSpeegle | Hankvscotb@outlook.com | 732 330 2384 | 1. Thank you for keeping cross traffic and left turns from BOTA at 41 (not going forward with RCUT intersection). This was an important issue for many residents. 2. The eastbound lanes of BOTA at 41 have been reduced from 3 lanes to 2 lanes. I would request we keep the existing 3 lanes, they are needed. 3. How will you increase the sidewalk width on the south side of BOTA, west of 41? I looked at the existing sidewalk and I don't see how you can increase the sidewalk when you have BeauCiel garage, Hyatt entry steps and Hyatt garage limiting the ability to expand to one side and existing royal palms limiting the ability to expand to the other side. I would not want any existing royal palms or other existing landscaping/trees removed to increase sidewalk width. For example, in front of the Beau Ciel garage there are mature bushes which hide the garage and I would hate to have these removed, making the garage more visible. 4. When the Bay Park submitted their site plans, the plan was to make a larger sidewalk "promenade" on the north side of BOTA, to encourage pedestrians to cross to the north side of the street, rather than walk on the side of BOTA where pedestrians would have to cross several driveways (Hyatt, Beau Ciel, COTB). My understanding from the Bay Park plans is that the sidewalk on the south side of BOTA would not be as wide as the northern side. Could we keep the sidewalk on the south side the same width as it is now - also allows for existing bushes/trees to remain. 5. BOTA west of 41 has been narrowed and this may impact traffic flow around large trucks and busses that have to park in the street, in front of the Hyatt and the Beau Ciel, and particularly in the culdesac. Perhaps the south side of BOTA (at least in the culdesac area of Condo on the Bay driveway and Beau Ciel garage driveway up to the crosswalk) could be kept as is to keep the road a bit wider? 6. I hope the existing mature trees in the BOTA medians, west of 41, will remain? 7. At the west end of BOTA, in the culdesac, could you replace the ugly yellow striped pavement markings (west of the median) with something more attractive? 8. At the southwest end of BOTA, can you delete the sidewalk extension? The sidewalk currently ends at Condo on the Bay driveway and we want to discourage pedestrians from crossing the driveway. The Bay Park plan was to encourage pedestrians to cross to the north side of BOTA using the crosswalks and enter the Park from the north. 9. How does the MURT fit into the plans for BOTA west of 41? Would not want an additional crosswalk. 10. I would prefer to keep the medians on BOTA east of 41. |
| 17 | 6/3/22 12:25:04 | 6/3/22 13:07:58 | anonymous | English (United States) | Suzanne Lynch | suzanne.lynch@comcast.net | | |

News List

Design for 2 complete streets to be unveiled at open houses

Post Date: 05/13/2022 8:30 AM

Contact: Jason Bartolone, Communications Specialist 941-928-3988; jason.bartolone@sarasotaFL.gov



Sarasota, FL: The City of Sarasota will unveil draft concepts for the construction of “complete streets” on 10th Street and Boulevard of the Arts at two upcoming open houses.

A [community survey](#) about the project conducted in February received more than 1,000 responses. The public’s vision was then incorporated into conceptual designs, which will be presented in two sessions on Monday, May 23, at the SRQ Media Studio in the City Hall Annex, 1565 First Street.

- 3-4:30 p.m.: Boulevard of the Arts – west of U.S. 41
- Starting at 5:30 p.m.: Boulevard of the Arts and 10th Street – east of U.S. 41

Attendees will have an opportunity to learn more about the designs and share feedback that will help transform these corridors and the Boulevard of the Arts-U.S. 41 intersection.

A complete street is an age-friendly design that promotes safe travel to and from a destination, whether walking, biking, riding transit or driving a vehicle. It’s also accessible to all who utilize it and provides a sense of place for the community. Each complete street is unique to the community and can include different features like enhanced sidewalks, street lighting, benches, street trees and landscaping, public art and more.

For more information, contact Transportation Planner [Corinne Arriaga](#) at 941-263-6362.

-30-

About the City of Sarasota: Distinguished by U.S. News and World Report as one of the top 10 Best Places to Live in the U.S., the City of Sarasota is a diverse and inclusive community located on Florida’s Suncoast with 56,000 year-round residents, several internationally recognized cultural arts venues, stunning sunsets along Lido Beach and Major League spring training baseball with the Baltimore Orioles. Learn more about us at www.SarasotaFL.Gov

[Return to full list >>](#)

Sign-in and Comments
 Public Meeting
 5/23/2022 @ 3:00PM

53
 57*

**Boulevard of the Arts/10th St Complete Streets
 Public Meeting**

Monday, May 23, 2022
 3:00 – 4:30 PM

City Hall Annex – SRQ Media Studio

PLEASE PRINT CLEARLY

| NAME | ADDRESS | EMAIL ADDRESS | PHONE # |
|-----------------------|---------------------------|---------------------------------|--------------|
| ✓ Elizabeth Hinkelman | 888 Blvd of the Arts | bethwhnp@comcast.net | 941-400-4911 |
| ✓ GERARD RADFORD | 888 BLVD OF THE ARTS #103 | GRAD FORD_555@MSN.COM | 941-302-2930 |
| ✓ Jen Ahearn-Koch | on file | | |
| ✓ Maria L. Haber | 750 N. Tamiami Trail #111 | maria@marialhaber.com | |
| ✓ JAMES FANKAM | 1777 MAIN ST., SUITE 200 | james.fankam@kaiserfamilyca.com | 941 379 7620 |
| ✓ Mind Koff | | | |
| ✓ CHRISTIAN HERSHMAN | 1420 Blvd ARTS | | 941-962-5147 |
| ✓ Lynn Morris | 1421 Blvd ARTS | | |
| ✓ Cheryl Mendelson | 2437 Floyd St | | |
| ✓ Laura Hennessey | | lhennessey@vuwfoundation.org | |

10

Boulevard of the Arts/10th St Complete Streets Public Meeting

Monday, May 23, 2022
3:00 – 4:30 PM

City Hall Annex – SRQ Media Studio

PLEASE PRINT CLEARLY

| NAME | ADDRESS | EMAIL ADDRESS | PHONE # |
|---------------------|--|--------------------------------|--------------|
| ✓ Alvimarre Corales | Sarasota, FL | alvimariz@gmail.com | 941-246-1891 |
| ✓ Corinne Arriaga | Bradenton, FL | corinne.arriaga@sarasotafl.gov | — |
| ✓ Steven/Marceline | LANDRY SARASOTA, FL | STEVEN.LANDRY@COMCAST.NET | 248-613-8740 |
| ✓ DONALD GOLDSMITH | 540 N. TAMIAHI TRAIL SARASOTA FL | donald.goldsmith@hklaw.com | 917-362-1250 |
| ✓ Erin Swider | 900 914 Southern Pine Ln Sarasota | erin.swider@hinley-horn.com | 813 482 4036 |
| ✓ D. Ohrenstein | 1565 1st Street | (on file) | |
| ✓ R. LISI | 540 N TAMIAHI TRAIL | LIRICK@AFL.COM | 4014736170 |
| ✓ T. Reis | 990 Blvd. of the Arts | TeieLreis@gmail.com | 941-373-1203 |
| ✓ ANAND PALLEGAR | SARASOTA FL | ANAND@DREAMLARGE.ORG | 941.444.9988 |
| ✓ Horace Norrell | 888 Blvd of the Arts | hnorrell@comcast.net | 941-400-8216 |

Boulevard of the Arts/10th St Complete Streets Public Meeting

Monday, May 23, 2022
3:00 – 4:30 PM

City Hall Annex – SRQ Media Studio

PLEASE PRINT CLEARLY

| NAME | ADDRESS | EMAIL ADDRESS | PHONE # |
|--|--|-------------------------------|--------------|
| ✓ Cory Holding | 1500 N Lockwood Ridge Rd Sarasota, FL 34207 | corya.holding@sarasotafl.gov | 941-321-1234 |
| ✓ Mary Hale | 750 N. Tamiami Trail #203 | emaryhale@comcast.net | NA |
| ✓ ANK & CAROL <small>VONSPEEGLE</small> | 988 BLVD OF ARTS 215 | cv54454@me.com | — |
| ✓ DONNA & JOHN MOFFITT | 988 BLVD OF ARTS #912 | donnamoffsitt@hotmail.com | |
| ✓ Larry Grossman | Brandon | PDCLKG@hotmail.com | 941-225-5791 |
| ✓ Deb Huffman | 888 Blvd Arts 304 | debbie.huffman@fontamoves.com | 578-419-1160 |
| ✓ PAUL ROSENSTEIN | 990 Blvd OF THE ARTS | psr@me.com | |
| ✓ Diane Desmery | 988 Blvd. of Arts | dmdesmery@mac.com | — |
| ✓ Ron & Gayle Timmerman | 888 Blvd. of the Arts #1006 | gayletimmerman@hotmail.com | 503-805-7027 |
| ✓ Dirk Plessner | 990 Blvd. of the Arts # 404 | dpplessner@gmail.com | 941-346-6950 |

Boulevard of the Arts/10th St Complete Streets Public Meeting

Monday, May 23, 2022
3:00 – 4:30 PM

City Hall Annex – SRQ Media Studio

PLEASE PRINT CLEARLY

| NAME | ADDRESS | EMAIL ADDRESS | PHONE # |
|--------------------|----------------------|-----------------------------|--------------|
| ✓ Rachel RAPPOPORT | 990 Blvd of the Arts | RachelRAPPOPORT31@GMAIL.COM | 404 2425595 |
| ✓ Joanne Foley | 671 Coconut Ave | | |
| ✓ Emmalee Legler | 2221 8th St. 34237 | emmalee@jonfswiftinc.com | 941 951-6100 |
| ✓ Monica Lee | . | | |
| ✓ Suzanne Lynch | 888 Blvd of the Arts | Suzanne.Lynch@comcast.net | |
| ✓ Dominic Pannol | 1064 N. Tamiami! | — | — |
| ✓ BILL DIEHL | 888 BLVD OF THE ARTS | BILLJACKIEDIEHLE@GMAIL.COM | |
| ✓ VICTOR SIMPKINS | 800 Suncreech Dr. | victorsimpkins@gmail.com | |
| ✓ Glenn Shackman | 990 Blvd of the Arts | bshackman@aol.com | |
| ✓ Nancy Shapiro | 990 Blvd of the Arts | Xtremmen@aol.com | |

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Boulevard of the Arts/10th St Complete Streets Public Meeting

Monday, May 23, 2022
3:00 – 4:30 PM

City Hall Annex – SRQ Media Studio

PLEASE PRINT CLEARLY

| NAME | ADDRESS | EMAIL ADDRESS | PHONE # |
|-------------------------------|------------------------------------|---------------------------------------|------------------|
| ✓ Bill Waddill | 700 Coconut Ave, #321 | Bill.Waddill@bayparkconservancy.org | 941-266-7717 |
| ✓ Patrick Gannon | 888 Blvd of the Arts 1508 | pgannon@gmail.com | 941-357-6224 |
| ✓ Tim Schoch | 988 Blvd " " 212 | SRQTIM@GMAIL.COM | 513-509-1234 |
| ✓ Leon Thibout | 930 N. Tamiami Trail | Lthibout@gmail.com | 941-266-0291 |
| ✓ Kindra Marinelli | 3374 Hadfield Greene | Candika9@yolo.com | 941-952-3650 |
| ✓ Wm Marinelli | " " | " " | " |
| ✓ TIM BURGESS | | TBURGESS@TAMPABAYS.COM | 941-376-1111 |
| ✓ CARL Shoffstall | 129 Tyler Dr Sarasota FL 34236 | CARL a. Florida playstructures.com | 813-967- 2687 |
| ✓ Pam Kemper | 888 Blvd of the Arts #102 | PamelaKemper@gmail.com | 617-817-2564 |
| → | 2377 Burton Lane Sarasota 34239 | DeeAnnaDowdle@gmail.com | 941-350-3571 |

Boulevard of the Arts/10th St Complete Streets Public Meeting

Monday, May 23, 2022
3:00 – 4:30 PM

City Hall Annex – SRQ Media Studio

PLEASE PRINT CLEARLY

| NAME | ADDRESS | EMAIL ADDRESS | PHONE # |
|---------------|----------------|-----------------------------------|--------------|
| MARK KAUFFMAN | | MARK KAUFFMAN 2015- @PRATT.COM | 352-6314 |
| Josh Weiner | 1623 North Dr. | jweiner@thelongboat.org | 503-781-2785 |
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**Boulevard of the Arts/10th St Complete Streets
Public Meeting**

**Monday, May 23, 2022
3:00 – 4:30 PM**

City Hall Annex – SRQ Media Studio

****PLEASE PRINT CLEARLY****

| NAME | ADDRESS | EMAIL ADDRESS | PHONE # |
|---------------|-----------------------------------|-------------------------|--------------|
| Shannon Roddy | 1415 2 nd St. Sarasota | srodgy@vwfoundation.org | 815-224-8436 |
| | | | |
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Boulevard of the Arts/10th St Complete Streets

Public Meeting

May 23, 2022

COMMENT CARD

Name:

DONALD A GOLDSMITH

Email or Phone #:

donald.goldsmith@hklaw.com

Comments:

The failure to address pedestrian crossing of 41 by reducing
the lanes from 5 to 4 & installing a median from Fruitville
through BOTA is a major mistake. Even with a raised platform
pedestrians still need to traverse 5 lanes in a limited time
& can get caught in the middle when lights change. It
happens now & nothing in the plan addresses it. The current
left turn lane should be eliminated & a pedestrian median
should be added.

Also shrubbery should be added to the east side of
Tamiami Trail to protect pedestrians from northbound traffic.
Most people walking to the Bay Park will be on the east side of
Tamiami Trail when coming from downtown.

(over)

In addition to the raised intersection, the green light should be replaced with a flashing yellow (retain the red of course). This will reinforce the raised intersection & avoid accidents by speeding cars who otherwise would fail to slow down. The combination of a raised intersection & flashing yellow would achieve much of the benefit of a roundabout which is no longer feasible.

State permission should be requested for ~~to~~ the above changes which will enhance safety, avoid anxiety in crossing Tammam Trail & reduce the current excessive decibel level of cars speeding over the unfriendly bricks

Happy to meet to discuss.

Donald Goldsmith

(917) 362-1250

P.S. Planners should spend time during rush hours & at night to observe the challenges faced for pedestrian safety.



Boulevard of the Arts/10th St Complete Streets

Public Meeting

May 23, 2022

COMMENT CARD

Name: _____ *M Hale*

Email or Phone #: _____

Comments: *I like — /*
can only help.



Boulevard of the Arts/10th St Complete Streets

Public Meeting

May 23, 2022

COMMENT CARD

Name: _____

Email or Phone #: _____

debbie.huffman@floridamoves.com

Comments:

Median strips on BOTA west of 4/1 need new trees,
Existing ones are unattractive = old

Keep people from walking in front of 888/988⁷
driveway - It is a blind spot for exiting vehicles

Don't install sidewalk - so people walk down
North side of street



Boulevard of the Arts/10th St Complete Streets

Public Meeting

May 23, 2022

COMMENT CARD

Name:

Steve LANDRY

Email or Phone #:

STEVEN.LANDRY@COMCAST.NET
248-613-8740

Comments:

CORNER of 41 NORTH AND BOTA

SHOULD Be A SMALL ROUNDABOUT !!

TRAFFIC LIGHTS = DISASTER / SAFETY ISSUES
HURRICANE ESCAPE ROUTE

4 ROUNDABOUTS + A TRAFFIC LIGHT

IS UNSAFE. Please RECONSIDER

A SMALL R/A.



Boulevard of the Arts/10th St Complete Streets

Public Meeting

May 23, 2022

COMMENT CARD

Name: Suzanne Lynch

Email or Phone #: _____

Comments:

Very happy that BOTA/41 will not be RCU T intersection

Western end of BOTA: (undecided about eliminating parking) good that parking was reduced

but road seems narrow. maneuvering thru
caldesac when trucks are parked may be
difficult.

COTB, Beau Ciel + Hyatt contractors/vendors use
the on street parking on BOTA. Where will they
park? may have equipment to haul.

will curbside management zones block traffic flow?
How will these zones be managed + who will manage?

would like to see where sidewalk connecting
south side BOTA to park path will go

Boulevard of the Arts/10th St Complete Streets
Public Meeting
May 23, 2022

COMMENT CARD

ENFORCEMENT OF
PARKING & PROP-OFF -
WHAT IS THE PLAN SO
RESIDENTS CAN SAFELY
TRANS GRESS THE
STREET? ENFORCEMENT
IS KEY

Optional:

Name: DONNA MOFFITT Phone: _____

Email: _____

Boulevard of the Arts/10th St Complete Streets
Public Meeting
May 23, 2022

COMMENT CARD

I WOULD LIKE TO SEE MORE
ART ON BLVD OF THE ARTS
SCULPTURES, OVERHEAD ART AND
SOMETHING NICE IN THE
ROAD AND SIDEWALKS

Optional:

Name: Bill Diehl Phone: _____

Email: _____

Boulevard of the Arts/10th St Complete Streets
Public Meeting
May 23, 2022

COMMENT CARD

would like to see the ability to turn left
(driving w) into Florida Ave.

Optional:

Name: Mindy Kauffman Phone: 941-413-0481

Email: mindy@redcommercialgroup.com

Boulevard of the Arts/10th St Complete Streets
Public Meeting
May 23, 2022

COMMENT CARD

Left turn out of Publix on
10th Street please!

Optional:

Name: Emmalee Legler Phone: _____

Email: emmalee@jonswiftinc.com

Boulevard of the Arts/10th St Complete Streets
Public Meeting
May 23, 2022

COMMENT CARD

Do not use bricks in intersection.
They triple the decibel level of
cars driving over them.

Optional:

Name: DONALD A GOLDSMITH Phone: 917-362-1250

Email: donald.goldsmith@hklaw.com

Boulevard of the Arts/10th St Complete Streets
Public Meeting
May 23, 2022

COMMENT CARD

THE BAY IS BUILDING A
PLAYGROUND DIRECTLY ACROSS
FROM AN ACTIVE DRIVEWAY AND
BUILDING A SIDEWALK DIRECTLY
ACROSS THE DRIVEWAY ENCOURAGING
CHILDREN TO RUN AHEAD OF
Optional: PARENTS INTO THE EXIT TRAFFIC

Name: DONNA MOFFITT Phone: 941-702-8695

Email: danneperry.moffitt@hotmail.com

Boulevard of the Arts/10th St Complete Streets
Public Meeting
May 23, 2022

COMMENT CARD

Reduce Radius at Intersections w/ Bulbouts
or Squaring off Intersections. TF
OR, Reduce length of Prod. Walk
- eliminate hot spots from exposure of speeding
pedestrians w/ tight curb approach to intersection
Tight angles to take left turn (and)

Optional:

Name: LARRY GROSSMAN Phone: 941-225-5741

Email: PGROSS@HOTMAIL.COM

Slow Traffic may report Halts as
noise coming later for pedestrians
minimize unneeded sound (not
needed by the vehicle)

Re-route SRT Piece at Transion
AND POTA T. moving ~~another~~ east of
Transion

IT WILL HAVE TO BE CORRECTED IN THE NEAR FUTURE

VERY SHORTSIGHTED

COMMENT CARD

CLOSING FLORIDA AVENUE FROM CAR GOING W + ABILITY TO TURN L (EAST) (SOUTH)

+

CARS GOING E + ABILITY TO TURN L (NORTH)

IS SHORTSIGHTED - THAT CORNER 10TH + FLA WILL BE ACTIVATED BY DOQ BAR + 2 CONDO/APT PROJECTS, CARS WILL MAKE ILLEGAL U-TURNS

Optional: AFTER THE ISLANDS END

Name: MARK KAUFFMAN Phone: 941-350-6314

Email: MARK.KAUFFMAN.2015@GMAIL.COM

R/D

COMMENT CARD

BRAZIL

IPANEMA BEACH COPT CABANA BEACH

Decorative Sidewalks

Optional:

Name: Phone:

Email:

COMMENT CARD

Don't add any more Palms on Blvd of the Arts. We need more shade trees between Hyatt + US 41!

Optional:

Name: Phone:

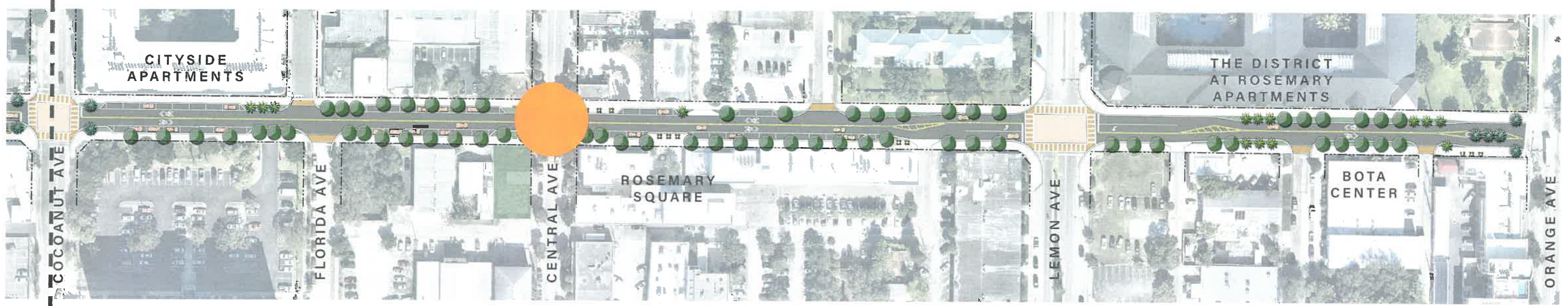
Email:

North orange - Bike path to LTF



MATCHLINE - SEE IMAGE BELOW

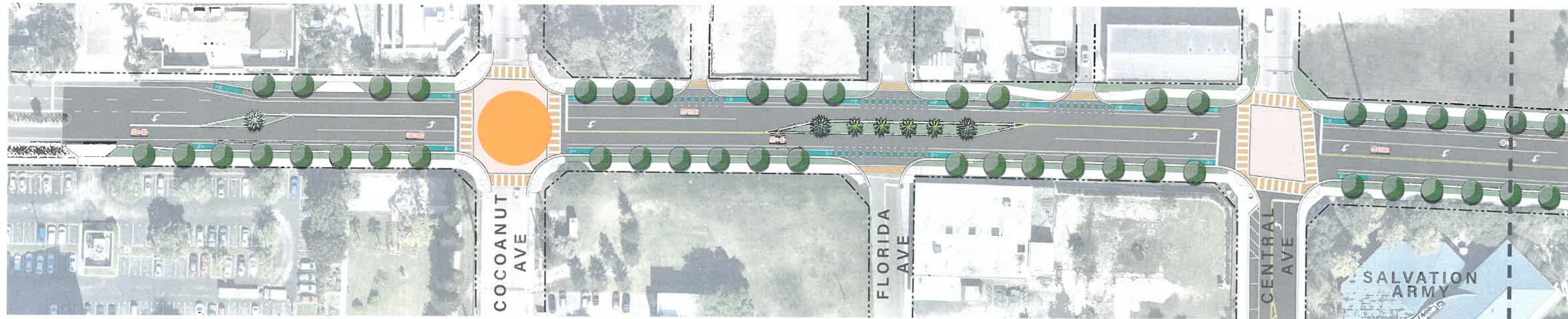
MATCHLINE - SEE IMAGE ABOVE



BOULEVARD OF THE ARTS



- PLACE A GREEN DOT NEXT TO WHERE YOU WOULD LIKE TO SEE MORE TREES
- PLACE A YELLOW DOT NEXT TO WHERE YOU WOULD LIKE TO SEE MORE SITE FURNISHINGS
- PLACE AN ORANGE DOT NEXT TO WHERE YOU WOULD LIKE TO SEE SPECIAL FEATURES



MATCHLINE - SEE IMAGE BELOW

MATCHLINE - SEE IMAGE ABOVE



10TH STREET

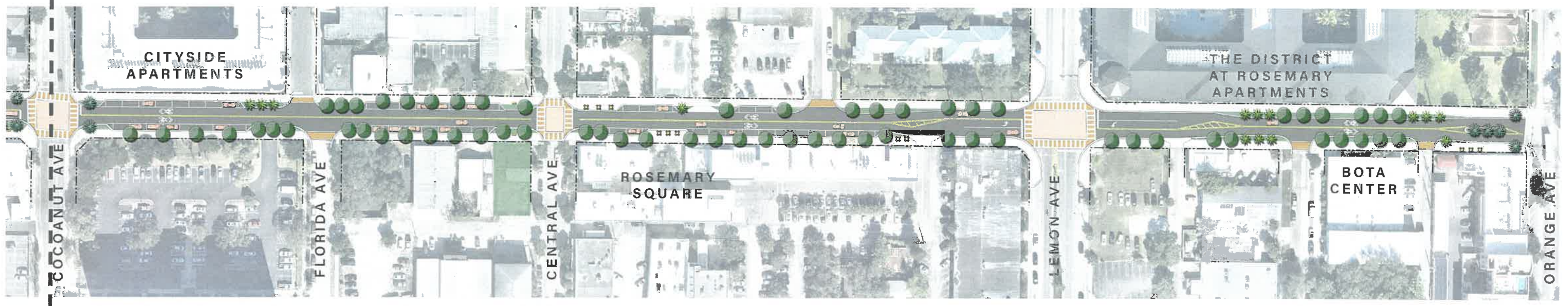


- PLACE A GREEN DOT NEXT TO WHERE YOU WOULD LIKE TO SEE MORE TREES
- PLACE A YELLOW DOT NEXT TO WHERE YOU WOULD LIKE TO SEE MORE SITE FURNISHINGS
- PLACE AN ORANGE DOT NEXT TO WHERE YOU WOULD LIKE TO SEE SPECIAL FEATURES



MATCHLINE - SEE IMAGE BELOW

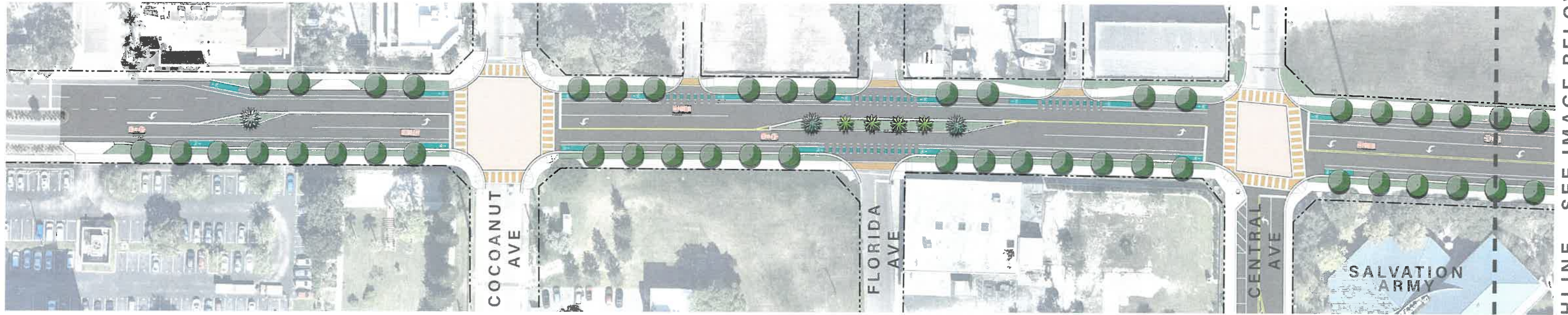
MATCHLINE - SEE IMAGE ABOVE



BOULEVARD OF THE ARTS



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MATCHLINE - SEE IMAGE BELOW

MATCHLINE - SEE IMAGE ABOVE



10TH STREET



PLACE A GREEN DOT NEXT TO WHERE YOU WOULD LIKE TO SEE MORE TREES

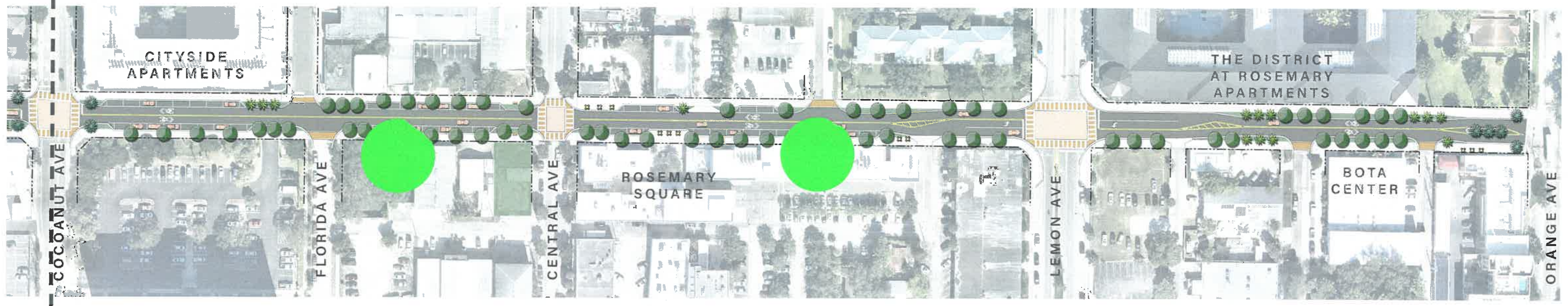
PLACE A YELLOW DOT NEXT TO WHERE YOU WOULD LIKE TO SEE MORE SITE FURNISHINGS

PLACE AN ORANGE DOT NEXT TO WHERE YOU WOULD LIKE TO SEE SPECIAL FEATURES



MATCHLINE - SEE IMAGE BELOW

MATCHLINE - SEE IMAGE ABOVE



BOULEVARD OF THE ARTS



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8' Sidewalk on 10th

MATCHLINE - SEE IMAGE ABOVE



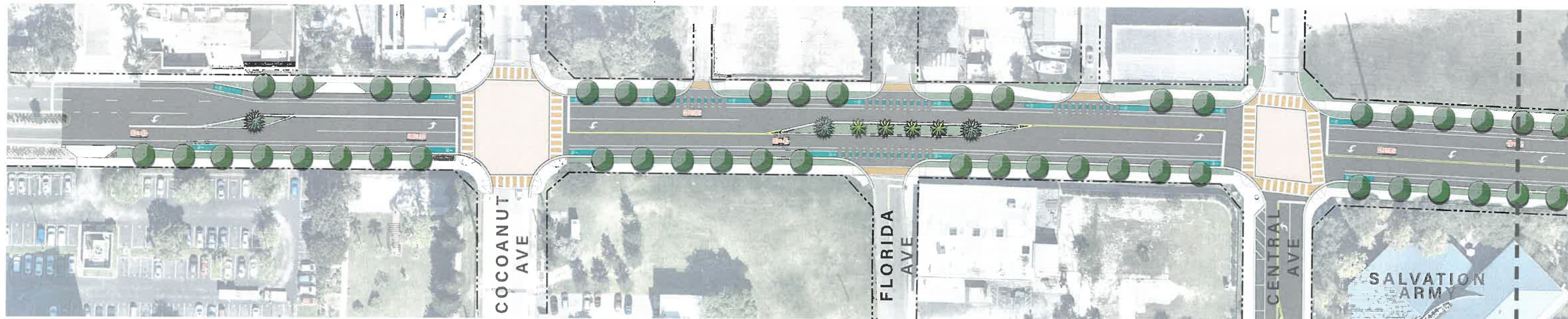
10TH STREET



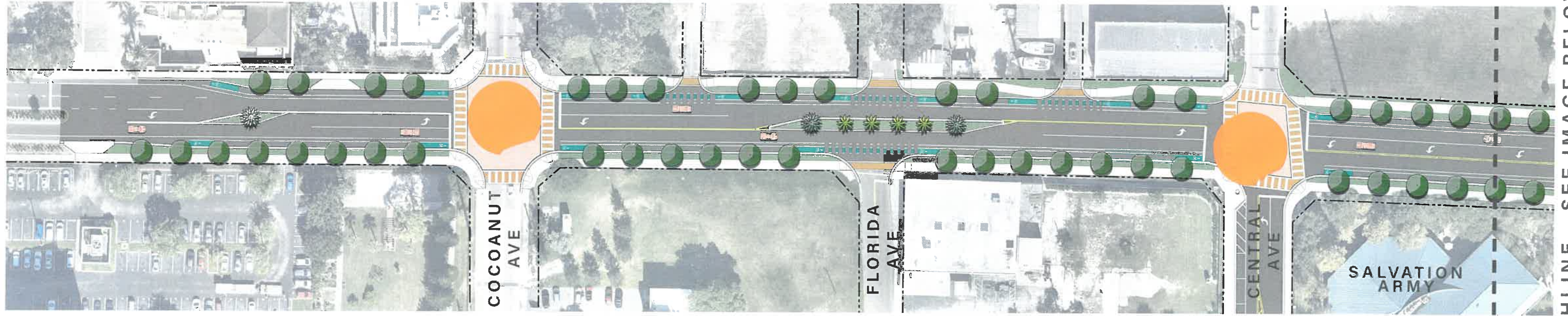
PLACE A GREEN DOT NEXT TO WHERE YOU WOULD LIKE TO SEE MORE TREES

PLACE A YELLOW DOT NEXT TO WHERE YOU WOULD LIKE TO SEE MORE SITE FURNISHINGS

PLACE AN ORANGE DOT NEXT TO WHERE YOU WOULD LIKE TO SEE SPECIAL FEATURES

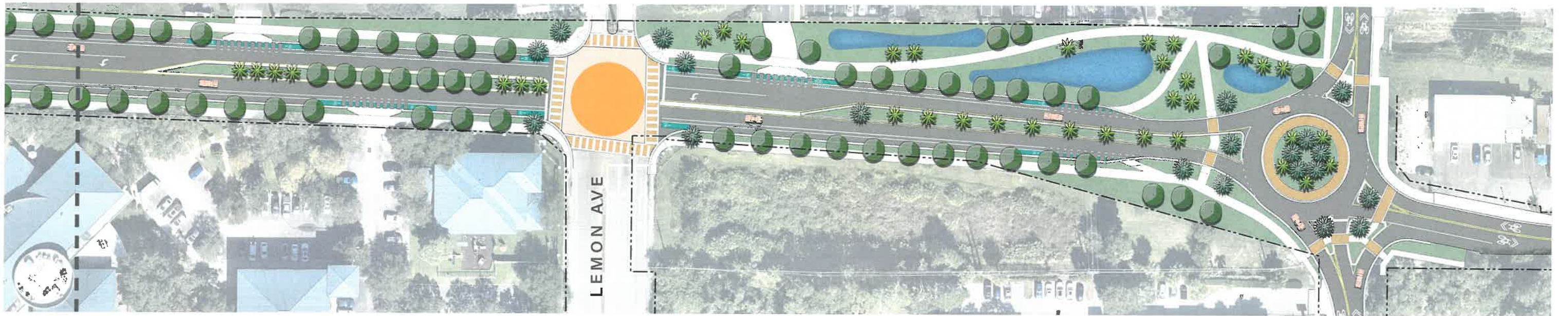


MATCHLINE - SEE IMAGE BELOW



MATCHLINE - SEE IMAGE BELOW

MATCHLINE - SEE IMAGE ABOVE



10TH STREET



PLACE A GREEN DOT NEXT TO WHERE YOU WOULD LIKE TO SEE MORE TREES

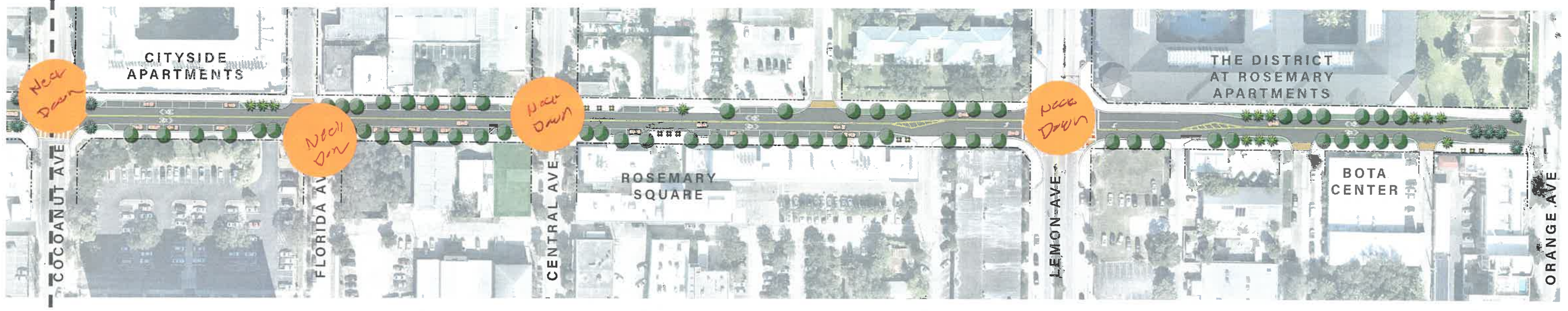
PLACE A YELLOW DOT NEXT TO WHERE YOU WOULD LIKE TO SEE MORE SITE FURNISHINGS

PLACE AN ORANGE DOT NEXT TO WHERE YOU WOULD LIKE TO SEE SPECIAL FEATURES



MATCHLINE - SEE IMAGE BELOW

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BOULEVARD OF THE ARTS

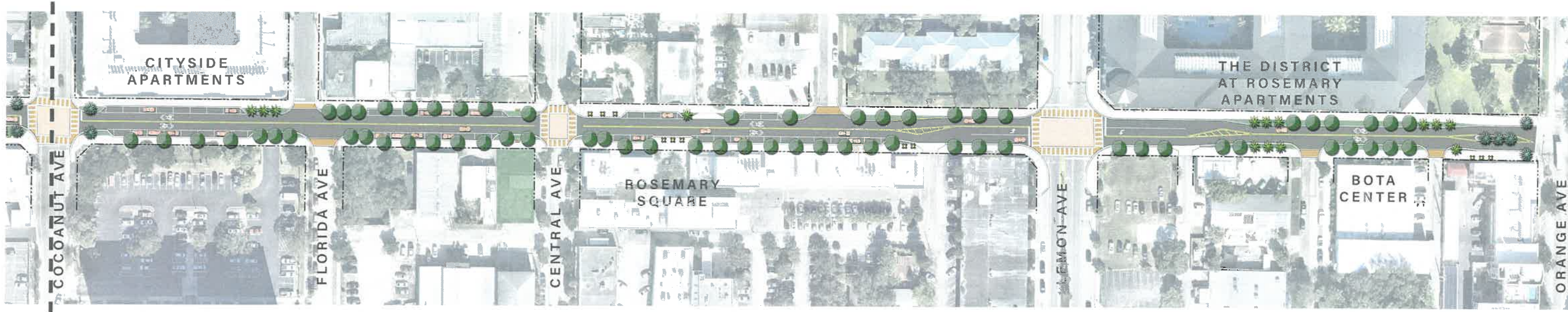


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MATCHLINE - SEE IMAGE BELOW

MATCHLINE - SEE IMAGE ABOVE



BOULEVARD OF THE ARTS



PLACE A GREEN DOT NEXT TO WHERE YOU WOULD LIKE TO SEE MORE TREES

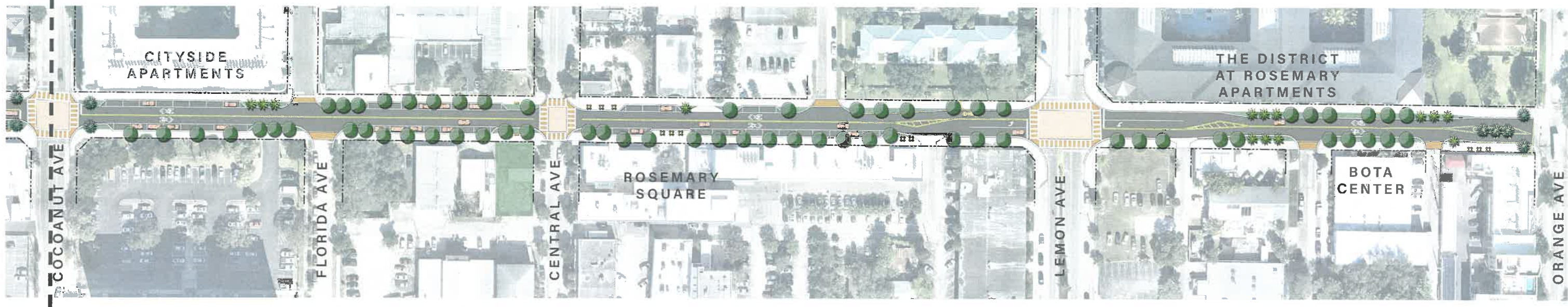
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MATCHLINE - SEE IMAGE BELOW

MATCHLINE - SEE IMAGE ABOVE



BOULEVARD OF THE ARTS



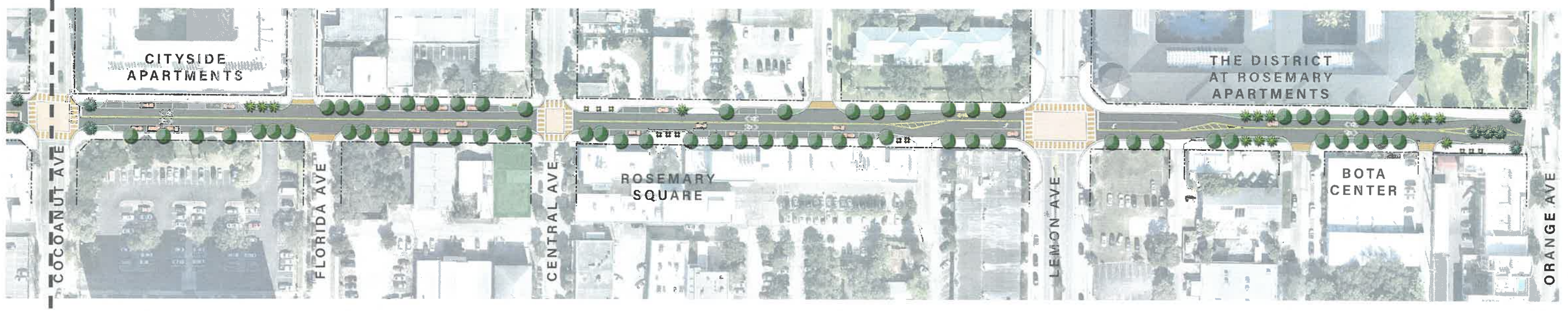
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Should Be A Small Roundabout (Smaller Than Fruitville)



MATCHLINE - SEE IMAGE BELOW

MATCHLINE - SEE IMAGE ABOVE



BOULEVARD OF THE ARTS

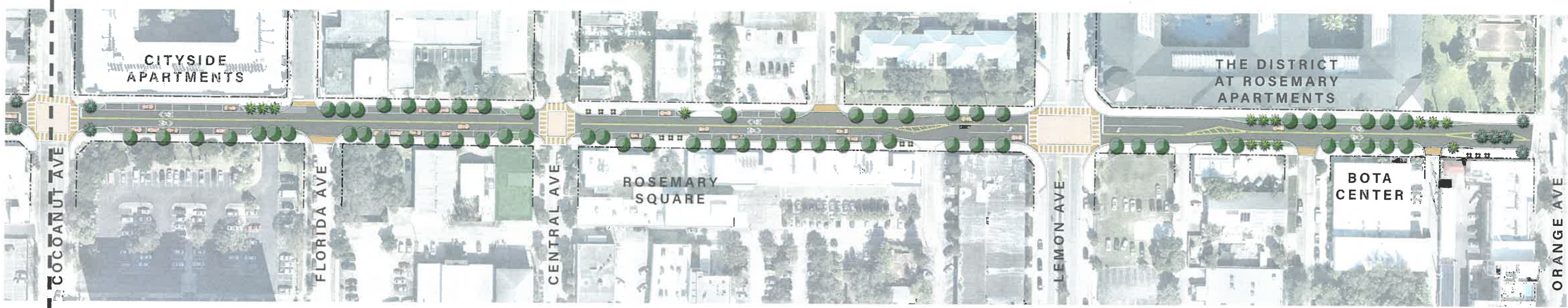


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MATCHLINE - SEE IMAGE BELOW

MATCHLINE - SEE IMAGE ABOVE



BOULEVARD OF THE ARTS



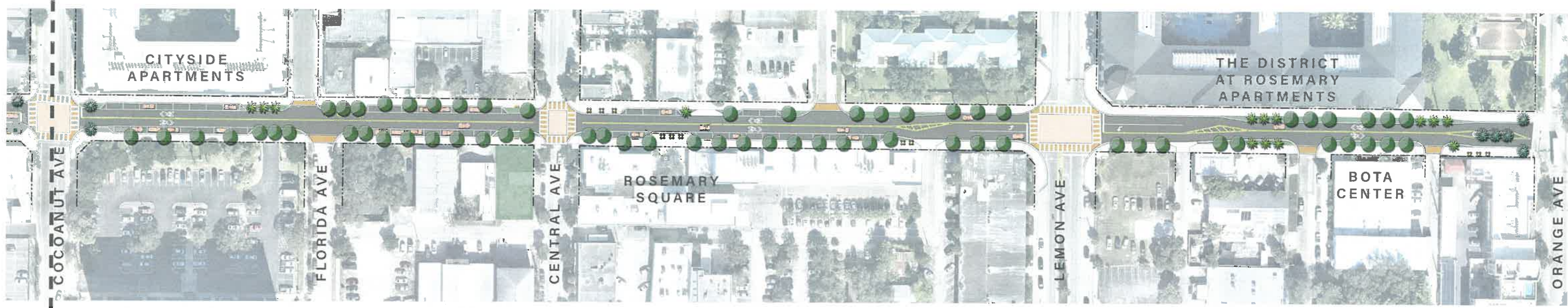
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UNSAFE WALKWAY



MATCHLINE - SEE IMAGE BELOW

MATCHLINE - SEE IMAGE ABOVE

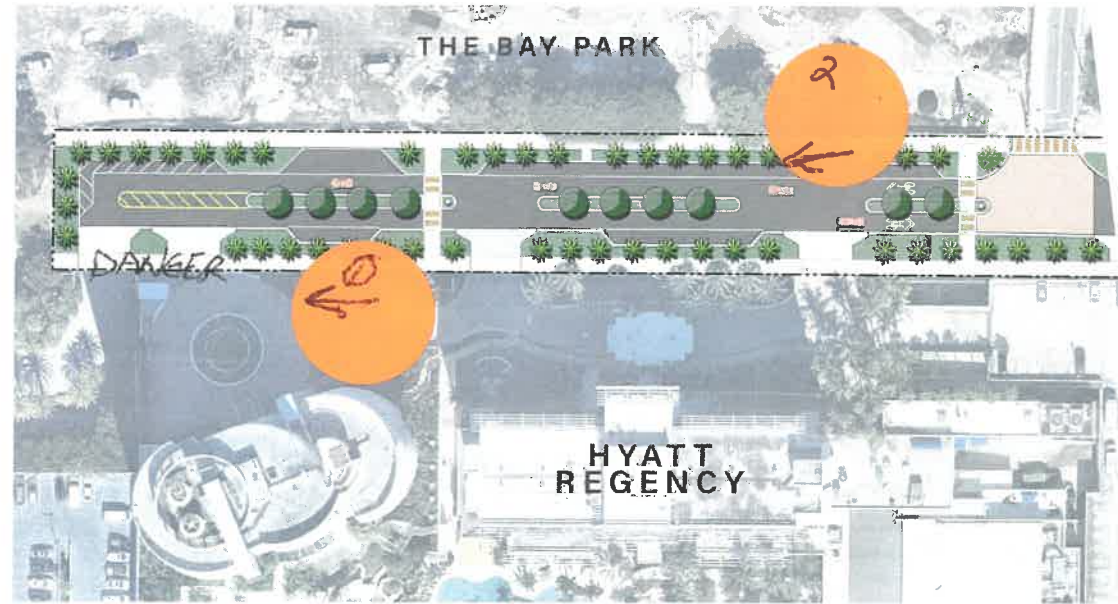


BOULEVARD OF THE ARTS



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- PLACE AN ORANGE DOT NEXT TO WHERE YOU WOULD LIKE TO SEE SPECIAL FEATURES

MATCHLINE - SEE IMAGE ABOVE



Boulevard of the Arts / 10th St Complete Streets
 Public Meeting
 May 23, 2022
COMMENT CARD

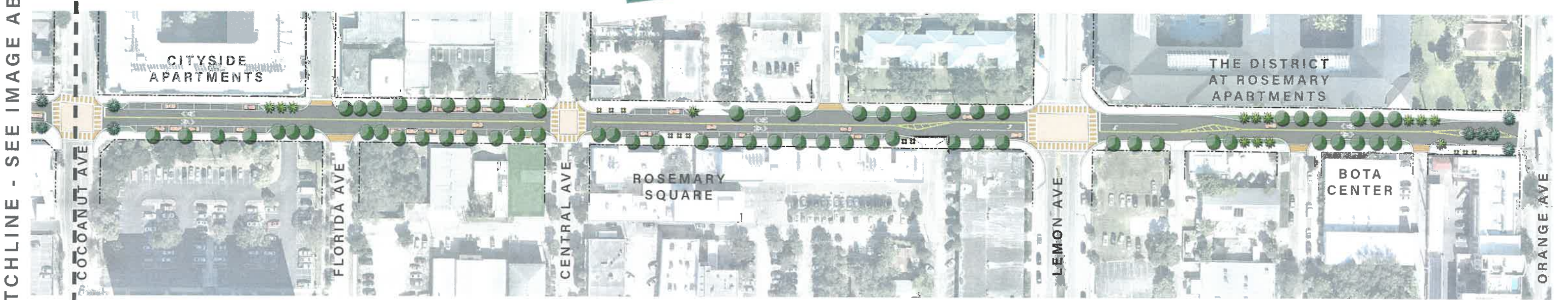
① SIDEWALK SHOULD BE CLOSED AT CROSSWALK (AREA IS DANGEROUS IN FRONT OF COTB)

② IS THERE A PLAN TO MANAGE NO PARKING OTHER THAN SIGNS

Optional: _____ Phone: _____
 Name: _____
 Email: _____



MATCHLINE - SEE IMAGE BELOW



BOULEVARD OF THE ARTS



PLACE A GREEN DOT NEXT TO WHERE YOU WOULD LIKE TO SEE MORE TREES

PLACE A YELLOW DOT NEXT TO WHERE YOU WOULD LIKE TO SEE MORE SITE FURNISHINGS

PLACE AN ORANGE DOT NEXT TO WHERE YOU WOULD LIKE TO SEE SPECIAL FEATURES

NO SIDEWALK HERE ← DANGEROUS!!!

Boulevard of the Arts/10th St Complete Streets
Public Meeting
May 23, 2022

COMMENT CARD

THIS SIDEWALK
NEEDS TO GO
DANGEROUS!!!

Optional:

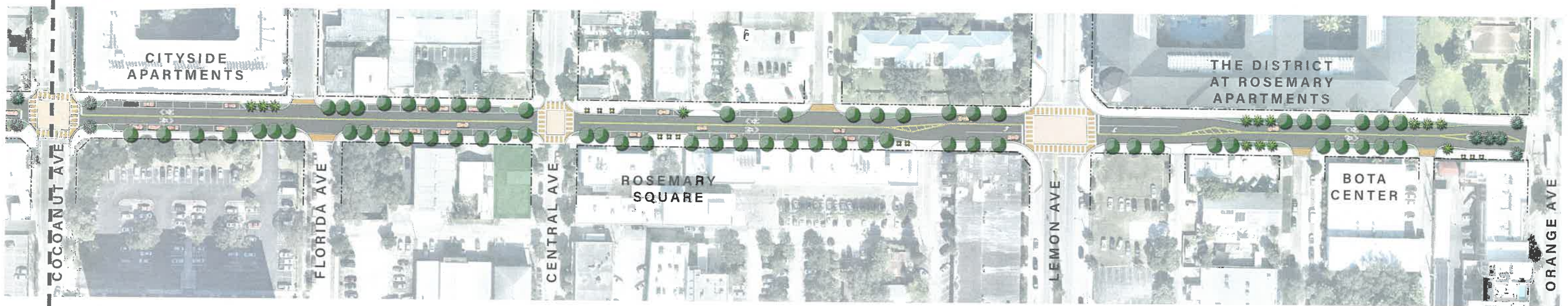
Name: _____ Phone: _____

Email: _____



MATCHLINE - SEE IMAGE BELOW

MATCHLINE - SEE IMAGE ABOVE



BOULEVARD OF THE ARTS



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Sign-in and Comments
Public Meeting
5/23/2022 @ 5:30PM

33

**Boulevard of the Arts/10th St Complete Streets
Public Meeting**

Monday, May 23, 2022
5:30 PM

City Hall Annex – SRQ Media Studio

PLEASE PRINT CLEARLY

| NAME | ADDRESS | EMAIL ADDRESS | PHONE # |
|------------------------|---------------------------|--------------------------|-----------------------|
| ✓ Frederick T. Fout | 801 N. Orange | fred.fout@oursma.org | x 297 941-926-1700 |
| ✓ Lindatt Holland | 617 Gillespie Ave | lholland4@comcast.net | 941 313-0201 |
| ✓ Merry Sanders | 445 N. Orange Av. | merrysanders@comcast.net | |
| ✓ Charlie Nagelschmitt | 1004 W TAMiami TRL | nagelschmitt@gmail.com | 802-999-1858 |
| ✓ Kristien Van Hecke | 2023 10 th St | kristienvan@yahoo.com | 941 840 2728 |
| ✓ Rick O'Connell | 710 N. LEMON AVE | ROLANDROLL@GMAIL.COM | 941 302-8171 |
| ✓ JON ROBINSON | 1158 TAHITI PKWY | JMROBINSON@HOTMAIL.COM | 941-780-3909 |
| ✓ DAVID LOUGH | 711 COCOANUT | davidlough1@gmail.com | 760 497-9230 |
| ✓ Jim Braselton | 1350 5 th -304 | jabraselton@gmail.com | 703-932-2420 |
| ✓ John Barney | 1285 Blvd of the Arts | JBARNEY@ME.COM | 484-332-1467 |

Boulevard of the Arts/10th St Complete Streets Public Meeting

Monday, May 23, 2022
5:30 PM

City Hall Annex – SRQ Media Studio

PLEASE PRINT CLEARLY

| NAME | ADDRESS | EMAIL ADDRESS | PHONE # |
|--------------------|---|------------------------|---------|
| ✓ Nathan Perry | 610 Cohen Way | NRP3@ool.com | _____ |
| ✓ JOHN GARTON | 800 N TAMiami TrL | JGARTON@AEM.ORG | |
| ✓ Chris Baer | 750 N. TAMiami TrL | Baerc3436@gmail.com | _____ |
| ✓ Richard Thomas | 489 Acacia Dr. | rthomzs247@comcast.net | _____ |
| ✓ LOIS ALTMAN | 750 N. TAMiami TrL UNIT 1409 | ALTMANLOIS@GMAIL.COM | |
| ✓ ANAND PALLEGAR | SARASOTA, FL | ANAND@DREAMLARGE.ORG | |
| ✓ CHRIS CANTALIONE | 443 WINGFIELD DRIVE SARASOTA 34232 | CIANFACH112@GMAIL.COM | |
| ✓ BOB ROTHBARD | 700 COCONUT AVE #253 SARASOTA FL 34236 | ROTHBARD1@GMAIL.COM | |
| | | | |
| | | | |

Boulevard of the Arts/10th St Complete Streets Public Meeting

Monday, May 23, 2022
5:30 PM

City Hall Annex – SRQ Media Studio

PLEASE PRINT CLEARLY

| NAME | ADDRESS | EMAIL ADDRESS | PHONE # |
|--------------------|---------------------------|-----------------------------|--------------|
| ✓ Donna Gutierrez | 1282 May Ln | DGutierrez397@gmail | 609-489-7399 |
| ✓ NORMAN FREEMAN | 930 N TAMiami Trl #305 | normanfreeman19@comcast.net | 941-374-6542 |
| ✓ Gabriella Folino | 3225 Beneva Road unit 102 | folino.gabriella@gmail.com | 415 987 9836 |
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Boulevard of the Arts/10th St Complete Streets Public Meeting

Monday, May 23, 2022
5:30 PM

City Hall Annex – SRQ Media Studio

PLEASE PRINT CLEARLY

| NAME | ADDRESS | EMAIL ADDRESS | PHONE # |
|----------------------|-----------------------------------|---|-------------------|
| ✓ CARL Shoffstall | 127 Tyler Dr Sarasota FL 34234 | CARL.shoffstall @floridaplastics.com | 813-967 2689 |
| ✓ V Calderin | POB 4241 SRQ 34230 | VMCALderon@aol.com | 941-366 3708 - |
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Boulevard of the Arts/10th St Complete Streets Public Meeting

Monday, May 23, 2022
5:30 PM

City Hall Annex – SRQ Media Studio

PLEASE PRINT CLEARLY

| NAME | ADDRESS | EMAIL ADDRESS | PHONE # |
|-------------------|---|----------------------------------|----------------|
| ✓ Jude Levy | #1013 930 N. Tamiami Tr 34236 | levyjude@gmail.com | 706-0545 |
| ✓ Julie Leach | WBTT 1012 N Orange Ave | jleach@westcoastblacktheatre.org | (941) 366-1505 |
| ✓ MARTHA MICHALS | 711 Coconut Ave | marthajmichals@gmail.com | — |
| ✓ William Lakin | 750 N Tamiami #1409 | wlakin@together.net | — |
| ✓ Lou Costa | 575 BLUE JAY | on file | on file |
| ✓ JOHN SCHOPFER | 1355 Blvd of the Arts Sarasota, FL 34236 | jschoepfer@outlook.com | 941 952-9438 |
| ✓ J. PARKS | 1460 Manly St 691 Coconut | JPARKS@SOUTHERNARCHITECTURE.COM | — |
| ✓ Pam. Poindexter | 59236 1064 N. TAMIAAMI TR. #1415 | poindexter@global.net | |
| ✓ Paula Sheppard | SARASOTA 34236 | | |
| ✓ Jen Ahern-Koh | on file | | |

Comments by Richard Thomas



MATCHLINE - SEE IMAGE BELOW

MATCHLINE - SEE IMAGE ABOVE



rthomas 247 @ comcast.net
5/23/22

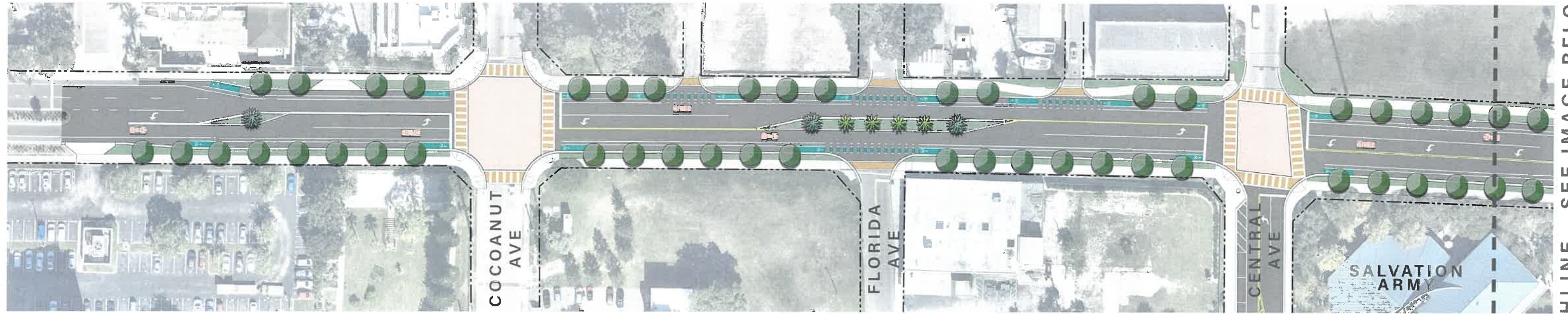
BOULEVARD OF THE ARTS

- PLACE A GREEN DOT NEXT TO WHERE YOU WOULD LIKE TO SEE MORE TREES
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Bus route mainst



Bus to Orange Ave



MATCHLINE - SEE IMAGE BELOW

MATCHLINE - SEE IMAGE ABOVE



10TH STREET



PLACE A GREEN DOT NEXT TO WHERE YOU WOULD LIKE TO SEE MORE TREES

PLACE A YELLOW DOT NEXT TO WHERE YOU WOULD LIKE TO SEE MORE SITE FURNISHINGS

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Boulevard of the Arts/10th St Complete Streets
Public Meeting
May 23, 2022

COMMENT CARD

NEED MUCH MORE PARKING
FOR THE BAY ATTENDEE!

Optional:

Name: _____ Phone: _____

Email: _____

Boulevard of the Arts/10th St Complete Streets
Public Meeting
May 23, 2022

COMMENT CARD

10TH STREET MUST HAVE
4 LANES. IF ONLY TWO,
MORE E→W TRAFFIC ON
BLVD OF ARTS + FRUITVILLE!
HOW DO FOLKS TRAVEL E↔W?

Optional:

Name: _____ Phone: _____

Email: _____

Boulevard of the Arts/10th St Complete Streets
Public Meeting
May 23, 2022

COMMENT CARD

A tree or two (or other greenery)
at 10th/Orange into Gillespie
Park

Optional:

Name: Linda Holland Phone: _____

Email: _____

Boulevard of the Arts/10th St Complete Streets
Public Meeting
May 23, 2022

COMMENT CARD

Thank you for clarifying
things. Most importantly
would be the schedule of
the round-about construction
on 10th in relation to school
traffic/school year schedule.

Optional:

Name: Fred Fout Phone: 941-926-1700 ^{ex 297}

Email: fred.fout@oursma.org

Boulevard of the Arts/10th St Complete Streets
Public Meeting
May 23, 2022

COMMENT CARD

I have great concern about the
intersection at Blvd of Arts +
41. Cars going south^{on 41} will often do
a U-Turn to go North on 41. Pedestrians
crossing at the light often have to
dodge these cars. Will there be
a pedestrian overpass?

Optional:

Name: Chris Baer Phone: _____

Email: baerc3436@gmail.com

Boulevard of the Arts/10th St Complete Streets
Public Meeting
May 23, 2022

COMMENT CARD

10th Street needs more lanes than
it currently has and more law
enforcement presence. It's too
congested at this time and could get
worse. Of course, it needs to
be beautified too.

Optional:

Name: Norman Freeman Phone: 941-374-6542

Email: normanfrmn@comcast.net
normanfreeman19@comcast.net

**Boulevard of the Arts/10th St Complete Streets
Public Meeting
May 23, 2022**

COMMENT CARD

It would be great to overlay
future plans on the map. For
example the Bay Park.

• Could the corridor be more activated?
- restrict parking along street - provide
active frontage (retail, housing w/ stoops etc) →

Optional:

Name: Gabriella Folino Phone: 415-987-9836

Email: folino.gabriella@gmail.com

-opportunity for linear parks!

*offer incentives for developers to
provide a setback

*create a linear park at key
intersections, (art, playground,
benches etc)

*see linear parks in San
Diego

Boulevard of the Arts/10th St Complete Streets
Public Meeting
May 23, 2022

COMMENT CARD

* canopy trees on BOTA
- large
need to mitigate
loss of trees at
4th/Cocoant area

Optional:

Name: DAVID LOUGH Phone: (710) 497-9230
Email: davidlough1@gmail.com

Boulevard of the Arts/10th St Complete Streets
Public Meeting
May 23, 2022

COMMENT CARD

Looks great!

Optional:

Name: _____ Phone: _____
Email: _____

Boulevard of the Arts/10th St Complete Streets
Public Meeting
May 23, 2022

COMMENT CARD

Wide sidewalks
Canopy trees
please

Optional:

Name: Wide Levy Phone: _____
Email: _____

From: [Larry Grossman](mailto:Larry.Grossman)
To: corinne.tucker@sarasotafl.com
Subject: Comments on Complete Streets
Date: Wednesday, May 25, 2022 6:44:58 AM

Appreciate the opportunity to comment on the Complete Street proposal for Boulevard for the Arts and for 10nth Street in Sarasota.

I commend the City for its dedication towards making Downtown Sarasota and its urban neighborhoods safer for pedestrians and bicyclists through street redesign.

I sent you an article which features the intersection neckdown as a means of slowing vehicular traffic and making the pedestrian crossings safer and shorter. I would like the City to redesign the intersections to project or bubble out the sidewalks and place the pedestrian crossings to connect the extended sidewalks. Get rid of the traditional radial sidewalk curves that encourage vehicles to whip around the corner. Make them slow down and square up and look for oncoming pedestrians or bicyclists before making a right turn. Signalized intersections are particularly dangerous as motorists try to beat the green or right turn on the red.

I don't think there is a good solution for the intersection of Boulevard for the Arts and US Route 41. Too small for a roundabout so the signal will have to stay. The elevated pedestrian platform at the intersection may slow traffic although with the green light it is likely that traffic will plow through.

As an aside I would relocate the art piece at the Embassy Suites building corner which is obliterated by the traffic signalization equipment to another location, perhaps the median of Boulevard for the Arts west of the intersection with US 41.

I have the same comment for 10nth Street with respect to intersection redesign to offer neckdowns instead of traditional radial curves. These extensions of sidewalk also allow for street furniture like trash receptacles or bike racks to be located on the sidewalk without interfering with pedestrian

movement.

10nth Street still has too much roadway compared to traffic and the queues for turning movements are far too generous.

Thank You

Larry Grossman

9412255741

Sent from [Mail](#) for Windows

Richard C Thomas

May 27,2022

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These comments refer to my mark-up drawing of Boulevard of the Arts dated 5/27/22 and hand-delivered to Corrine Arriaga 5/27/22:

We require an efficient public transit system in order to accommodate the increased population density and commerce that this neighborhood and our city as a whole are experiencing. In 2002 our MPO adopted the findings of a Public Transportation System Analysis which recommended a Best Bus Plan.

When the Bay Runner trolley began service along Main Street to South Lido this year an important part of that plan was realized, albeit two decades in arrears. The Best Bus Plan calls for the primary trunk route within city limits to run from SRQ through downtown to South Tamiami Trail. The route through downtown that provides service for the most customers is SRQ – North Trail – Boulevard of the Arts – Orange Avenue – Mound Street – South Trail.

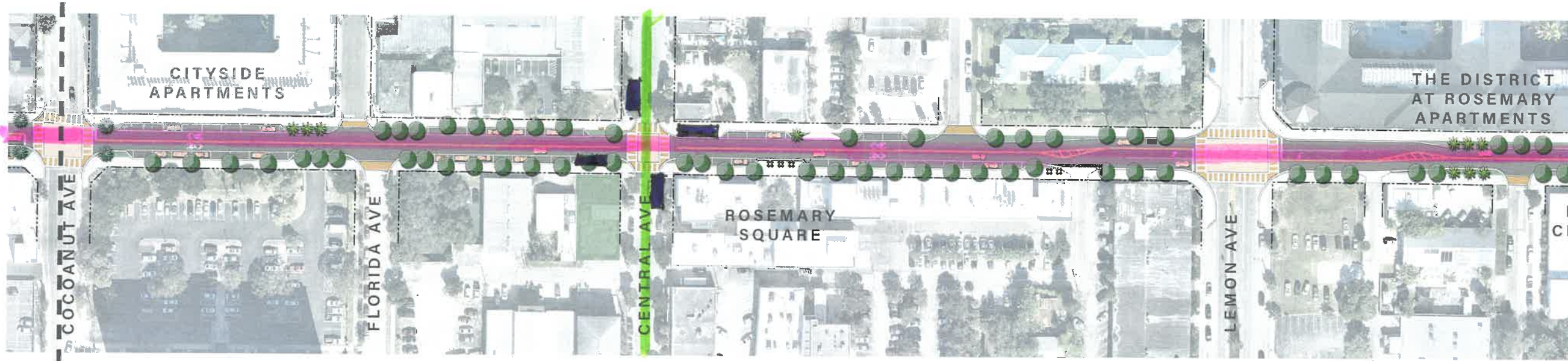
My mark-up drawing shows the primary North-South bus line in pink. The local bus line currently running along Coconut Avenue should be shifted to Central Avenue as shown by the green line. Bus stops are depicted as blue triangles.

Bus prioritize all intersections. The Boulevard of the Arts – North Orange Avenue intersection could use a roundabout.

Our city must catch up to the demands that past and continuing development place on it. Our citizens deserve a public transportation system that answers our mobility needs. SCAT is clearly not up to the task. The Lemon Avenue bus terminal must go. Heed the Best Bus Plan. Do something!



MATCHLINE - SEE IMAGE ABOVE



Richard C. Thomas 5/29/2022
 See attached letter for Comments

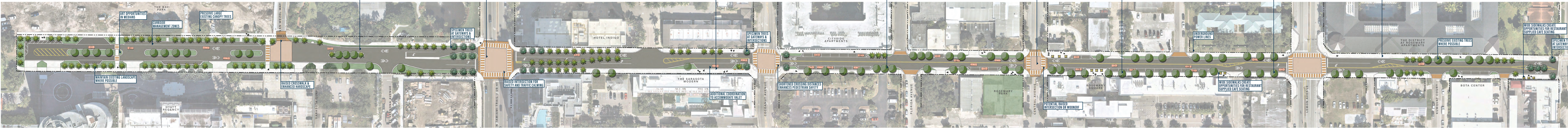
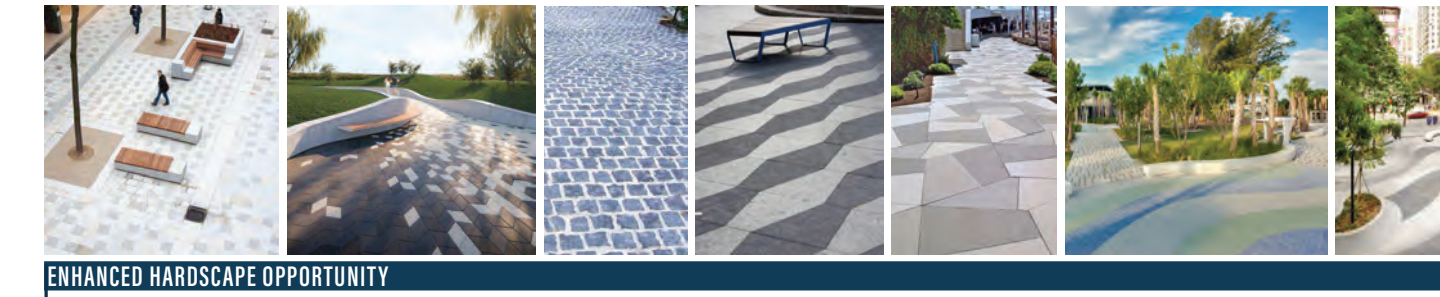
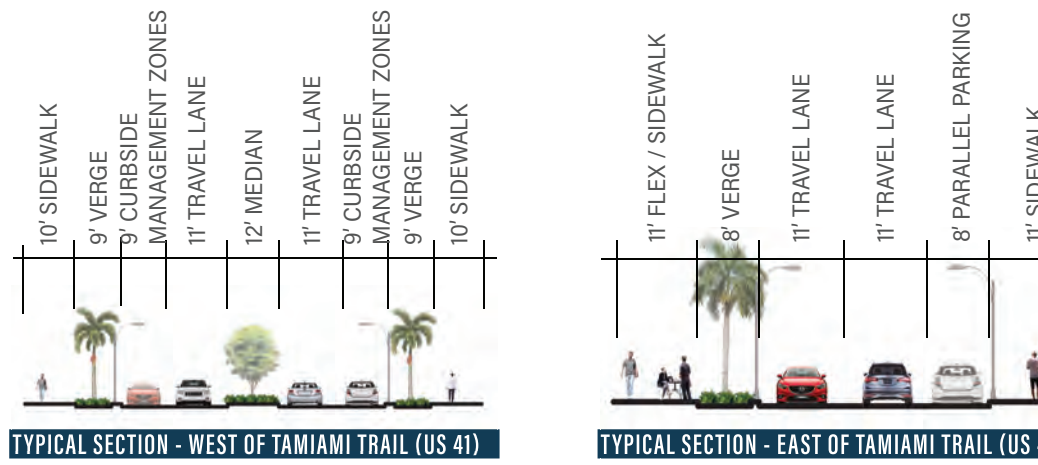
BOULEVARD OF THE ARTS

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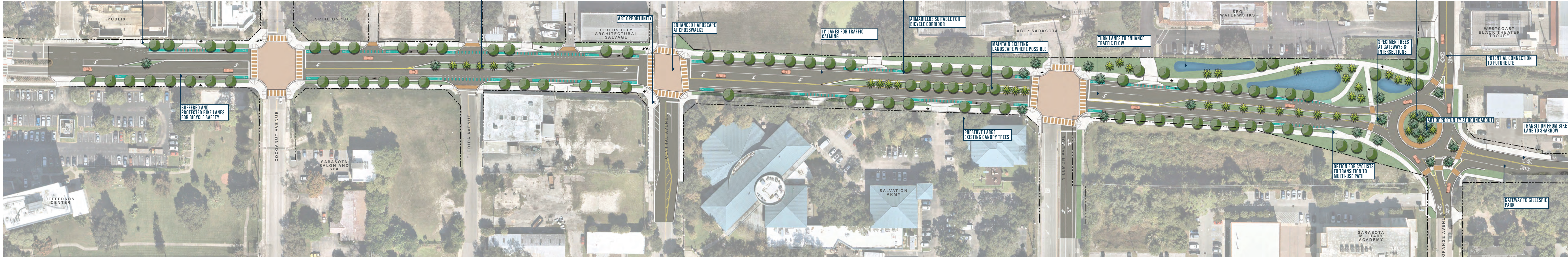
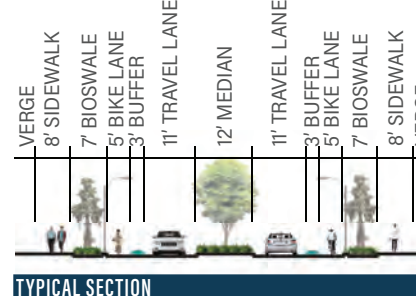
APPENDIX

B

**BOULEVARD OF THE ARTS AND
10TH FULL SIZE CONCEPT PLAN**



BOULEVARD OF THE ARTS
SARASOTA, FLORIDA



10TH STREET
SARASOTA, FLORIDA

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